



HILLINGDON
LONDON



Major Applications Planning Committee

To Councillors on the Committee

Eddie Lavery (Chairman)
Ian Edwards (Vice-Chairman)
Peter Curling
Jazz Dhillon
Janet Duncan (Labour Lead)
Carol Melvin
John Morgan
Brian Stead
David Yarrow

Date: THURSDAY, 5 MARCH 2015

Time: 7.00 PM, OR UPON THE
RISING OF THE NORTH
PLANNING COMMITTEE,
WHICHEVER IS THE
LATER.

Venue: COMMITTEE ROOM 5 -
CIVIC CENTRE, HIGH
STREET, UXBRIDGE UB8
1UW

**Meeting
Details:** Members of the Public and
Press are welcome to attend
this meeting

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further information.**

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Putting our residents first

Lloyd White
Head of Democratic Services
London Borough of Hillingdon,
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www.hillingdon.gov.uk

Useful information for residents and visitors

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A useful guide for those attending Planning Committee meetings

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Petitions and Councillors

Petitions - Those who have organised a petition of 20 or more borough residents can speak at a Planning Committee in support of or against an application. Petitions must be submitted in writing to the Council in advance of the meeting. Where there is a petition opposing a planning application there is also the right for the applicant or their agent to address the meeting for up to 5 minutes.

Ward Councillors - There is a right for local councillors to speak at Planning Committees about applications in their Ward.

Committee Members - The planning committee is made up of the experienced Councillors who meet in public every three weeks to make decisions on applications.

How the Committee meeting works

The Planning Committees consider the most complex and controversial proposals for development or enforcement action.

Applications for smaller developments such as householder extensions are generally dealt with by the Council's planning officers under delegated powers.

An agenda is prepared for each meeting, which comprises reports on each application

Reports with petitions will normally be taken at the beginning of the meeting.

The procedure will be as follows:-

1. The Chairman will announce the report;
2. The Planning Officer will introduce it; with a presentation of plans and photographs;
3. If there is a petition(s), the petition organiser will speak, followed by the agent/applicant followed by any Ward Councillors;

4. The Committee may ask questions of the petition organiser or of the agent/applicant;
5. The Committee debate the item and may seek clarification from officers;
6. The Committee will vote on the recommendation in the report, or on an alternative recommendation put forward by a Member of the Committee, which has been seconded.

About the Committee's decision

The Committee must make its decisions by having regard to legislation, policies laid down by National Government, by the Greater London Authority - under 'The London Plan' and Hillingdon's own planning policies as contained in the 'Unitary Development Plan 1998' and supporting guidance. The Committee must also make its decision based on material planning considerations and case law and material presented to it at the meeting in the officer's report and any representations received.

Guidance on how Members of the Committee must conduct themselves when dealing with planning matters and when making their decisions is contained in the 'Planning Code of Conduct', which is part of the Council's Constitution.

When making their decision, the Committee cannot take into account issues which are not planning considerations such as the effect of a development upon the value of surrounding properties, nor the loss of a view (which in itself is not sufficient ground for refusal of permission), nor a subjective opinion relating to the design of the property. When making a decision to refuse an application, the Committee will be asked to provide detailed reasons for refusal based on material planning considerations.

If a decision is made to refuse an application, the applicant has the right of appeal against the decision. A Planning Inspector appointed by the Government will then consider the appeal. There is no third party right of appeal, although a third party can apply to the High Court for Judicial Review, which must be done within 3 months of the date of the decision.

Agenda

CHAIRMAN'S ANNOUNCEMENTS

- 1 Apologies for Absence
- 2 Declarations of Interest in matters coming before this meeting
- 3 To sign and receive the minutes of the meetings held on 21 January 2015 and 10 February 2015 1 - 12
- 4 Matters that have been notified in advance or urgent
- 5 To confirm that the items marked in Part 1 will be considered in public and those items marked in Part 2 will be heard in private

PART I - Members, Public and Press

Items are normally marked in the order that they will be considered, though the Chairman may vary this. The name of the local ward area is also given in addition to the address of the premises or land concerned.

Major Applications with a Petition

	Address	Ward	Description & Recommendation	Page
6	88-94 Long Lane, Ickenham 52129/APP/2014/2996	Ickenham	Demolition of 5 existing dwellinghouses and redevelopment of the site for a 85 unit Class C2 care home for the elderly of 1.5 to 2.5 storeys in height with associated landscaping and car parking (40 spaces in total), stopping up of existing vehicular accesses on Long Lane and construction of new vehicular access onto Long Lane. Recommendation: Approval	13 - 70 168-187

Major Applications without a Petition

	Address	Ward	Description & Recommendation	Page
7	Hayes and Harlington, Station Road, Hayes 10057/APP/2014/4338	Botwell	Application under Schedule 7 of the Crossrail Act 2008 for the approval of Plans and Specifications associated with the erection of a new station building, external lift shaft, footbridge with lift shafts, extension of platforms 2/3 and 4/5 with canopy modifications, new waiting room to Platform 4/5 and associated lighting. Recommendation: Approval	71 - 84 188-109
8	Pronto Industrial Estate and 585 - 591 Uxbridge Road, Hayes 4404/APP/2014/2506	Botwell	Change of use of B1(c) floorspace to provide 12 additional residential units and associated ancillary works. (Amendment to planning permission ref: 4404/APP/2011/2079, dated 30-03-2012 (Application to replace extant planning permission ref: 4404/APP/2008/3558, dated 23-03-2009); Redevelopment of site to provide replacement Class B1(c) light industrial space and 34 two-bedroom and 9 one-bedroom flats with associated car parking, landscaping and amenity space). Recommendation: Approval	85 - 104 210-217
9	1 Nobel Drive, Harlington 46214/APP/2014/2827	Heathrow Villages	Conversion and extension of existing office building to form a 200 bedroom hotel with banqueting suite, conference facilities, and rooftop restaurant, including a seven-storey extension to rear, a three storey addition at roof level, and single-storey side extension, together with the creation of a new vehicle access, and alterations to car parking and landscaping. Recommendation: Approval	105 - 148 218-248

10	West Drayton Garden Village, Porters Way, West Drayton 5107/APP/2014/4304	West Drayton	Reserved matters (appearance and landscaping) in compliance with conditions 2 and 3 for Phase 5 (Block A) (82 residential units) of planning permission ref: 5107/APP/2009/2348, dated 01/10/2010, for the proposed mixed used redevelopment of the Former NATS Site. Recommendation: Approval	149 - 166 249-276
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Plans for Major Applications Planning Committee

12 Plans Pack - 5th March 2015

167 - 276

Minutes

MAJOR APPLICATIONS PLANNING COMMITTEE

21 January 2015

Meeting held at Council Chamber - Civic Centre,
High Street, Uxbridge UB8 1UW



HILLINGDON
LONDON

	<p>Committee Members Present: Councillors Eddie Lavery (Chairman), Ian Edwards (Vice-Chairman), Jazz Dhillon, Janet Duncan (Labour Lead), Carol Melvin, John Oswell, Brian Stead, Alan Chapman and David Yarrow</p> <p>Also Present: Cllr Brian Crowe and Cllr John Riley (<i>Item 6</i>)</p> <p>LBH Officers Present: Matthew Duigan, Planning Service Manager, Syed Shah, Highway Engineer, Adrien Waite, Major Applications Manager, Nicole Cameron, Legal Advisor Danielle Watson, Democratic Services Officer.</p>
121.	<p>APOLOGIES FOR ABSENCE (<i>Agenda Item 1</i>)</p> <p>Apologies for absence were received from Cllr Peter Curling and Cllr John Morgan with Cllr John Oswell and Cllr Alan Chapman substituting.</p>
122.	<p>DECLARATIONS OF INTEREST IN MATTERS COMING BEFORE THIS MEETING (<i>Agenda Item 2</i>)</p> <p>None.</p>
123.	<p>MATTERS THAT HAVE BEEN NOTIFIED IN ADVANCE OR URGENT (<i>Agenda Item 3</i>)</p> <p>None.</p>
124.	<p>TO CONFIRM THAT THE ITEMS MARKED IN PART 1 WILL BE CONSIDERED INPUBLIC AND THOSE ITEMS MARKED IN PART 2 WILL BE HEARD IN PRIVATE (<i>Agenda Item 4</i>)</p> <p>It was confirmed that all items would be considered in Part 1 public.</p>
125.	<p>GAELIC ATHLETIC ASSOCIATION, WEST END ROAD, RUISLIP 24373/APP/2014/1946 (<i>Agenda Item 5</i>)</p> <p>Extension of car park from 92 to 136 spaces, including 8 disability and 7 electric bays; additional vehicular exit and new pedestrian gate to West End Road; 30 cycle parking stands; taxi point; pedestrian circulation routes; realignment and reconstruction of grass playing pitch; new irrigation (groundwater abstraction borehole), drainage and storm water attenuation works; erection of partially roofed (three bays) single tier stand for spectator seating with ancillary</p>

accommodation below including changing rooms for players and officials, gym, medical facilities and toilets; pedestrian only access path along northern boundary; a low level standing terrace, with covered area for disabled spectators and a camera stand; new southern perimeter access track; floodlit artificial-surfaced training/warm-up area with demountable enclosure to enable use as part of overflow car parking area (186 spaces); 15 metres high ball catch netting at each end of the pitch and retractable netting 7 metres high to the northern boundary; enclosures within the site comprising fencing, turnstiles and gates; replacement scoreboard; and, associated works including landscaping.

Officers introduced the report and referred members to the addendum sheet that had been circulated.

The application related to the Gaelic Athletic Association (GAA) Grounds on West End Road in Ruislip. Members noted that the ground had a longstanding use as a sports and recreation ground and had been used as a training ground by Queen's Park Rangers Football Club until the mid 1970's. Since this time the grounds had been used by the GAA and was the home of Gaelic sports (Gaelic football and hurling) in London.

The current application sought the redevelopment of the sports ground in order to substantially improve the existing facilities, which were considerably outdated and did not meet modern expectations or requirements in terms of player or spectator comfort or safety.

Members noted that the main issues in this case were considered to be the principle of the development within the Green Belt, impacts on neighbour amenity and traffic impacts. In principle there was support for the enhancement of facilities at the site which would support sport, culture, wellbeing and health objectives. Overall, officers considered that the proposal fully complied with Green Belt policies and was acceptable in principle. This was a view shared by the Greater London Authority.

In relation to the impact of the proposals on neighbouring amenity, neighbours had expressed concerns about loss of light, increased noise, loss of privacy and the impact of floodlighting.

In relation to loss of light the main structures were proposed to be separated from the residential properties by a sufficient distance to avoid loss of light or dominance. With respect to noise this had been considered in the context that the site was currently a sports ground utilised for competitive sporting events, and that the proposals were intended to enhance the facilities. Members noted that the ground capacity and number of matches would not be increased. It was acknowledged that the enhanced facilities may result in some increase in attendance at smaller games.

Members were informed that whilst the existing and proposed use of the ground would inevitably cause a degree of noise on match days, the Council's Environmental Protection Unit considered the proposals acceptable and it was not considered a refusal on noise grounds could be substantiated.

The floodlights have been carefully designed to avoid light spill and would be subject to

conditions controlling their use, in these circumstances it was not considered that the floodlights would cause unacceptable impacts on neighbour amenity.

In relation to privacy, Members noted that the main stand was over 100m from the residential properties meaning there would be no unacceptable impacts when considered against Policy. Members questioned the boundary treatment condition as it read as if the responsibility lay with the householder rather than the developer. Officers suggested that the condition could be amended and updated to address Members concerns. Members also questioned the height of the fences as there was concern that spectators would be able to see over residents' fences if they were on the stand, additional concerns regarding litter were also raised. Officers informed Members that the stands would be used only during larger games and that the final details of the fences could be secured by condition.

Impacts on residential amenity and the operation of the public highway were considered to be appropriately addressed within the application such that they would not have unacceptable impacts. Members asked for clarity about the positioning of the pedestrian access and asked whether residents objecting to the scheme were mistaken. Officers informed Members that there was no pedestrian access against the rear of the resident's properties.

A verbal change to the addendum was made and corrected, which should read 282 car parking spaces not 382. In relation to parking, Members were aware that the facility had existing parking arrangements, however, the training ground could be used as an overflow car park.

Members questioned condition 20 regarding the PA system, which did not give details of when the system would be in use. Officer informed Members that hours of use would be included to address Members concerns.

The recommendation for approval was moved, seconded and on being put to the vote was unanimously agreed.

Resolved - That the application be approved, subject to the conditions and informatives set out in the officer's report and addendum sheet circulated at the meeting.

126. **15 KING EDWARDS ROAD, RUISLIP 43419/APP/2014/3879** (*Agenda Item 6*)

Erection of two detached houses with dormers and integral garages, new crossovers and associated external works (involving demolition of the existing house).

Officers introduced the report and referred members to the addendum sheet that had been circulated.

The application related to 15 King Edwards Road, Ruislip which sought the demolition of the existing house and the erection of two new detached houses. The site was located within the developed area in a street characterised by detached houses in plots of varying size.

The application would serve to improve the utilisation of land and increase the amount of housing provided at the site in accordance with adopted planning policy. Officers explained that the design and layout was considered acceptable and that the proposal would fit appropriately within the context of the surrounding street.

The development would provide appropriate living conditions for its future occupiers and would avoid any unacceptable impacts on the amenity of neighbouring occupiers. Further the proposal was served by adequate parking, was acceptable in highways terms, and would provide appropriate levels of accessibility.

Officers informed Members that Ruislip Conservation Panel had made comments which were not included in the addendum, however, no new issues had been raised.

A local Ward Councillor spoke regarding the proposals and made the following comments:

- Ward Councillors had concerns.
- The Council's Conservation Officers views had not been challenged.
- The proposed site was cramped.
- The officers' presentation did not give a full view of the property.
- There were parking issues in Monarch's Way which would be exasperated.
- The demolition of the existing building could not be justified.
- Every conservation area had a boundary.
- The proposal would not enhance the area.
- The existing property added character to the area.

The Chairman highlighted that there was objection made by the local residents' association and no petition had been received. Members appreciated when Ward Councillors attended meetings to put their views across, however, Members were mindful that there were no grounds to refuse the proposal and that such as decision would be overturned at appeal.

Members questioned whether a condition could be added that would prevent the garage being converted into additional living spaces. Officers agreed that a condition could be added to alleviate Members concerns.

The recommendation for approval was moved, seconded and on being put to the vote was unanimously agreed.

Resolved - That the application be approved, subject to the conditions and informatives set out in the officer's report and addendum sheet circulated at the meeting.

127. **COMET STORE, WILLOW TREE LANE, HAYES 56751/APP/20174/3915** (*Agenda Item 7*)

Variation of condition 19 (Restriction on Sale of Goods) of planning permission

198/APP/2001/69 dated 24/04/2001 in order to enable the sale of additional goods from Unit 2, Willow Tree Land Retail Park (S73 Application).

Officers introduced the report and referred members to the addendum sheet that had been circulated.

The application related to the vacant comet store at Willow Tree Lane and sought to amend the types of goods which could be sold at the premises to facilitate its occupation by B&M Ltd.

Officers considered that in this case given the limited extension to the range of goods the proposal would not result in any unacceptable impacts on other centres. Further the proposal would bring back into use a vacant retail unit which would serve to create jobs and benefit the economy overall.

The recommendation for approval was moved, seconded and on being put to the vote was unanimously agreed.

Resolved - That the application be approved, subject to the conditions and informatives set out in the officer's report and addendum sheet circulated at the meeting.

The meeting, which commenced at 6.00 pm, closed at 6.50 pm.

These are the minutes of the above meeting. For more information on any of the resolutions please contact Danielle Watson on Democratic Services Officer: 01895 277488. Circulation of these minutes is to Councillors, Officers, the Press and Members of the Public.

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Minutes

MAJOR APPLICATIONS PLANNING COMMITTEE

10 February 2015

Meeting held at Committee Room 5 - Civic Centre,
High Street, Uxbridge UB8 1UW



HILLINGDON
LONDON

	<p>Committee Members Present: Councillors Eddie Lavery (Chairman), Ian Edwards (Vice-Chairman), Peter Curling, Jazz Dhillon, Janet Duncan (Labour Lead), Carol Melvin, John Morgan, Brian Stead and David Yarrow</p> <p>LBH Officers Present: Matthew Duigan, Planning Service Manager, Syed Shah, Highway Engineer, Adrien Waite, Major Applications Manager, Nicole Cameron, Legal Advisor Danielle Watson, Democratic Services Officer.</p>
128.	<p>APOLOGIES FOR ABSENCE (<i>Agenda Item 1</i>)</p> <p>None.</p>
129.	<p>DECLARATIONS OF INTEREST IN MATTERS COMING BEFORE THIS MEETING (<i>Agenda Item 2</i>)</p> <p>Cllr. Peter Curling declared a pecuniary interest in item 7 as he was an employee of Brunel University and left the room during the consideration thereof.</p>
130.	<p>TO SIGN AND RECEIVE THE MINUTES OF THE MEETINGS HELD ON 9 DECEMBER 2014 AND 6 JANUARY 2015 (<i>Agenda Item 3</i>)</p> <p>The minutes of the meetings held on 9 December 2014 and 6 January 2015 were agreed as a correct record.</p>
131.	<p>MATTERS THAT HAVE BEEN NOTIFIED IN ADVANCE OR URGENT (<i>Agenda Item 4</i>)</p> <p>None.</p>
132.	<p>TO CONFIRM THAT THE ITEMS MARKED IN PART 1 WILL BE CONSIDERED IN PUBLIC AND THOSE ITEMS MARKED IN PART 2 WILL BE HEARD IN PRIVATE (<i>Agenda Item 5</i>)</p> <p>It was confirmed that all items would be considered in Part 1 public.</p>
133.	<p>E C HOUSE, SWALLOWFIELD WAY, HAYES - 38065/APP/2015/206 (<i>Agenda Item 6</i>)</p> <p>Section 73 application seeking a variation to condition 2 (approved plans) of planning permission ref. 38065/APP/2014/2143 (Re-development of the site to provide 5 industrial units (Use Class B1(c), B2 and B8) with associated parking,</p>

servicing and landscaping (Involving demolition and refurbishment of existing units) dated 2/12/14) to remove the 2 metre gap between units 2 and 3 by extending either or both units.

Officers introduced the report and referred members to the addendum sheet that had been circulated.

The application related to EC House on Swallowfield Way and sought a minor material amendment to a previously granted planning consent. The amendment in effect sought to fill in the gap between two buildings. The amendment was not considered to result in any unacceptable material planning impacts. Members noted that there would be no neighbouring impact.

The recommendation for approval was moved, seconded and on being put to the vote was unanimously agreed.

Resolved - That the application be approved, subject to the conditions and informatives set out in the officer's report and addendum sheet circulated at the meeting.

134. **BRUNEL UNIVERSITY, KINGSTON LANE, HILLINGDON - 532/APP/2014/4036**
(Agenda Item 7)

Variation of condition 7 (to extend opening hours until 22:00 Monday to Saturday) of planning permission ref: 532/APP/2001/1858 dated 15/10/2002: Provision of replacement sports pitches (including new synthetic playing surfaces), erection of floodlighting, boundary fences and storage building, together with associated parking and access improvements (Site 3). (S73 application).

Officers introduced the report and referred members to the addendum sheet that had been circulated.

The application related to Site 3 at Brunel University, the site of the sports pitches and sought the variation of a condition relating to the hours of use of floodlighting. Members noted that the floodlighting was originally given consent in 2002 and this application permitted the existing floodlights on site and floodlights on the cemetery pitch. The floodlights on the cemetery pitch had to date not been installed, but the permission was valid and the applicant was now intending to complete this aspect of the works.

The application also sought the extended hours of operation for the floodlighting. Members noted that it was also extremely relevant that the Council previously granted a temporary variation of the condition to enable the use of the flooding on the site until 22:00 Monday to Saturday for a temporary period of 18 months. The purpose of this temporary consent was to ascertain whether the extended hours of operation would have adverse impacts on the amenity of nearby residents.

Members noted that there were no complaints received in respect of the floodlighting during this temporary period which was extremely relevant when considering this matter; however, it was relevant that only the floodlights currently on the site had been subject to a trial period and that the floodlights on the cemetery pitch had no such trial.

Officers explained that in this case the main reasons for the conditions existence

related to the protection of residential amenity and therefore this was the sole consideration. Members noted that 6 letters of objection had been received raising concerns relating to light and noise nuisance, however, in respect of the existing floodlights they had been operated at the requested hours for an 18 month trial period, during which no complaints regarding either light or noise nuisance have been received by the Council.

The lack of complaints during the trial period did provide strong evidence that the use of the existing floodlights for the extended hours did not result in unacceptable impacts on the amenity of nearby residents and weighed strongly in favour of the grant of permanent permission in relation to these floodlights.

Members noted that in respect of the lighting for the cemetery pitches this had not been the subject of any such trial period. As such, there was less evidence to demonstrate that extended operation of lights in this location would not cause adverse impacts. It was therefore considered that only a temporary period, to allow for a trial, should be granted in respect of these particular floodlights.

Officers informed Members that no alterations would be made to the parking arrangements and clarified that the application referenced variations of the original application, simply restating what permission was originally granted for.

Members questioned whether residents of Robinwood Grove were happy with the proposals as they had previously raised concerns which were the reason for the floodlights to be restricted for use past 9pm. Officers informed Members that no complaints had been received from residents of Robinwood Grove.

The recommendation for approval was moved, seconded and on being put to the vote was unanimously agreed.

Resolved - That the application be approved, subject to the conditions and informatives set out in the officer's report and addendum sheet circulated at the meeting.

135. **SILVERDALE HOUSE, PUMP LANE, HAYES - 49670/APP/2014/3854** (*Agenda Item 8*)

Demolition of Silverdale House and the erection of a new industrial unit, use class B1(c), B2 and B8, at the Provident Industrial Estate, Pump Lane, Hayes.

Officers introduced the report and referred members to the addendum sheet that had been circulated.

The application related to Silverdale House on Pump Lane, which was a dilapidated industrial building located within a designated Industrial and Business Area and a Strategic Industrial Location. The application sought permission for the demolition of the existing industrial building and its replacement with a new purpose building industrial building to modern standards.

Members noted that also relevant to this consideration was the fact that a similar proposal for 3 industrial units was granted consent. The schemes were not identical and there had been changes to planning policy in the intervening period, however, the relationships were similar and the main development plan and intent of relevant policies remained the same.

Members noted that the separation from the nearest residential property was 26.5m which compared favourably to the previous consent which was 21.8m. The principle of the proposed industrial development was acceptable within an industrial and business area and the proposal would enhance the appearance of the area. Additionally, in respect to impacts on residential occupiers, the proposal would be adequately separated such as to avoid impacts by way of dominance or loss of outlook and would have no windows which would cause loss of privacy.

In respect of noise the proposal would have a service yard on the Pump Lane frontage and a condition would secure appropriate boundary treatment to mitigate noise to acceptable levels. Subject to conditions it was considered the proposal would therefore reduce noise impacts when compared to a continued use of the existing site. Members requested that an additional condition be added to landscaping to physically prevent access.

Members questioned the ratio of parking numbers and officer clarified that there were 17 proposed parking spaces, 5 of which were electric charging points and 2 for disabled parking.

The recommendation for approval was moved, seconded and on being put to the vote was unanimously agreed.

Resolved - That the application be approved, subject to the conditions and informatives set out in the officer's report and addendum sheet circulated at the meeting.

136. **SILVERDALE HOUSE, PUMP LANE, HAYES - 49670/APP/2014/3855** (*Agenda Item 9*)

Redevelopment of the site to provide 6 industrial units (Use Classes B1(c), B2 and B8) and the provision of associated landscaping, parking and service space (involving demolition of existing building).

Officers introduced the report and referred members to the addendum sheet that had been circulated.

The application sought permission for the demolition of the existing industrial building and its replacement with 6 industrial units to modern standards. This application was similar to the previous heard by Members although there would be 6 industrial units instead of the 1.

Members noted the changes in comparison to the previously heard application and requested that the 2 fire doors be conditioned to ensure that they were shut at all times unless required for the use they were intended.

Officers also made a verbal change to the addendum as there was an existing vehicular crossover which needed to be stopped up.

The recommendation for approval was moved, seconded and on being put to the vote was unanimously agreed.

Resolved - That the application be approved, subject to the conditions and

	<p>informatives set out in the officer's report and addendum sheet circulated at the meeting.</p>
137.	<p>BUILDING B5, 4 ROUNDWOOD AVENUE, STOCKLEY PARK - 37205/APP/2014/3056 (<i>Agenda Item 10</i>)</p> <p>Extensions, refurbishment, and alterations, including recladding, to existing office building, together with associated works, including landscaping and alterations to car parking.</p> <p>Officers introduced the report and outlined details of the application.</p> <p>Members noted that the Committee had previously resolved to grant consent for permission subject to condition and a legal agreement. The legal agreement included a requirement that the original Stockley Park travel plan be amended and a £20,000 bond.</p> <p>Having further considered this matter, the original Stockley Park travel plan, which related to other units, was outside of the applicants control and therefore the original requirement was overly onerous. Further, the level of bond was higher than had been agreed on other developments in Stockley Park.</p> <p>Members questioned how these travel plans were monitored; officers informed Members that some of the travel plans were monitored by TfL to see if the business in question was achieving what was laid out in their travel plan. It was also noted that these bonds could be taken away from businesses who did not comply with their travel plan.</p> <p>The recommendation for the Heads of Term to be amended was moved, seconded and on being put to the vote was unanimously agreed.</p> <p>Resolved - That the Travel Plan Head of Terms be amended.</p>
	<p>The meeting, which commenced at 6.00 pm, closed at 6.31 pm.</p>

These are the minutes of the above meeting. For more information on any of the resolutions please contact Danielle Watson on Democratic Services Officer: 01895 277488. Circulation of these minutes is to Councillors, Officers, the Press and Members of the Public.

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Agenda Item 6

Report of the Head of Planning, Sport and Green Spaces

- Address** 88-94 LONG LANE ICKENHAM
- Development:** Demolition of 5 existing dwellinghouses and redevelopment of the site for a 85 unit Class C2 care home for the elderly of 1.5 to 2.5 storeys in height with associated landscaping and car parking (40 spaces in total), stopping up of existing vehicular accesses on Long Lane and construction of new vehicular access onto Long Lane.
- LBH Ref Nos:** 52129/APP/2014/2996
- Drawing Nos:** Interim Travel Plan
Planning Statement
AA4378/1005
AA4378/2001
AA4378/2004 rev. A
AA4378/2011
AA4378/2020
AL4378/001 Rev. B
AL4378/002 Rev. B
Acoustic report
Arboricultural Report
Care Home Needs Assessment
Construction Method Statement
Design and Access Statement
Ecological Assessment
Economic Benefits Statement
Energy Report
Heritage Statement
Statement of Community Engagement
Townscape and Visual Assessment
Air Quality Assessment
Transport Statement (as amended by 4 x A4 autotrack diagrams, accident data and trip generation calculations received on 13/1/15)
Letter from Carterwood, dated 20/8/14
Landscape Design
Agent's covering email dated 21/10/14
2595.08B
Agent's covering email dated 11/9/14
AA4378/2006 rev. E
AA4378/2007 rev. E
AA4378/2008 rev. D
AA4378/2015 rev. A
AA4378/2016 rev. A
AA4378/2005 rev. E
AA4378/2017 rev. A
AA4378/2018 rev. A
AA4378/2019 rev. A
Un-numbered Plan Diagram of Roof Pitches

Date Plans Received: 20/08/2014
Date Application Valid: 15/09/2014

Date(s) of Amendment(s): 20/01/2015
20/08/2014
21/10/2014
14/02/2015
13/01/2015
11/09/2014
09/10/2014

1. **SUMMARY**

This application is for the demolition of 5 detached houses within the Ickenham Village Conservation Area to allow the re-development of the site for a 85 suite Class C2 care home for the frail elderly. The building would mainly comprise of a part two, part three storey block, but by concealing the upper floors with the roof space, it would appear as a one and a half to two and a half storeys high and would have an 'H' shaped layout with an extended rear wing which would extend along much of the depth of the site. Vehicular access would be provided on the northern side of the site, adjoining the Douay Martyrs School with a total of 40 car parking spaces being provided.

The proposed care home would involve the take up of much more garden land than the existing houses, but having regard to adjoining and surrounding sites which contain large buildings with in-depth footprints, the overall quality of the scheme, and the impacts of the scheme in terms of the character of the area, landscaping and tree impacts, and the considerations advised by the London Plan such as biodiversity, flood risk and climate change, it is considered that a reason for refusal could not be justified in relation to the loss of gardens.

The loss of 5 Class C3 houses is strictly contrary to policy H3 of the saved UDP but the GLA advise that the application should be considered in terms of the more up to date policies in the NPPF and London Plan which advise that Class C2 accommodation should be included as part of the Borough's overall housing provision and therefore results in a net increase of 80 units on this site. The Council's Adult Social Care do query the need for the facility within the borough, but the GLA advise that there is significant pan - London and Hillingdon need for the facility and that this need for care home places is expected to increase.

The Council's Urban Design/ Conservation Officer raises no objections to the demolition of the houses, subject to their details being recorded, which would be controlled by condition. The proposal has also formed the subject of pre-application discussions and advice and the scheme has undergone various revisions. The conservation officer is fully supportive of this scheme and considers it to be a high quality and well considered proposal that would not harm the character or appearance of the Ickenham Village Conservation Area.

The scheme would not harm the amenities of surrounding residential occupiers and fully complies with Council guidance. Furthermore, the Highway Officer is satisfied that the

proposed parking is sufficient to serve the development and the proposed access arrangements would not prejudice highway safety. The Tree/ Landscape Officer also considers that the scheme is acceptable in terms of its impact upon existing trees on site and that the proposed landscaping scheme is of a high quality that would support the care home residents and over time, improve the character of the area. Furthermore the proposal has been assessed with respect to biodiversity impacts and the Council's Sustainability Officer advises that the impact is acceptable and with enhancements that could be controlled with conditions, could be improved. Flood impacts are also acceptable.

The Mayor does not raise any in principle objections to the scheme in his Stage 1 report.

The application is recommended favourably, but further archaeological investigation is required, the results of which needs to be referred to GLAAS and the application would need to be referred back to the Mayor.

2. RECOMMENDATION

That subject to:-

1. The Mayor not directing the Council under Article 6 of the Town and Country Planning (Mayor of London) Order 2008 to refuse the application, or under Article 7 of the Order that he is to act as the local planning authority for the purpose of determining the application, and

2. The Greater London Archaeological Advisory Service (GLAAS) not objecting to the application following further archaeological investigation,

delegated powers be given to the Head of Planning and Enforcement to grant planning permission, subject to any relevant amendments agreed by the Head of Planning and Enforcement and also those requested by the Greater London Authority and the following:

A) That the Council enters into an agreement with the applicant under Section 106 of the Town and Country Planning Act 1990 (as amended) and/or Section 278 of the Highways Act 1980 (as amended) and/ or other appropriate legislation to secure:

(i) Highways Works: the developer will required to enter into a S278/S38 agreement to secure highways works including the new access onto Long Lane and ther stopping up/re-instatement as public footpath of the existing crossovers.

(ii) Travel Plan

(iii) Construction Training: A financial contribution is sought equal to Training costs: £2500 per £1m build cost + Coordinator costs of£9600 or an in kind scheme to be provided to this value.

(iv) Employment Training: An Employment Training Strategy to be submitted.

(v) Air Quality Monitoring: A financial contribution to the sum of £25.000.

(vi) Project Management & Monitoring Fee: 5% of total cash contributions.

B) That in respect of the application for planning permission, the applicant meets the Council's reasonable costs in the preparation of the S106 Agreement and any abortive work as a result of the agreement not being completed.

C) That officers be authorised to negotiate and agree the detailed terms of the

proposed agreement and conditions of approval.

D) That if any of the heads of terms set out above have not been agreed and the S106 legal agreement has not been finalised before the 6th March 2015, or any other period deemed appropriate that delegated authority be given to the Head of Planning and Enforcement to refuse the application for the following reason:

'The applicant has failed to provide a commensurate package of planning benefits to maximise the health and social benefits, namely highway improvements, construction training, air quality monitoring and project management of the scheme to the community. The proposal therefore conflicts with Policy R17 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).'

E) That subject to the above, the application be deferred for determination by the Head of Planning and Enforcement under delegated powers, subject to the completion of the legal agreement under Section 106 of the Town and Country Planning Act 1990 and other appropriate powers with the applicant.

F) Following the Council's Community Infrastructure Levy coming into force, the applicant pay the required levy on the additional floorspace actually created.

G) That if the application is approved, the following conditions be attached:-

1 COM3 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

2 COM4 Accordance with Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers, AA4378/2005 rev. E, AA4378/2006 rev. E, AA4378/2007 rev. E, AA4378/2008 rev. D, AA4378/2015 rev. A, AA4378/2016 rev. A, AA4378/2017 rev. A, AA4378/2018 rev. A and AA4378/2019 rev. A and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and the London Plan (July 2011).

3 COM5 General compliance with supporting documentation

The development hereby permitted shall not be occupied until the following has been completed in accordance with the specified supporting plans and/or documents:

Noise mitigation measures on pages 19 & 20 of the Acoustic Report

Thereafter the development shall be retained/maintained in accordance with these details for as long as the development remains in existence

REASON

To ensure that the development complies with the objectives of Policies OE1 and OE3 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

4 RES6 Levels

No development shall take place until plans of the site showing the existing and proposed ground levels and the proposed finished floor levels of all proposed buildings have been submitted to and approved in writing by the Local Planning Authority. Such levels shall be shown in relation to a fixed and known datum point. Thereafter the development shall not be carried out other than in accordance with the approved details.

REASON

To ensure that the development relates satisfactorily to adjoining properties in accordance with policy BE13 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

5 COM7 Materials (Submission)

No development shall take place until details of all materials and external surfaces, including details of balconies, window reveals, eaves lines, roof hips and verges of the new building have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy BE13 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

6 RES8 Tree Protection

No site clearance or construction work shall take place until the details have been submitted to, and approved in writing by, the Local Planning Authority with respect to:

1. A method statement outlining the sequence of development on the site including demolition, building works and tree protection measures.
2. Detailed drawings showing the position and type of fencing to protect the entire root areas/crown spread of trees, hedges and other vegetation to be retained shall be submitted to the Local Planning Authority for approval. No site clearance works or development shall be commenced until these drawings have been approved and the fencing has been erected in accordance with the details approved. Unless otherwise agreed in writing by the Local Planning Authority such fencing should be a minimum height of 1.5 metres.

Thereafter, the development shall be implemented in accordance with the approved details. The fencing shall be retained in position until development is completed.

The area within the approved protective fencing shall remain undisturbed during the course of the works and in particular in these areas:

- 2.a There shall be no changes in ground levels;
- 2.b No materials or plant shall be stored;
- 2.c No buildings or temporary buildings shall be erected or stationed.
- 2.d No materials or waste shall be burnt; and
- 2.e No drain runs or other trenches shall be dug or otherwise created, without the prior written consent of the Local Planning Authority.

REASON

To ensure that trees and other vegetation can and will be retained on site and not damaged during construction work and to ensure that the development conforms with policy BE38 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

7 RES9 Landscaping (car parking & refuse/cycle storage)

No development shall take place until a landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

1. Details of Soft Landscaping

- 1.a Planting plans (at not less than a scale of 1:100),
- 1.b Written specification of planting and cultivation works to be undertaken,
- 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate

2. Details of Hard Landscaping

- 2.a Refuse Storage
- 2.b Cycle Storage
- 2.c Means of enclosure/boundary treatments
- 2.d Car Parking Layouts (including demonstration that 5% of all parking spaces are served by electrical charging points)
- 2.e Hard Surfacing Materials
- 2.f External Lighting
- 2.g Other structures (such as play equipment and furniture)

3. Details of Landscape Maintenance

- 3.a Landscape Maintenance Schedule for a minimum period of 5 years.
- 3.b Proposals for the replacement of any tree, shrub, or area of surfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.

4. Schedule for Implementation

5. Other

- 5.a Existing and proposed functional services above and below ground
- 5.b Proposed finishing levels or contours

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies BE13, BE38 and AM14 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Policies 5.11 (living walls and roofs) and 5.17 (refuse storage) of the London Plan (July 2011)

8 COM28 Visibility Splays - Pedestrian

The access for the proposed car parking shall be provided with those parts of 2.4m x 2.4m pedestrian visibility splays which can be accommodated within the site in both directions and shall be maintained free of all obstacles to the visibility between heights of 0.6m and 2.0m above the level of the adjoining highway.

REASON

In the interests of highway and pedestrian safety in accordance with policy AM7 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

9 NONSC Programme of Building Recording

No demolition shall take place until the applicant (or their heirs and successors in title) has secured the implementation of a programme of building recording and reporting to English Heritage Recording Level 1 in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the local planning authority in writing. No development shall take place other than in accordance with the Written Scheme of Investigation unless agreed in writing with the Local Planning Authority.

REASON

Built heritage assets on this site will be affected by the development. The planning authority wishes to secure building recording in line with NPPF, and publication of results, in accordance with Section 12 of the NPPF.

10 NONSC Facilities for People with Disabilities

The care home use hereby approved shall not commence until the development has been provided with a layout and facilities which meet the requirements set out in BS 8300:2009 (Code of Practice on Design of buildings and their approaches to meet the needs of disabled people), including:-

- a. Accessible parking bays should be sited within 50m of the entrance. They should be a minimum of 4.8m x 2.4m and marked and signed in accordance with BS 8300:2009+A1:2010.
- b. Given the nature of the proposed development, at least one enlarged accessible parking bay, 3m x 6m, should be provided.
- c. In accordance with BS 8300:2009+A1:2010, clause 4.2.1.1, a setting down point and parking space, 4.8m x 8m, for taxis, Dial-a-Ride and accessible minibus vehicles with tail lifts, should be provided in close proximity to the main entrance.
- d. A ratio of 1 'assisted bathroom' (or assisted showers provided this meets residents needs) to 8 service users should be shown on plan, including details of the internal layout and specifications should be provided. Reference to BS 8300:2009+A1:2010, clause 12.3. Floor gully drainage should be provided in all bathrooms where showers are to be provided.
- e. A minimum of 10% of ensuite bathrooms should be designed to allow independent use by wheelchair users. Reference to BS 8300:2009+A1:2010 should be made.

The layout and all facilities which form part of the scheme shall be retained and maintained in good working order for so long as the building remains in use.

REASON

To ensure that the care home is accessible and welcoming to all sections of the population, including older and disabled people in accordance with policy AM13 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and policies 3.1, 3.8 and 7.2 of the London Plan (2011).

11 NONSC Fire Excavation Plan

Prior to the first occupation of the development hereby approved a comprehensive fire emergency plan that demonstrates how disabled people will be safeguarded from fire and enabled to evacuate the building shall be submitted to and approved in writing by the Local Planning Authority.

REASON

To ensure that adequate facilities are provided for people with disabilities in accordance with policy AM13 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and policies 3.1, 3.8 and 7.2 of the London Plan (2011).

12 NONSC Privacy Scheme

Notwithstanding the details on the approved plans, details of additional measures to protect the privacy of residential bedroom on the rear elevation and rear wing, which may include privacy screens and/or angled windows shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the units and thereafter permanently retained.

REASON

To ensure adequate privacy is maintained, in accordance with Policy BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

13 NONSC Mechanical Plant Noise Level

The rating level of the noise emitted from the site by any chiller or refrigeration unit, Air Conditioner or other mechanical plant shall be at least 5dB below the existing background noise level. The noise levels shall be determined at the boundary of the nearest noise sensitive premises. The measurements and assessment shall be made in accordance to the latest British Standard 4142, 'Method for rating industrial noise affecting mixed residential and industrial areas'.

REASON

To safeguard the amenity of the surrounding area in accordance with policy OE1 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

14 NONSC Scheme for the Control of Noise and Odour

No air extraction system shall be used on the premises until a scheme for the control of noise and odour emanating from the site has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of measures as may be approved by the LPA. Thereafter, the scheme shall be implemented and maintained in full compliance with the approved measures.

REASON

To safeguard the amenity of the occupants of surrounding properties in accordance with policy OE1 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012)

15 NONSC Delivery Hours

There shall be no loading or unloading of vehicles, including delivery and the loading or unloading of goods outside the hours of 08:00 and 20:00, Monday to Friday, and between the hours of 08:00 and 14:00 on Saturdays. The site shall not be used for loading or unloading of vehicles, including delivery and the loading or unloading of goods on Sundays or Bank Holidays.

REASON

To safeguard the residential amenity of the occupiers of adjoining and nearby properties in accordance with Policy OE3 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

16 NONSC External Lighting Scheme

No external lighting shall be installed on the land unless the details have first been

submitted to and approved in writing by the Council. Any installation of external lighting shall only be carried out in accordance with the previously approved details.

REASON

To safeguard the amenity of surrounding properties in accordance with policies BE13 and OE1 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

17 NONSC Details of Plant, Machinery and Fuel

Before the development is commenced details of any plant, machinery and fuel burnt, as part of the energy provision and the location of the flue(s) for the development shall be submitted to the LPA for approval. This shall include pollutant emission rates at the flue(s) with or without mitigation technologies and needs to be considered as part of a wider air quality assessment, as set out in the EPUK CHP Guidance 2012, if applicable. The use of ultra low NOx emission gas CHPs and boilers is recommended. The development should as a minimum be 'air quality neutral' and demonstrably below the relevant building emissions benchmarks.

REASON

To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

18 NONSC Energy Assessment

Prior to the commencement of development a detailed energy assessment shall be submitted to and approved in writing by the Local Planning Authority.

The assessment shall clearly set out the baseline energy performance of the development, and the detailed measures to reduce CO2 emissions by 35% from a 2013 Part L baseline. The assessment shall provide detailed specifications of the measures and technology set out in outline energy assessment. The assessment shall also include full details (including specification and location on the site) of the low and zero carbon technology preferred option. The information shall also include details of the type, location and management of biomass material should biofuel dependent boilers or heating systems be required.

Finally, the assessment must include appropriate measuring and monitoring proposals and how the information will be submitted to the Local Planning Authority to demonstrate CO2 reductions are being met.

The development must proceed in accordance with the approved scheme.

REASON

To ensure appropriate carbon savings are delivered in accordance with London Plan Policy 5.2.

19 NONSC Contaminated Land

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance Document on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate

all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use; and

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement, along with details of a watching brief to address undiscovered contamination.

(ii) If during development works contamination not addressed in the submitted remediation scheme is identified, the updated watching brief shall be submitted and an addendum to the remediation scheme shall be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a comprehensive verification report shall be submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

(iv) No contaminated soils or other materials shall be imported to the site. All imported soils for landscaping purposes shall be clean and free of contamination. Before any part of the development is occupied, all imported soils shall be independently tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

REASON

To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

20 COM31 Secured by Design

The building(s) shall achieve 'Secured by Design' accreditation awarded by the Hillingdon Metropolitan Police Crime Prevention Design Adviser (CPDA) on behalf of the Association of Chief Police Officers (ACPO). No building shall be occupied until accreditation has been achieved.

REASON

In pursuance of the Council's duty under section 17 of the Crime and Disorder Act 1998 to consider crime and disorder implications in excising its planning functions; to promote the well being of the area in pursuance of the Council's powers under section 2 of the Local Government Act 2000, to reflect the guidance contained in the Council's SPG on Community Safety By Design and to ensure the development provides a safe and secure environment in accordance with London Plan (July 2011) Policies 7.1 and 7.3.

21 NONSC Sustainable Water Management

Prior to commencement, a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how it, incorporates sustainable urban drainage in

accordance with the hierarchy set out in Policy 5.15 of the London Plan and will:

i. provide information on all Suds features including the method employed to delay and control the surface water discharged from the site and:

a. calculations showing storm period and intensity and volume of storage required to control surface water and size of features to control that volume.

b. any overland flooding should be shown, with flow paths depths and velocities identified as well as any hazards, (safe access and egress must be demonstrated).

c. measures taken to prevent pollution of the receiving groundwater and/or surface waters;

d. how they or temporary measures will be implemented to ensure no increase in flood risk from commencement of construction.

ii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime. Including appropriate details of Inspection regimes, appropriate performance specification, remediation and timescales for the resolving of issues.

iii. provide details of the body legally responsible for the implementation of the management and maintenance plan.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

iii. incorporate water saving measures and equipment.

iv. provide details of water collection facilities to capture excess rainwater;

v. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (November 2012) Policy 5.12 Flood Risk Management of the London Plan (July 2011) and National Planning Policy Framework (March 2012) and the Planning Practice Guidance (March 2014). To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

22 NONSC Scheme of Ecological Enhancement

Prior to the commencement of development an ecological enhancement scheme shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall clearly detail measures to promote and enhance wildlife opportunities within the landscaping and the fabric of the buildings including measures such as habitat walls, bird and bat boxes and nectar rich planting. The scheme shall aim to include an area of land dedicated to wildlife habitat. The development must proceed in accordance with the approved scheme.

REASON

To ensure the development contributes to ecological enhancement in accordance with Policy EM7 (Local Plan) and Policy 7.28 of the London Plan.

23 NONSC Living Screens

Prior to commencement of development a scheme for the inclusion of living screens shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall provide details of the types of living material to be used and the locations and

methods of maintenance where necessary. The development should proceed in accordance with the approved plans.

REASON

To ensure the development contributes to a number of objectives in compliance with Policy 5.11 of the London Plan and Policy EM1 of the Local Plan.

INFORMATIVES

1 I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 I53 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

NPPF1	NPPF - Delivering sustainable development
NPPF7	NPPF - Requiring good design
NPPF10	NPPF - Meeting challenge of climate change flooding costal
NPPF11	NPPF - Conserving & enhancing the natural environment
NPPF12	NPPF - Conserving & enhancing the historic environment
LPP 3.2	(2011) Improving health and addressing health inequalities
LPP 3.8	(2011) Housing Choice
LPP 3.14	(2011) Existing Housing - Efficient use of stock
LPP 3.16	(2011) Protection and enhancement of social infrastructure
LPP 3.17	(2011) Health and social care facilities
LPP 4.12	(2011) Improving opportunities for all
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.3	(2011) Sustainable design and construction
LPP 5.6	(2011) Decentralised Energy in Development Proposals
LPP 5.7	(2011) Renewable energy
LPP 5.9	(2011) Overheating and cooling
LPP 5.10	(2011) Urban Greening
LPP 5.11	(2011) Green roofs and development site environs
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.15	(2011) Water use and supplies
LPP 6.3	(2011) Assessing effects of development on transport capacity
LPP 6.5	(2011) Funding Crossrail and other strategically important transport infrastructure
LPP 6.9	(2011) Cycling
LPP 6.10	(2011) Walking
LPP 6.13	(2011) Parking
LPP 7.1	(2011) Building London's neighbourhoods and communities

LPP 7.2	(2011) An inclusive environment
LPP 7.3	(2011) Designing out crime
LPP 7.4	(2011) Local character
LPP 7.5	(2011) Public realm
LPP 7.6	(2011) Architecture
LPP 7.8	(2011) Heritage assets and archaeology
LPP 7.13	(2011) Safety, security and resilience to emergency
LPP 7.14	(2011) Improving air quality
LPP 7.15	(2011) Reducing noise and enhancing soundscapes
LPP 7.19	(2011) Biodiversity and access to nature
LPP 7.21	(2011) Trees and woodland
EC2	Nature conservation considerations and ecological assessments
EC5	Retention of ecological features and creation of new habitats
BE4	New development within or on the fringes of conservation areas
BE13	New development must harmonise with the existing street scene.
BE18	Design considerations - pedestrian security and safety
BE19	New development must improve or complement the character of the area.
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE22	Residential extensions/buildings of two or more storeys.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
OE1	Protection of the character and amenities of surrounding properties and the local area
OE3	Buildings or uses likely to cause noise annoyance - mitigation measures
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
H3	Loss and replacement of residential accommodation
H10	Proposals for hostels or other accommodation for people in need of care
R17	Use of planning obligations to supplement the provision of recreation leisure and community facilities
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
AM13	AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): - (i) Dial-a-ride and mobility bus services (ii) Shopmobility schemes (iii) Convenient parking spaces (iv) Design of road, footway, parking and pedestrian and street

	furniture schemes
AM14	New development and car parking standards.
AM15	Provision of reserved parking spaces for disabled persons
HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006
LDF-AH	Accessible Hillingdon , Local Development Framework, Supplementary Planning Document, adopted January 2010
SPD-NO	Noise Supplementary Planning Document, adopted April 2006
SPD-PO	Planning Obligations Supplementary Planning Document, adopted July 2008
SPG-AQ	Air Quality Supplementary Planning Guidance, adopted May 2002
SPG-CS	Community Safety by Design, Supplementary Planning Guidance, adopted July 2004

3 13 Building Regulations - Demolition and Building Works

Your attention is drawn to the need to comply with the relevant provisions of the Building Regulations, the Building Acts and other related legislation. These cover such works as - the demolition of existing buildings, the erection of a new building or structure, the extension or alteration to a building, change of use of buildings, installation of services, underpinning works, and fire safety/means of escape works. Notice of intention to demolish existing buildings must be given to the Council's Building Control Service at least 6 weeks before work starts. A completed application form together with detailed plans must be submitted for approval before any building work is commenced. For further information and advice, contact - Residents Services, Building Control, 3N/01 Civic Centre, Uxbridge (Telephone 01895 250804 / 805 / 808).

4 115 Control of Environmental Nuisance from Construction Work

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

5 128 Food Hygiene

The Council's Commercial Premises Section should be consulted prior to the use of the

premises so as to ensure compliance with the Food Safety Registration Regulations 1990, Hygiene (General) Regulations 1970, The Food Act 1984, The Health and Safety at Work Act 1974 and any other relevant legislation. Contact: - Commercial Premises Section, 4W/04, Civic Centre, High Street, Uxbridge, UB8 1UW (Telephone 01895 250190).

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The applicant is reminded of the duties set out in the Disability Discrimination Act 1995, with regard to employment and service provision. Whilst an employer's duty to make reasonable adjustment is owed to an individual employee or job applicant, the responsibility of service providers is to disabled people at large, and the duty is anticipatory. The failure to take reasonable steps at this stage to facilitate access will therefore count against the service provider, if/when challenged by a disabled person. It is therefore recommended that the applicant takes full advantage of the opportunity that this development offers, to improve the accessibility of the premises to people with mobility and sensory impairments.

3. CONSIDERATIONS

3.1 Site and Locality

The 0.9ha site is located on the north western side of Long Lane, some 40m to the north east of its junction with Swakeleys Drive. The site comprises 5 detached residential houses on large plots, 4 of which front Long Lane, albeit set well back from the road (Nos. 88 - 94) and have relatively narrow long rear gardens. A fifth house known as Dormy House is a backland development which extends along the rear of the Long Lane properties and is accessed, together with an adjoining property known as No. 7A, from Court Road via a private single width access located between Nos. 5A and 7 Court Road. The five properties are of mixed style and size, although they are all characterised by their mature gardens which include a number of fine trees, some of which are clearly visible from surrounding roads and contribute to the sylvan character of the area.

The site forms part of an established residential area to the north of the North Hillingdon Local Centre which is served by Hillingdon Underground Station on the Metropolitan and Piccadilly Underground lines, which Long Lane crosses, together with the A40 some 240m to the south of the site. Immediately adjoining the site to the south and east is predominantly two storey detached and semi-detached housing, with similar properties also fronting the opposite side of Long Lane to the east. To the north, the site is adjoined by the Douay Martyrs School, which contains a locally listed building.

The site is located on the south eastern side of the Ickenham Village Conservation Area of which it forms a part, with the boundary in this vicinity running along the opposite side of Long Lane. The curtilage of Dormy House is covered by TPO 438 with part of the adjoining rear garden of No. 90 Long Lane also being covered by TPO 482a.

3.2 Proposed Scheme

The proposal is for the demolition of the five detached houses on site to enable its re-development as a mainly part two storey, part three storey 5,861 sq.m. registered Class C2 care home for the frail elderly with associated access, parking and landscaping. The care home would provide 85 bedrooms/suites and have a maximum capacity for 89 residents. The proposed care home would be set back from its boundaries and have an H-

shaped footprint with an extended wing in the north-western corner at the rear of the site. The upper floor of the building would largely be contained within the roof space so that it would appear on its primary frontage onto Long Lane as 1.5 to 2 storeys in height, rising to 2.5 storeys in the centre of the site, before reducing to 2 storeys on the north-western wing.

The central section of the building would contain mainly communal space over two floors, including the building's main entrance on its northern elevation, restaurant, cafe, lounges, library, activity spaces, cinema, hair salon and spa bathing and therapy space with the side wings mainly containing residential accommodation. There would be a mix of unit types, including 12 studio and 17 one-bedroom units on the ground floor, 13 studio and 17 one-bedroom units on the first floor. The top floor within the central roof space of the building would provide a 24 bed dementia wing, although it is likely that most residents would have some degree of dementia. A small basement would be provided in the north eastern corner of the building.

The design of the building incorporates articulated and varied elevations and traditional hipped and gabled roofs. The scale and massing of the building responds to its context and relationship with neighbouring properties, with the taller two and a half storey central wing being sited towards the centre of the site. The bulk and mass of the roof is further broken up with dormers of varied sizes and mixed design. The main roofs are steeply pitched, reflecting the 'arts and craft' style prevalent within the surrounding area, with roof elements nearer the ground at a lower pitch to create the traditional 'swept' character. The Long Lane frontage is particularly important and here the building has been set back to replicate the siting of the existing houses and designed to reflect that of a similar terraced block further along Long Lane.

The building would be set within landscaped grounds, involving the retention of many existing trees and new planting. A series of enclosed garden and courtyard areas around the building would be created, mainly along the southern boundary which have been designed to have their own character and individual recreation/activity space.

The new access road that would run along the northern side of the site to access the parking/ servicing area adjacent to the main entrance (comprising 10 spaces, including 2 disabled spaces) and the larger car park at the rear (comprising 30 spaces, including 7 tandem spaces). The proposal involves the stopping up of the existing vehicular crossovers on Long Lane and the construction of a new vehicular access along the northern side of the site from Long Lane, adjoining the Douay Martyrs School.

The care home would provide a 24 hour operation with typically three shifts for care staff, comprising a morning shift which would commence between 06:00 and 08:00, an afternoon shift which would commence around mid-afternoon and a night shift commencing between 20:00 and 23:00. Ancillary staff such as kitchen and housekeeping personnel can also work outside normal hours with start and end times of around 07:00 and 14:00-15:00 respectively. Some administration as well as management staff would work normal daytime hours of around 09:00-17:00.

The scheme has formed the subject of two previous pre-application enquiries with officers.

The application is supported by the following documents:-

Design and Access Statement:-

This provides a statement summary and describes the applicant, Signature Senior

Lifestyle Ltd. and its operating ethos. The site and its local context is described and a site analysis is produced. The statement goes on to outline the development and reasoning behind the design approach taken, including the results of meetings held with Council officers and the public. The landscape proposals are described, including the design principles adopted and the analysis that led to its design. Access considerations are then discussed, together with sustainability and energy issues.

Planning Statement:

This provides an introduction to the proposals, describes the site and notes that there is no relevant planning history. It goes on to describe the proposals and summarises the pre-application discussions that have taken place with Council officers and the local community. Relevant planning policy is described and the proposals and their impacts are assessed against the development plan. The statement concludes by stating that the scheme is acceptable in terms of the identified impacts and the relevant policies of the development plan and in particular the loss of 5 dwellings as 85 Class C2 suites would be provided as a replacement which the NPPG advises should be considered as a form of housing. If this is not accepted, there remain compelling material considerations to grant permission, namely there is a demonstrable need for the development, there would be an associated continual release of family housing, the overall quantum of care home units provided and the significant economic benefits of the scheme.

Heritage Statement:

This provides an introduction to the report and assesses the statutory constraints affecting the site. The history of the site and its surroundings are described and the report then provides an assessment of significance of the heritage assets present within the area (with the exception of below ground archaeology). The proposed development is described and the impacts of the development on the heritage assets identified as being of significance, namely the Ickenham Village Conservation Area and the setting of the locally listed buildings at the Douay Martyrs School which are then assessed. The report concludes that through careful design, the proposed development will make a positive contribution to the Ickenham Conservation Area and the setting of the nearby locally listed building and therefore accords with national and local planning policy and legislation.

Townscape and Visual Assessment:

This provides an introduction to the study and outlines relevant national and local planning policy. The site and its setting is described and the townscape character defined and a photographic visual assessment presented. The development proposals are described and their impact on the townscape character and the wider visual environment are assessed. The report concludes that given the existing character of the townscape and the high degree of visual containment on the site, restricting medium and longer distance views, together with the design of the scheme which breaks up the building's massing, the proposed care home building can be accommodated on site without resulting in any unacceptable change to the overall character of the townscape and therefore complies with policy.

Transport Statement:

This provides an introduction to the proposals and describes the existing transport network. The existing site is described, results of a traffic survey are presented and accident records analysed. Relevant national, strategic and local planning policy and guidance is

assessed and details of pre-application highway advice noted. The development proposals are then described as they relate to highway matters. Results of the traffic appraisal are presented, including predicted traffic generation using the TRICS database and conclusions are presented.

Economic Benefits Assessment:

This provides an introduction to the study and defines the study area. Relevant national, regional and local planning policies and guidance as it relates to housing is assessed. The statement goes on to consider the socio economic context of the site, advising that the study area has an ageing population which will continue to grow, the ready availability of the local labour force would support the construction and operational phases of the development, economic benefits of £12.5m over the anticipated construction phase and £2m per annum during operational phase with some 90 full and part time employment positions would accrue to the local economy, savings to the NHS of approximately £29,500 per annum and with a large proportion of residents aged 65+ within the study area living within under occupied houses, the proposal offers the opportunity to downsize, freeing up housing stock, potentially at a rate of approximately 30 houses per annum.

Arboricultural Report:

This provides the background to the report and describes its methodology. The report goes on to assess the existing trees in terms of their quality and value, the impact of the proposed development upon the trees in and around the site, methods of reducing the impacts on retained trees, proposed tree protection measures and the proposed tree planting strategy.

Landscape Design:

This document provides additional supporting information for the landscape strategy and masterplan. There are sections on the design principles, site analysis, constraints and opportunities, communal garden design, boundary treatments and provides an indicative planting palette, plan and sections.

Ecological Assessment:

This provides the background to the study, describes the site and the survey and evaluation methodology which includes a desktop study, initial habitat and faunal surveys, including building surveys, tree inspections on and close to the site and limited further dusk and dawn surveys. Results are presented and recommendations are made. The report concludes that based on the evidence obtained from the detailed survey work and with the implementation of the recommendations set out in the report, there is no ecological reason to suggest that any ecological designations, habitats of nature conservation interest or any protected species would be significantly harmed by the proposals and that a number of net gains for biodiversity should be delivered by the proposed development.

Care Home Needs Assessment:

This provides an introduction to the study and assesses general population trends and advises of the implications for dementia care. Recent legislative changes are assessed and key issues for the care home sector are identified. The report goes on to consider the type of accommodation proposed and how this would compare to the local market. The methodology for assessing need for elderly care is described and results are presented.

Acoustic Report:

This provides the background to the assessment, including an ambient noise level survey carried out between 30/5/14 and 1/6/14. The site and methodology are described. Internal and external noise criteria are assessed and results of the survey are presented and discussed. Minimum construction attenuation values are established which mainly relate to the glazing/ ventilation specifications and noise design criteria set for proposed external plant. The report concludes by stating that providing the mitigation measures to the building facade and noise criteria followed for the plant and equipment, the scheme would comply with relevant standards and the NPPF allowing planning permission to be granted.

Energy Report:

The report sets out the background and the policy framework to the report and advises of the measures that would be used and assesses the feasibility of a range of energy technologies following the three stages of the London Plan energy hierarchy. Recommendations are made, including the preferred option of a gas fired Combined Heat and Power Unit and either a Ground Source Heat Pump or Biofuel/Dual Fuel Condensing Boiler system.

Construction Method Statement:

This sets out the management approach to construction activities on site and details the processes and procedures that will be followed.

Interim Travel Plan:

This provides possible measures to reduce reliance on the private car.

Air Quality Assessment:

The assessment sets out the background and scope of the study and reviews relevant planning policy and air quality legislation and guidance. The report then assesses indicative background pollution concentrations and models predicted pollution levels. The report evaluates the potential impacts of the emissions from the two options being considered for the proposed heating plant and development traffic on ambient air quality and assesses future air quality in terms of whether this constrains the proposed development of the site. The report concludes that the proposal will be compliant with all relevant national and local planning policy and air quality legislation and that air quality considerations will not impact on the development or occupation of the site.

Statement of Community Engagement:

This sets out Hillingdon's guidance for prospective developers on community involvement, details the consultation undertaken by the applicant, including a public exhibition over 2 days at the Ickenham United Reform Church, feedback mechanisms, public comments received and the applicant's response.

Letter from Carterwood dated 20/8/14:

This provides further clarification on the assessment of existing care home bedspaces.

3.3 Relevant Planning History

Comment on Relevant Planning History

There is no planning history on this site of relevance to the proposed development with the only applications submitted relating to extensions and alterations of the existing houses.

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

PT1.E7	(2012) Raising Skills
PT1.HE1	(2012) Heritage
PT1.BE1	(2012) Built Environment
PT1.EM1	(2012) Climate Change Adaptation and Mitigation
PT1.EM6	(2012) Flood Risk Management
PT1.EM7	(2012) Biodiversity and Geological Conservation
PT1.EM8	(2012) Land, Water, Air and Noise
PT1.T1	(2012) Accessible Local Destinations
PT1.CI1	(2012) Community Infrastructure Provision

Part 2 Policies:

NPPF1	NPPF - Delivering sustainable development
NPPF7	NPPF - Requiring good design
NPPF10	NPPF - Meeting challenge of climate change flooding coastal
NPPF11	NPPF - Conserving & enhancing the natural environment
NPPF12	NPPF - Conserving & enhancing the historic environment
LPP 3.2	(2011) Improving health and addressing health inequalities
LPP 3.8	(2011) Housing Choice
LPP 3.14	(2011) Existing Housing - Efficient use of stock
LPP 3.16	(2011) Protection and enhancement of social infrastructure
LPP 3.17	(2011) Health and social care facilities
LPP 4.12	(2011) Improving opportunities for all
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.3	(2011) Sustainable design and construction
LPP 5.6	(2011) Decentralised Energy in Development Proposals
LPP 5.7	(2011) Renewable energy
LPP 5.9	(2011) Overheating and cooling

LPP 5.10	(2011) Urban Greening
LPP 5.11	(2011) Green roofs and development site environs
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.15	(2011) Water use and supplies
LPP 6.3	(2011) Assessing effects of development on transport capacity
LPP 6.5	(2011) Funding Crossrail and other strategically important transport infrastructure
LPP 6.9	(2011) Cycling
LPP 6.10	(2011) Walking
LPP 6.13	(2011) Parking
LPP 7.1	(2011) Building London's neighbourhoods and communities
LPP 7.2	(2011) An inclusive environment
LPP 7.3	(2011) Designing out crime
LPP 7.4	(2011) Local character
LPP 7.5	(2011) Public realm
LPP 7.6	(2011) Architecture
LPP 7.8	(2011) Heritage assets and archaeology
LPP 7.13	(2011) Safety, security and resilience to emergency
LPP 7.14	(2011) Improving air quality
LPP 7.15	(2011) Reducing noise and enhancing soundscapes
LPP 7.19	(2011) Biodiversity and access to nature
LPP 7.21	(2011) Trees and woodland
EC2	Nature conservation considerations and ecological assessments
EC5	Retention of ecological features and creation of new habitats
BE4	New development within or on the fringes of conservation areas
BE13	New development must harmonise with the existing street scene.
BE18	Design considerations - pedestrian security and safety
BE19	New development must improve or complement the character of the area.
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE22	Residential extensions/buildings of two or more storeys.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
OE1	Protection of the character and amenities of surrounding properties and the local area
OE3	Buildings or uses likely to cause noise annoyance - mitigation measures

OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
H3	Loss and replacement of residential accommodation
H10	Proposals for hostels or other accommodation for people in need of care
R17	Use of planning obligations to supplement the provision of recreation, leisure and community facilities
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
AM13	AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): - (i) Dial-a-ride and mobility bus services (ii) Shopmobility schemes (iii) Convenient parking spaces (iv) Design of road, footway, parking and pedestrian and street furniture schemes
AM14	New development and car parking standards.
AM15	Provision of reserved parking spaces for disabled persons
HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006
LDF-AH	Accessible Hillingdon , Local Development Framework, Supplementary Planning Document, adopted January 2010
SPD-NO	Noise Supplementary Planning Document, adopted April 2006
SPD-PO	Planning Obligations Supplementary Planning Document, adopted July 2008
SPG-AQ	Air Quality Supplementary Planning Guidance, adopted May 2002
SPG-CS	Community Safety by Design, Supplementary Planning Guidance, adopted July 2004

5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- **9th October 2014**

5.2 Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

58 neighbouring properties have been consulted, the application has been advertised in the local press on 24/9/14 and a site notice dated 18/9/14 has been displayed on site. A further 14 day consultation period has also been undertaken on 18/2/15 to the revised plans and elevations received on 20/1/15 and the revised access arrangements on Long Lane received on 14/2/15. To date, 3 petitions, objecting to the proposals have been received, together with 17 individual responses, 16 objecting to the proposals and 1 in general support.

First petition with 20 signatories states:-

'I object to the planning application reference 52129/APP/2014/2996 on the grounds of the visual impairment of the overdevelopment of the site in a Conservation Area. Additionally the impact of the highway in relation to safety, disturbance and noise.'

Second petition with 29 signatories states:-

'I object to the planning application reference 52129/APP/2014/2996 on grounds of visual impairment of the over development of the site in a Conservation Area.

Additionally the impact of the highway in relation to safety, disturbance and the noise.

It also seems like an act of vandalism to wilfully destroy five existing properties, which will totally alter the outlook of our locality.'

Third petition with 37 signatories states:-

'We, the undersigned, fully support the Ickenham Residents' Association in their objective of ensuring that the wishes of its members as well as the wishes of the people of Ickenham in general, are heard by the London Borough of Hillingdon's North Planning Committee.'

Objection comments:-

Impact on Conservation Area/Character of Area

- (i) A large building of this scale and use will be detrimental to the Conservation Area, out of keeping with its character,
- (ii) Proposal does not respect the local context or street pattern and in particular, the scale and proportions of surrounding buildings. This represents overdevelopment of the site and a huge change to the local area with its excessive height, footprint and density, detrimental to the local environment and contrary to policy which seeks good design and respect for the character of the surroundings and regard to the spaces between buildings,
- (iii) Proposal is unsuitable within the middle of a residential area - the area is currently characterised by detached/semi-detached properties, not the very large building that is proposed, (iv) This is another corporate residence being built that will reduce the sense of community in the area,
- (v) Approval would only leave three family houses on this stretch of road,
- (vi) The 5 houses that would be demolished could house around 25 people between them, whereas the proposal could hold around 125 people with staff, visitors etc, a fivefold increase whereas there is a need to avoid town cramming,
- (vii) There will be a huge loss of green garden space to building,
- (viii) Proposal will result in loss of trees and wildlife. There are protected trees at Nos. 90 and 96 Long Lane and Dormy House and extent of excavation with heavy excavation equipment could impact on root system of these trees,

Impact on Residential Amenity

- (ix) The size of the building will be overbearing with greater height and extent of building,
- (x) A two/three storey building and planting of large trees along our rear boundary in Swakeleys Drive which is only 40ft from our back door will block our sunlight, effect our garden shrubbery with falling branches damaging fencing and mar the skyline, destroying the pleasant open outlook, a main reason for buying the bungalow,
- (xi) Adjoining residential properties will be overlooked by proposed building, particularly No. 2b Swakeleys Drive and bathroom and rear bedrooms at No. 96 Long Lane, resulting in a loss of privacy, contrary to policy,
- (xii) Proposal will adversely affect view from No. 96 Long Lane,
- (xiii) Proposal will affect quality of life for residents and right to quiet enjoyment of their properties and gardens, contrary to Human Rights Act

- (xiv) Residents already have a school, another care home and soon to have a Tesco by Hillingdon Station to contend with. The noise and pollution are already at extreme levels,
- (xv) Proposal will significantly change our outlook to that of a large commercial property with reduced greenery,
- (xvi) Proposal will result in light pollution from the property and its parking and service areas, detrimental to ambiance of the area,
- (xvii) There would be additional noise at all hours of the day and night, 7 days a week from staff, delivery lorries, emergency vehicles, tradesmen, doctors, etc, particularly from the access and the car park which is too close to garden boundaries in Court Road. Delivery/collection hours of parts, materials and waste need to be acceptable,
- (xviii) How will waste be handled, controlled and stored?
- (xix) Construction work would involve much noise and mess, involving reversing heavy vehicles at unsocial hours,
- (xx) Tree planting on boundary will eventually damage foundations to our property,

Traffic Issues

- (xxi) Proposed access directly opposite our property could exacerbate existing problems when accessing and exiting our property due to impaired visibility through parked vehicles, speed of traffic and traffic queuing, causing potential risk and disruption,
- (xxii) This part of Long Lane is an accident 'hot-spot' with 2 recent accidents, one of which was fatal and known near misses which LBH has already had to implement traffic calming. Traffic increase will exacerbate this,
- (xxiii) Increased vehicles accessing and exiting the site could cause a major risk to the school children. At a minimum, the bus stop should be re-sited,
- (xxiv) Long Lane and surrounding areas are already extremely busy and congested. With proposed space for 40 vehicles, there will be a large increase in traffic and footfall along Long Lane which will exacerbate congestion from early morning to late evening, particularly during commuter and school rush hours,
- (xxv) With up to 90 part time staff, there would be a lack of on-site parking particularly at staff arrival/departure times, which will spill onto surrounding already heavily parked streets, causing more congestion around the school and Long Lane, no matter how and when staff start/depart work which will be prejudicial to road safety, particularly involving school children. Consideration should be given to a residents parking scheme,
- (xxvi) There will be no access from Court Road but developer may retain rights to use Court Road access which could cause problems in the future,

Other

- (xxvii) Demolition of 5 houses at a time when houses are in short supply will add to pressure to over-develop other sites as unlikely that the care home will free up any existing local housing,
- (xxviii) Proposal will exacerbate existing flooding and drainage problems - particularly in Court Road which is lower than the application site and is an area that easily floods, even after a small amount of rainfall. Flooding could also be exacerbated with removal of mature trees,
- (xxix) Air extraction system will disperse waste air into surrounding environment,
- (xxx) There are already other care homes in the area (11 within a couple of miles) so this development is purely commercial and of no benefit to the local area,
- (xxxi) If scheme is built and fails, a large site would be left for potential uses that might be undesirable,
- (xxxii) Proposal will reduce property values
- (xxxiii) Concerned about the possibility of additional or larger power cables passing over our property,
- (xxxiv) There is the possibility of drainage issues resulting in flooding of our rear garden,
- (xxxv) The Council should listen to its residents and put their interests first and not that of developers who have no consideration for Ickenham,

Comments in Support:-

- (i) I am in favour of this application which would provide a much needed asset to the local area providing there is no access to the development by vehicles or pedestrians from Court Road, even emergency services which should be conditioned and also that boundary fences be erected before the start of works so no access will be allowed from here and car park sited far enough away from our property and sound proof fence provided to car park,
- (ii) The current access to Dormy House being closed is welcomed,

GLA:

London plan policies on elderly care home provision, design, energy and transport are relevant to this application. Notwithstanding that the application complies with the London Plan, it would be improved by the following changes, which could possibly lead to the application becoming compliant with the London Plan:

- Care home provision: The provision of a care home counts towards the borough's housing target, resulting in a net gain. There is an identified pan London need for such facilities, as well as an acknowledged growing demand. The Council should consider whether affordable housing policies should be applied.
- Energy and sustainability: The carbon emissions and savings at each step of the energy hierarchy should be calculated once the matters relating to the size of the CHP unit proposed and renewable energy options have been reviewed by the applicant.
- Transport: The recommendations in the report with regards to car and cycle parking, servicing and construction, travel plan and CIL contribution should be considered before the report is referred back at Stage 2.

Greater London Archaeological Advisory Service:

Recommend Pre-Determination Archaeological Assessment/Evaluation

The National Planning Policy Framework (Section 12) and the London Plan (2011 Policy 7.8) emphasise that the conservation of archaeological interest is a material consideration in the planning process. Paragraph 128 of the NPPF says that applicants should be required to submit appropriate desk-based assessments, and where appropriate undertake field evaluation, to describe the significance of heritage assets and how they would be affected by the proposed development. This information should be supplied to inform the planning decision.

Appraisal of this planning application using the Greater London Historic Environment Record and information submitted with the application indicates a need for further information to reach an informed judgment of its impact on heritage assets of archaeological interest.

Although not within an Archaeological Priority Area this is a major development on a site which is currently relatively non-intensively developed giving good potential for preservation of buried archaeological remains within the green open spaces. In 1994 an archaeological excavation opposite this site at Long Lane Playing Fields recorded part of a Roman field system suggesting that a farm or settlement lay nearby. It is therefore quite likely that associated remains (and perhaps the settlement itself) could be present within the application site and this should be established before a decision is made which could lead to the loss of a heritage asset.

I therefore recommend that the following further studies should be undertaken to inform the preparation of proposals and accompany a planning application:

Evaluation

An archaeological field evaluation involves exploratory fieldwork to determine if significant remains are present on a site and if so to define their character, extent, quality and preservation. Field evaluation may involve one or more techniques depending on the nature of the site and its archaeological potential. It will normally include excavation of trial trenches. A field evaluation report will usually be used to inform a planning decision (pre-determination evaluation) but can also be required by condition to refine a mitigation strategy after permission has been granted.

I recommend trial trench evaluation as in this case the most rapid and reliable means of establishing the presence or absence of a heritage asset of archaeological interest.

The nature and scope of assessment and evaluation should be agreed with GLAAS and carried out by a developer appointed archaeological practice before any decision on the planning application is taken. The ensuing archaeological report will need to establish the significance of the site and the impact of the proposed development. Once the archaeological impact of the proposal has been defined a recommendation will be made by GLAAS.

The NPPF accords great weight to the conservation of designated heritage assets and also non-designated heritage assets of equivalent interest. Heritage assets of local or regional significance may also be considered worthy of conservation.

If archaeological safeguards do prove necessary, these could involve design measures to preserve remains in situ or where that is not feasible archaeological investigation prior to development. If a planning decision is to be taken without the provision of sufficient archaeological information then we recommend that the failure of the applicant to provide adequate archaeological information be cited as a reason for refusal.

Ickenham Residents' Association:

This Association objects strongly to the above proposal.

We do however recognise the need for the provision of such facilities, but not at this level of intensity or occupation, nor at this particular site within the Ickenham Conservation Area and as such completely disagree with the conclusion reached in the Planning Statement.

The Association would like to put this proposal into perspective, in that it is over 40% larger in terms of Gross Internal Area than the proposed Tesco store on the Master Brewer site.

Preamble

The Conservation Area at this point comprises of large detached houses with equally large settings. This proposal would alter completely from the foregoing description to one of a very large terraced commercial frontage, totally inconsistent with the existing Conservation Area.

The current footprint of five large detached houses with extensive green areas (gardens) and mature trees surrounding them will very largely be replaced with a massive concrete footprint supporting a highly dense form of accommodation.

Recent research of ours, printed in our last Ickenham Calling Newsletter indicated the following: "At the moment, Hillingdon has 64 care homes with 1200 beds, plus nine supported living units with 318 beds. The CCG (Hillingdon Clinical Commissioning Group) proposes to recruit two Community Specialist Nurses to support care homes in caring for residents, to avoid admission to hospital".

This, we feel, indicates there is no particular lack of such provision in Hillingdon, despite the

applicants' claim, and further indicates a possible move from institutional care to that of care in the community, i.e. at home.

One final point in our preamble is that of cost. We have been told that typical pricing for accommodation in this facility would be in the region of £2000 per week - far beyond the means of most people in the local area, even though it is planned to be an 'end of life' facility with an expected stay of probably less than two years.

Planning Issues.

1) Built Environment.

One of the key factors of the Conservation Areas, beyond its design and appearance in respect to the built form or physical features, is its intensity of use to which buildings and land are put. Conservation Areas can be particularly sensitive to change of use proposals, particularly if such proposals were to bring greater traffic generation or an increase in the number of commercial vehicles.

The proposal as presented would completely change the street scene as viewed from Long Lane from one of individual large houses in large spacious plots to a large terraced commercial structure higher, wider, and deeper than the existing properties, and as referred to above will increase traffic generation, and introduce commercial vehicle movements.

With the exception of the Douay Martyrs school building along the northern edge, the remainder of the site is surrounded by substantial detached and semi-detached houses, which currently enjoy the benefits of an open, green environment, consistent with a Conservation Area. This will be replaced with extensive buildings at the end of their gardens (Court Road & Swakeleys Drive) covering the majority of the plot, resulting in a considerable loss of amenity and character.

The proposal can in no way claim to "preserve or enhance the Conservation Area" and the loss of existing houses would, by way of the positive contribution they currently make to the Conservation Area, be detrimental to it.

We therefore feel this application contravenes the following policies of the UDP Saved Policies. Pt 1.10; BE4, BE5, BE13, BE 19, BE21, BE24, and OE1.

2) Flooding.

The area in question already suffers from a high water table with frequent instances of local surface water drainage problems. Occasionally this build up has been known to put such pressure on the local sewerage network as to have instances of sewerage overspill as well.

The introduction of such a large area being placed under concrete (1.2 Hectares), together with the subsequent loss of natural drainage, coupled with the introduction of some 89 (min) accommodation units will, in our view, add considerably to the risk of further instances of flooding.

As indicated there will be a substantial increase in the percentage of the site covered in hard surfacing and whilst we acknowledge there has been some attempt to balance soft and hard landscaping proposals, the applicant has failed to offer any SUDs input which would sufficiently demonstrate that the site in its proposed use would not flood leading to the flooding of neighbouring sites due to the high water table.

We therefore feel this application contravenes the following policies of the UDP Saved Policies. Pt 1.11; OE7, OE8, OE9, and OE10

3) Change of Use.

In relation to Policy R15 of the UDP we have already commented (in our preamble) that this area is,

in our opinion, fairly well covered for the provision of care facilities, so no such need exists.

Further the development is not in sympathy with either the style or character of the existing street scene.

This particular site however has no transport facilities passing the door and shops are a lengthy and arduous walk away. Medical facilities (GPs) are already overstretched in the area.

We therefore feel that this application does not comply with policy R15 and H10.

4) Traffic/Safety

Whilst conceding that this proposal 'may' not generate too much traffic, we feel it has been understated.

It sits on a very contentious piece of road with regular heavy congestion, on a difficult bend, with a traffic Island opposite/adjacent to the proposed vehicular entrance, opposite access to Turnstone Close and not too far distant from a heavily used School Crossing. The residents and neighbours in the vicinity of the houses planned to be demolished comment that the 'difficult bend' referred to above has caused a number of vehicle accidents, particularly speeding cars late at night.

The assumption that traffic impact at a location that can accommodate up to 85 residents, with between 80 and 90 members of staff can be minimised through the introduction of a site travel plan is in our view simplistic. We believe that the staff alone will generate more than the 7 - 10 two way trips detailed within the Travel Plan.

We also have a concern that the survey undertaken on the 18th June 2014 was not carried out at the correct a.m. and p.m. peak times; 0730 - 0830 and 1545 -1645, as opposed to 0745 - 0845 and 1645-1745 as used by the recent surveys provided by Tesco and Morrisons for the Hillingdon Circus applications.

The number of vehicles returned from the survey were also lower by over 100 in each direction than the volumes reported in both the Tesco and Morrisons Transport Assessments and by LBH for the proposed extension to Glebe School.

In addition, the travel plan is silent on any committed development in the area such as the Glebe School extension and the recently approved Tesco store on the former Master Brewer site.

It should be understood that any additional traffic in the Ickenham area will have an impact on the journey time and queue lengths currently experienced by the users of this already congested route.

We are concerned that adequate consideration/provision may not have been provided for 'Loading Arrangements' within the curtilage of the site. An accommodation site of this size and nature will require many 'service vehicle' movements to service the needs of such an institution, e.g. food supplies, medical supplies, recycling, waste disposal, clinical waste disposal, transport for residents. (See General Notes on the use of parking standards (LBH UPD Saved Policies Sept 2007 page 322)).

We therefore contest this application's robustness against Policies AM2 and AM7.

5) Environmental Impacts

We are concerned that, as the site will be operating around the clock, site deliveries and collections will take place outside normal working hours, leading to a noise impact on adjacent properties.

We are also concerned that kitchen waste will attract vermin if not stored and disposed of correctly.

We feel the plans are misleading, suggesting a green and pleasant setting, whereas we know that a number of substantial trees will have to be felled and that some of the green setting on the plans will comprise bin storage and other similar facilities.

6) Summary

In summary we would suggest that the introduction of a purely commercial site and operation within the confines of a Conservation Area, so designated as being 'of large residential properties set in 'spacious grounds' is wholly inappropriate.

For all of the above reasons, and the contravention of many of your planning policies, as indicated, we strongly oppose this application and ask that you refuse planning permission.

Ward councillor:

I note the above planning application and draw some similarities to an application that was refused at the Major Planning Committee for the demolition of two houses and for the construction of five houses with a similar siting, being mainly at the rear of the front remaining property. That site was located opposite to Oak Farm Library and again fronts the same busy Long Lane. Both that site and this site are bounded by bespoke detached houses of some special character, albeit with the Ickenham site, the houses are set back from Long Lane and located in an area that might be classified as having some special character.

This redevelopment to the rear of the existing adjoining properties that will remain will in my opinion detract from the quality of the living environment for the current and future residents. This will also apply to those residents living in Court Road whose rear properties face this proposed development and will also be affected should this development go ahead.

In addition, there will be a loss of good quality building stock and established rear garden fauna, flora and outlook.

Could I therefore request that my concerns are added to the planning report that will come before the planning committee.

Internal Consultees

CONSERVATION/ URBAN DESIGN OFFICER:

Initial comments:

This site lies on the edge of, but within the Ickenham Village Conservation Area. It is located away from the historic core of the conservation area, and where the general character of the area is of good quality, 2 storey, detached and semi detached houses of varied architectural styles and mostly 20th century in date. To the front the site is bounded by Long Lane and the other site boundaries are enclosed by existing residential development.

The site contains 5 existing houses of varied age and appearance. All of the houses contribute to some degree to the appearance of the conservation area and two are of interest. These are number 88 Long Lane, a typical interwar two storey house, with attractive applied decorative timber framing and Dormy House, a modern property. This is probably late 1960s in date and has an unusual green copper clad roof. From Long Lane, the frontage buildings are only seen in gap and glimpsed views and the dominant features of the townscape are the front boundary walls and mature trees. From the other adjacent roads, Swakeleys Drive and Court Road, the existing buildings are largely screened by houses and garden greenery. Dormy House is not visible.

Adjacent to the site is the Locally Listed Douay Martyrs School (Cardinal Hume Campus), this site includes the original early 20th century single storey school building and a number of low level

additional modern buildings. The bulk of these structures is concealed by the abundant tree cover and planting along the Long Lane frontage.

The application includes a well researched and thorough heritage statement, and a detailed site/view analysis. I concur with much of what is included in the heritage statement in terms of the assessment of this part of the conservation area and the heritage value of the existing buildings. Whilst no 88 is of some interest and quite attractive, it appears to be a fairly typical house of its date and type. Dormy House is more interesting and ideally a site visit to view this building needs to be arranged to come to a final decision on this matter. Other than this, there are no objections in principle to the demolition of the other structures if a scheme of an appropriate scale and design were forwarded. If demolition were found to be acceptable, then all the buildings should be recorded to English Heritage Recording Level 1.

The new building would have a significantly larger footprint than the houses it replaces. However, this is comparable with the neighbouring traditional school buildings, which although large, because they are single storey, do not look out of place within the local townscape. The articulated layout of the proposed building would break up its overall bulk up into smaller elements, its set back from the site frontage and boundaries, and partial screening by existing and new planting, would in general combine to reduce its potential impact on the immediate townscape. There are, however, concerns re the bulk and height of the core of the building, which is likely to be visible in views from Swakeleys Road across the back of the remaining Long Lane houses to the south and over the bungalows 2a and 2b Swakeleys Road (view point 10 of the Visual Analysis covers part of this). It appears that it may also be visible in views across the garden areas on the junction of Court Road and Swakeleys Drive (view point 12 does not show this fully). In addition, I am concerned that there would be an oblique view that exposes the bulk of the building across the frontages of 96 and 96a Long Lane (view point 7), even with the trees in full leaf. This is likely to be more visible during the winter.

The proposed building, in particular along the Long Lane frontage, has been very carefully designed to reflect the architecture and materials of some of the surrounding traditionally designed "Arts and Crafts" style houses. Thus the street elevation incorporates gables, a traditional clay tiled pitched roof with low eaves, dormers and chimneys. The rear elements are more of a mix of traditional and modern features, some parts of which appear to work better in design terms than others. The quite extensive areas of flat roof, the steep pitch and height of the core of the building (elevation 3) are of great concern, and the rather boxy gable end (elevation 10), whilst disguised to small extent by tile hanging, fails to conceal the true size of building beyond. Elevation 11, incorporates two levels of dormers, which is not a traditional feature of the area. In addition, some of the modern dormers/gables look rather over sized and a little heavy in design terms, in particular those forming part of elevations 1 and 7.

Whilst there would be no objection in principle to this application, there are still significant issues to be addressed re the demolition of Dormy House, the bulk and height of the core of the new building and elements of its design, such as its roof form. In addition, further information is required re the landscaping scheme in terms of successfully screening the building from the view points noted above. Revised drawings and additional information required.

Latest comments:

I confirm that I visited the site again today. Whilst no 88 is an attractive and well detailed property, it has a plain interior. Court Road also contains at least 3 quite similar dormer bungalows, one of which looks it might be by the same architect. It can therefore be recorded to EH Level 1, rather than level 3 as previously requested.

I have been back through the GLA's advice and we need to add a further condition re the detailing of the eaves, the roof hips and verges of the new building.

Just to confirm, I have no objection to the demolition of the other properties on the site including Dormy House, which other than its copper clad roof, appears to have little architectural interest. The remaining properties are modern, or in the case of the older houses, significantly altered, so that they contribute little to the quality of the townscape of this part of the conservation area.

HIGHWAY ENGINEER:

Original comments:

The site is located along the western side of Long Lane, which is a Classified Road and is also designated as a Local Distributor Road within the Council's Local Plan Part 2.

The site has a PTAL rating of 2.

The proposed care home will have a new access along Long Lane. The existing central hatching on Long Lane will be amended to provide turning space for vehicles turning right in to the site and the dwellings on the opposite side.

The applicant should revise the existing central hatching on Long Lane to provide a clear space for right turning vehicles and the access should also be designed to adequately accommodate turning circles for 10.5m long refuse/service with 0.3m margin for error without vehicles entering or overhanging the right turning lane. The access should be designed with tactile paving on both sides. Revised swept paths should be provided on plans showing the access and road markings.

As part of the highway works, the redundant accesses in front of the site will be stopped up and crossovers reinstated.

The personal injury accident data referred to in the Transport Statement should be provided together with stick diagram.

When considering the estimated trip generation and car parking, it is noted that a number of chosen sample sites are over 5 years old, which is not acceptable. Revised trip generation information should be provided based on up-to-date surveys and an explanation should be provided comparing the sample sites with the application site.

The refuse and waste store is located adjacent to the proposed vehicle access. This (the refuse and waste store) should be relocated to be within the site and the proposed site layout should provide adequate turning spaces for 10.5m refuse vehicles.

Further comments will be made upon receiving the revised/additional information.

Further comments:

The site is located along the western side of Long Lane, which is a Classified Road and is also designated as a Local Distributor Road within the Council's Local Plan Part 2.

The site has a PTAL rating of 2, which is categorised as low.

Further to my comments on 19th of November 2014, additional information has been submitted including revised access layout, road markings, swept paths, accident data, trip generation, and relocated refuse and waste store.

Two proposals are submitted for the layout of the proposed access. The proposed layout shown on drawing nos. 2595.08 and 2595.T09 is considered to be acceptable. As part of the proposed vehicle access works, the existing accesses, which will be redundant in the future, will need to be stopped up and the footway reinstated.

The existing central hatching on Long Lane is proposed to be amended and a right turn arrow will be added to provide turning space for vehicles turning right into the site. However, the houses on the opposite side are served with existing vehicle accesses. As such, the proposed changes to the central hatching should be amended to ensure that right turning into the existing accesses for these houses is not compromised.

The proposed access and changes to road markings should also include short lengths of double yellow lines to restrict on-street car parking in the vicinity of the proposed access, as required.

A record of personal injury accidents is provided for a period of 5 years upto 30th June 2013. An analysis of the collisions is however not provided. Notwithstanding this, the records do not show a high number of collisions in the vicinity of the site. Overall the pattern of collisions is not found to have any common issues in connection with road layout and/or vehicle speeds.

Revised estimates of trip generation are submitted. However these are still neither comparable nor robust. However, considering the limited sites available in the TRICS database and the proposed use being a care home and the scale of development, the proposals are not considered to result in significant vehicle movements.

The internal site layout including access road, lay-by, car parking, cycle parking, servicing, and relocated refuse and waste store is considered to be acceptable.

Subject to receiving a satisfactory revised layout of the central hatch markings and parking restrictions as discussed above, there is no objection on the highways aspect of the proposals. The proposed highway works are recommended to be covered within the S106 agreement. Adequate pedestrian visibility splays of 2.4m x 2.4m at the proposed access should be secured by way of a suitable planning condition.

Latest additional comments:

Following the previous highways comments, a revised drawing ref. 2595.08B has been received showing the the proposed off-site highway works.

The proposed off-site highway works broadly include amendments to the central hatching on Long Lane to facilitate the vehicles turning right to access the site and also the vehicles turning right to access the houses situated on the opposite side of the road, access works, tactile paving, short lengths of double yellow lines on Long Lane, closure of existing redundant accesses and reinstatement of footway, and other associated associated works as required. The proposed double yellow lines in proximity of the proposed access are required to facilitate service vehicle swept paths and to prohibit vehicles from parking closed to this access.

Subject to the above off-site highway works being secured through a S106 agreement and a condition attached securing 2.4mx2.4m pedestrian visibility splays at the point of vehicle access, no objection is raised from the highways point of view.

TREE AND LANDSCAPE OFFICER:

Landscape Character / Context:

Site description:

- The site is occupied by five individual detached houses situated within generous plots. Four of the houses front onto Long Lane, and have relatively narrow long rear gardens. The fifth, Dormy House, is a backland development which extends across the rear of the Long Lane properties, which is accessed from Court Road.
- These properties are of mixed styles and sizes. However, they are all set within well-established gardens which include some fine trees, some of which are clearly visible to the public and contribute

to the sylvan character of the area.

Landscape Planning designations:

- Selected trees are protected by Tree Preservation Order No.438, in the garden of Dormy House.
- TPO No. 482A protects three trees on the rear boundary of 90 Long Lane.
- Offsite, there are two protected trees on the front boundary of 96 Long Lane (TPO No. 496).
- The site lies within the Ickenham Village Conservation Area, a designation which affords protection to the remaining non-TPO'd trees.

Landscape constraints / opportunities:

- The tree cover on this, and adjacent, sites, make a significant contribution to the landscape and visual quality of the area.
- The trees also contribute to the environmental quality of the area (including air quality and biodiversity).
- Any proposed tree loss needs to be justified and suitable new replacement tree planting should be incorporated within a high quality landscape scheme to improve the age profile and succession of trees on the site.

Proposal:

The proposal is to demolish the existing five houses and redevelop the site for an 85 unit Class C2 care home for the elderly of 1.5-2.5 storeys in height with associated landscaping and car parking (40 spaces in total), stopping up of existing vehicular accesses on Long Lane and the construction of a new vehicular access onto Long Lane.

Landscape Considerations:

Saved policy BE38 seeks the retention and utilisation of topographical and landscape features of merit and the provision of new planting and landscaping wherever it is appropriate.

- The submission includes an Arboricultural Report by Tim Moya Associates, which includes an assessment of the existing trees (their quality and value), the impact of the proposed development upon the trees in and around the site, methods of reducing the impacts on retained trees, proposed tree protection measures and the proposed tree planting strategy. - All in accordance with the methodology set out in BS5837:2012 and several relevant Building Research Establishment documents (1998, 2006 and 2011).
- The survey information has been used, as intended, to inform the site layout and landscape design.
- The survey (4.3) has assessed 122No. individual trees, groups and shrubs. One of these trees is considered to be 'A' grade of high quality and value, with 14No.'B' grade (moderate), 89No. 'C' (low) and 18No. 'U' (poor).
- In Figure 1, Age Profile of Canopy Cover, the graph explains that most of the trees surveyed are 'mature' or 'early mature' with a relatively small proportion of young, or semi-mature trees.
- The Landscape Proposals are summarised in section 4.5-4.7.
- The section on tree works (4.9 - 4.12) and facilitation pruning confirms that 78No. trees will be removed of which 73No. are graded 'C' or 'U'. Trees of this low/poor quality are not generally considered to be a constraint on development.
- Arboricultural Impacts are summarised in section 4.8 and discussed more fully in section 5.0. With the exception of T51, a 'B' grade Liquidambar, all of the other TPO trees to be removed (T38, T39, T40 and T55), are either of poor or low quality.
- Tree works and facilitation pruning are specified from 4.9, with areas of 'no dig' described in 4.13 and indicated on drawing No. 231017-P-12-01/02a in Appendix A.
- At 5.11, the point is made that not all of the exact drainage and service runs are yet known. Where they are, the service runs have been located outside the root protection areas of trees and best practice guidance (such as the NJUG) will be followed.
- At 5.12 it is confirmed that trees will be protected throughout the demolition and construction process. A preliminary Arboricultural Method Statement and Arboricultural Method statement is

provided in Appendix F and a fuller Method Statement in document ref. 220610-PD-13a. This includes supervision on site by the author at key stages of the demolition and construction process.

- The tree information has been subject to a pre-application site visit with officers and the proposed tree removal and retention proposals reflects the outcome of the discussions.

- The Design & Access Statement sets out the key design principles and objectives for the landscape design, ecology and trees.

- The layout of the scheme is supported by PRP's drawing No. AL4378/001 Rev B a comprehensive Landscape Masterplan incorporating 66No. semi-mature trees (4.7).

- The key highlights the wide range of hard and soft landscape features around the site which are designed to enhance the boundaries and setting of the building.

- In addition to addressing the functional requirements of the site, the masterplan illustrates attention to detail which will enrich the lives of the residents by providing an aesthetically pleasing and stimulating series of external spaces and gardens.

- The proposal is supported by PRP's drawing No. AL4378/002 Rev B, Illustrative Boundary Proposal.

- An Ecological Assessment, by Aspect Ecology, concludes at 6.7 that 'there is no reason to suggest that any ecological designations, habitats of nature conservation interest or any protected species will be significantly harmed the proposals'.

- Furthermore, it considers that there are some net gains to be delivered by the development, with the introduction of bird boxes, bat boxes and hedgehog domes as specified in the appendices. Details of the numbers and locations of the installations are to be specified.

- A Townscape & Visual Assessment, prepared by PRP describes the site, its current landscape character and how the development will be accommodated on the site.

- The accompanying photographs from selected viewpoints illustrate only the current situation - in summer, when the screening capacity of vegetation is most effective. This is of limited help when assessing the visual impact of the proposed scheme, which involves the removal of selected trees, some of which are large and visually significant.

- Ideally the assessment should show the 'worst case' effect of the development within the winter landscape, following the removal of trees and the construction of the development.

- A Construction Method Statement has been submitted by PRP. There is no mention of advance tree works or tree protection - which should be in place prior to any demolition work or site set up. Reference to tree protection should be made under the headings of 'Minimising the impact...' or 'Environmental Management'.

- If the application is recommended for approval, landscape conditions should be imposed to ensure that the proposals preserve and enhance the character and local distinctiveness of the surrounding natural and built environment.

Recommendations:

The tree removal, retention and replacement strategy has been the subject of pre-application meetings designed to address tree and landscape issues.

The subsequent quality of the landscape proposals meets the stated objectives to provide a sustainable, high quality landscape which is well-integrated with the building and the needs of the occupants.

The proposed tree planting will secure improve the age structure / profile of the tree population and the new planting will contribute to a significant improvement to the landscape character of the Conservation Area.

No objection, subject to the above observations and RES6, RES8 (amendment to Construction Method Statement which should refer to the Tree Protection and Arboricultural Method Statement), RES9 (parts 1,2,3,4,5, and 6).

ACCESS OFFICER:

The site is located mainly within the Ickenham Village conservation area which is predominately residential. The proposal is to redevelop 5 private residential plots with an 85 room registered Care

Home for older people.

The Design & Access Statement refers to level access to the building from all entrances, including access onto patios, terraces and balconies. The main passageways to be used by residents would be a minimum of 1800 mm wide. Accessible toilets would be located close to remain communal facilities. Lifts are shown on plan.

Clarification/amendments are required to ensure accessible parking bays are sited within 50m of the entrance, with a minimum dimension of 4.8m x 2.4m and marked and signed in accordance with BS 8300:2009+A1:2010, at least one enlarged accessible parking bay, 3m x 6m, should be provided, a setting down point and parking space, 4.8m x 8m for taxis, Dial-a-Ride and accessible minibus vehicles with tail lifts, should be provided in close proximity to the main entrance, a ratio of 1 'assisted bathroom' (or assisted showers provided this meets residents needs) to 8 service users should be shown on plan, including details of the internal layout and specifications should be provided and a minimum of 10% of ensuite bathrooms should be designed to allow independent use by wheelchair users. Also, a fire evacuation plan is needed. These matters can be dealt with by condition.

ENVIRONMENTAL PROTECTION OFFICER (NOISE):

From a noise point of view, EPU have no concerns however please could the following conditions be added:

1) The rating level of the noise emitted from the site by any chiller or refrigeration unit, Air Conditioner or other mechanical plant shall be at least 5dB below the existing background noise level. The noise levels shall be determined at the boundary of the nearest noise sensitive premises. The measurements and assessment shall be made in accordance to the latest British Standard 4142, 'Method for rating industrial noise affecting mixed residential and industrial areas'.

Reason: To safeguard the amenity of the surrounding area in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

2) No air extraction system shall be used on the premises until a scheme for the control of noise and odour emanating from the site has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of measures as may be approved by the LPA. Thereafter, the scheme shall be implemented and maintained in full compliance with the approved measures.

Reason: To safeguard the amenity of the occupants of surrounding properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

3) There shall be no loading or unloading of vehicles, including delivery and the loading or unloading of goods outside the hours of 08:00 and 20:00, Monday to Friday, and between the hours of 08:00 and 14:00 on Saturdays. The premises [site] [specified plant or use] shall not be used on Sundays or Bank Holidays.

Reason: To safeguard the residential amenity of the occupiers of adjoining and nearby properties in accordance with Policy OE3 of the Hillingdon Unitary Development Plan.

Notes: The Borough has significant existing problems associated with activities and vehicle movements carried out at sensitive times, particularly during the evening and night. Restricting the hours of operation provides a valuable means of protecting the amenity of surrounding areas. Consequently, the use of the development will normally not be permitted during the evening or night unless it is accepted that such use is essential and justified.

4) No external lighting shall be installed on the land unless the details have first been submitted to and approved in writing by the Council. Any installation of external lighting shall only be carried out in accordance with the previously approved details.

Reason

To safeguard the amenity of surrounding properties in accordance with policies BE13 and OE1 of the Hillingdon Unitary Development Plan.

The noise statement states recommended mitigation measures on pages 19 & 20 of the report.

Providing these mitigation measures listed on page 20 of report ref SA-3203 dates the 30th June 2014 are adhered to by the developer, there is no need for a noise condition to be added. Should there be any variation in the specification it is advised to notify the Environmental Protection Unit accordingly.

Standard construction informative:

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with: -

A) Demolition and construction works should only be carried out between the hours of 08.00 hours and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank and Public Holidays.

B) All noise generated during such works should be controlled in compliance with British Standard Code of Practice BS 5228:1984.

C) The elimination of the release of dust or odours that could create a public health nuisance.

D) No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit, 3S/02, Civic Centre, High Street, Uxbridge, UB8 1UW (Tel.01895 277401) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

ENVIRONMENTAL PROTECTION OFFICER (LAND CONTAMINATION AND AIR QUALITY):

Comments have been provided below with regard to current air quality in the area, and the submitted air quality assessment. No information has been submitted with regard to land contamination. Whilst the current air quality in the area is poor at the roadside based on the air quality assessment and monitoring undertaken by the London Borough of Hillingdon there is insufficient grounds to refuse on air quality based on the London Council's Air Quality and Planning Guidance document. Conditions towards ensuring the development is acceptable on air quality grounds have been recommended as far as practicable.

The following information was submitted with regard to air quality:

· 88-94 Long Lane and Dormy House, Court Road, Ickenham: Air Quality Assessment, by Parsons Brinckerhoff for Signature Senior Lifestyle Ltd, dated October 2014.

Air Quality

The application site is within the AQMA, located adjacent to a school, a little way from the Hillingdon Circus junction. The assessment appears to have considered the 2014 cumulative impacts traffic data for the Master Brewer development. The air quality assessment, may have underestimated the local background for nitrogen dioxide and the emission factors used for future year seems to assume a slight reduction in nitrogen dioxide (NO₂). In spite of this the assessment does indicate that the NO₂ level is likely to exceed the EU limit value of 40 mg/m³ for about the first 2 metres from the Long Lane boundary of the site even without development. It is possible the exceedance may extend further onto the site, assuming the background level has been underestimated.

The air quality assessment indicates the development will increase NO₂ levels in the area by what is considered in the assessment to be a 'negligible' amount. The proposed development depending on the energy provision option used for the receptors considered, indicate a contribution from 0.01 to 0.16 mg/m³ for traffic and energy option A, and from 0.01 to 0.38 mg/m³ for traffic and energy option B.

As the development is in and will cause increases in an area already suffering poor air quality the following is requested:

Section 106

Section 106 obligation for up to £25,000 should be sought for contribution to the air quality monitoring network in the area with regard to this application.

Air Quality Neutral

The building emissions benchmark calculated for the site for both option A (2 x 20kWe CHPs) and option B (2 x 150kW boilers) energy provision are below the air quality neutral benchmark set for a C2 use. Option B due to more emissions is indicated to have a small and slight adverse impact on the site. (Note: the option A in the air quality assessment is not described in quite the same way as the option A in the Energy Statement which refers to GSHP and CHP.) Clarification is sought with regard to the use of bio-oil proposed for the site as well as with regard to the combined CHP GSHP option. Option A is preferred as it produces lower emissions including zero particulates, would not require the delivery and storage of bio-oil, and may have lower maintenance requirements. Biomass, which is referred to in the energy statement again has the same issues with more particulate emissions. The following condition is recommended for any permission that may be given. The energy statement would suggest the CHP emissions calculated assumes GSHPs are also in use at the site.

Air Quality Condition 1 - Details of Energy Provision (Mixed Use and residential)

Before the development is commenced details of any plant, machinery and fuel burnt, as part of the energy provision and the location of the flue(s) for the development shall be submitted to the LPA for approval. This shall include pollutant emission rates at the flue(s) with or without mitigation technologies and needs to be considered as part of a wider air quality assessment, as set out in the EPUK CHP Guidance 2012, if applicable. The use of ultra low NO_x emission gas CHPs and boilers is recommended. The development should as a minimum be 'air quality neutral' and demonstrably below the relevant building emissions benchmarks.

REASON: To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

Notes: This condition relates to the operational phase of residential and commercial development and is intended for the protection of future residents in a designated AQMA and Smoke Control Area. Advice on the assessment of CHPs is available from EPUK at:

http://www.iaqm.co.uk/text/guidance/epuk/chp_guidance.pdf. An area up to a distance of 10 times the appropriate stack height needs to be assessed. Guidance on air quality neutral is available at: <https://www.london.gov.uk/priorities/planning/consultations/draft-sustainable-design-and-construction>. They should contact Planning Specialists if they have any queries.

The transport emissions benchmark has been calculated for a C2 development, as the GLA have yet to set this. This indicates the transport emissions calculated for the development is below the TRAVL based transport emission benchmark calculated for this type of use.

Travel Plans

It is noted an Interim Travel Plan has been submitted with the application. This includes targets and travel plan monitoring. It may be advisable to condition for this.

Land Contamination

No information has been submitted with regard to land contamination with the planning application. The proposed end use is considered sensitive to contamination. The housing appears to have been built on open land and an orchard was indicated to the rear of the site on Ordnance Survey historical maps. A modified version of the standard condition is recommended for inclusion on any permission given, which includes testing of surface soils. It is advisable if soils are imported the source of the soil is agreed with the LPA prior to importing to the site.

Contaminated Land Condition

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance Document on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use; and

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement, along with details of a watching brief to address undiscovered contamination.

(ii) If during development works contamination not addressed in the submitted remediation scheme is identified, the updated watching brief shall be submitted and an addendum to the remediation scheme shall be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a comprehensive verification report shall be submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

(iv) No contaminated soils or other materials shall be imported to the site. All imported soils for landscaping purposes shall be clean and free of contamination. Before any part of the development is occupied, all imported soils shall be independently tested for chemical contamination, and the

results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

REASON: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

SUSTAINABILITY OFFICER:

I have no objections to the proposed development subject to the following conditions:

Ecology Comments

Bats were identified as using the site, although its value in their ongoing conservation status is relatively low as a consequence, it is considered likely that the proposals would be granted the relevant bat licences for the demolition of Buildings B1 and B2. There was a relatively low usage of the buildings and the value as breeding roosts is equally low. Nonetheless, the impacts on bats will require demolition of the two buildings to be undertaken in accordance with a Natural England licence. Furthermore, the development needs to accommodate bats to ensure their conservation status is maintained and supported. The development needs to contribute to a net biodiversity improvement in accordance with the National Planning Policy Framework. The following condition is therefore necessary to provide final details of the proposed enhancement measures:

Condition

Prior to the commencement of development an ecological enhancement scheme shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall clearly detail measures to promote and enhance wildlife opportunities within the landscaping and the fabric of the buildings including measures such as habitat walls, bird and bat boxes and nectar rich planting. The scheme shall aim to include an area of land dedicated to wildlife habitat. The development must proceed in accordance with the approved scheme.

Reason

To ensure the development contributes to ecological enhancement in accordance with Policy EM7 (Local Plan) and Policy 7.28 of the London Plan.

Living Walls and Roofs

Living walls and roofs can improve air quality, operate as carbon sinks and also be of importance for nature conservation. The following condition is therefore necessary:

Condition

Prior to commencement of development a scheme for the inclusion of living screens shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall provide details of the types of living material to be used and the locations and methods of maintenance where necessary. The development should proceed in accordance with the approved plans.

Reason

To ensure the development contributes to a number of objectives in compliance with Policy 5.11 of the London Plan and Policy EM1 of the Local Plan.

NB: Living screens can be used on 'blank elevations' and can consist of affordable wire trellising

systems which climbing plants can be trained up. These living screens can also be used for the growth of 'vertical allotments' and food production.

Energy Comments

The energy assessment submitted with the application demonstrates the development can meet the necessary 35% reduction in CO₂. The strategy is compatible with the London Plan Be Lean, Be Clean, Be Green hierarchy. However, it contains a couple of options for delivering the final stage of this list and accordingly the detailed design is not yet available. The following condition is therefore necessary:

Condition

Prior to the commencement of development a detailed energy assessment shall be submitted to and approved in writing by the Local Planning Authority.

The assessment shall clearly set out the baseline energy performance of the development, and the detailed measures to reduce CO₂ emissions by 35% from a 2013 Part L baseline. The assessment shall provide detailed specifications of the measures and technology set out in outline energy assessment. The assessment shall also include full details (including specification and location on the site) of the low and zero carbon technology preferred option. The information shall also include details of the type, location and management of biomass material should biofuel dependent boilers or heating systems be required.

Finally, the assessment must include appropriate measuring and monitoring proposals and how the information will be submitted to the Local Planning Authority to demonstrate CO₂ reductions are being met.

The development must proceed in accordance with the approved scheme.

Reason

To ensure appropriate carbon savings are delivered in accordance with London Plan Policy 5.2.

WATER AND FLOOD MANAGEMENT OFFICER:

The site is under a hectare and therefore does not require a Flood Risk Assessment however the site is still of substantial size and Environment Agency maps indicate that there is surface water ponding on the site. Therefore a clear drainage strategy should be submitted. This should include appropriate site investigations to indicate porosity on the site and understand groundwater issues as a basement is proposed. This should then inform a drainage strategy to control surface water run off to greenfield run off rates.

Prior to commencement, a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how it, incorporates sustainable urban drainage in accordance with the hierarchy set out in Policy 5.15 of the London Plan and will:

- i. provide information on all Suds features including the method employed to delay and control the surface water discharged from the site and:
 - a. calculations showing storm period and intensity and volume of storage required to control surface water and size of features to control that volume.
 - b. any overland flooding should be shown, with flow paths depths and velocities identified as well as any hazards, (safe access and egress must be demonstrated).
 - c. measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - d. how they or temporary measures will be implemented to ensure no increase in flood risk from commencement of construction.

ii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime. Including appropriate details of Inspection regimes, appropriate performance specification, remediation and timescales for the resolving of issues.

iii. provide details of the body legally responsible for the implementation of the management and maintenance plan.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

iii. incorporate water saving measures and equipment.

iv. provide details of water collection facilities to capture excess rainwater;

v. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

Reason

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (Nov 2012) Policy 5.12 Flood Risk Management of the London Plan (July 2011) and National Planning Policy Framework (March 2012) and the Planning Practice Guidance (March 2014). To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

TRAVEL PLAN CO-ORDINATOR:

I have no comments to make on this application.

ADULT SOCIAL CARE:

Feedback from older residents identifies a preference for living independently in the community rather than in residential care and the Council is pursuing extra care options. There are current bedspace surpluses in the Borough's care homes with existing and approved care home provision giving a supply of 1,300 units, although some may not be to market standard.

S106 OFFICER:

I have reviewed the proposals and am seeking the following S106 Planning Obligations:

1. Highways Works: the developer will required to enter into a S278/S38 agreement to secure highways works including the new access onto Long Lane.
2. Travel Plan
3. Construction Training: A financial contribution is sought equal to Training costs: £2500 per £1m build cost + Coordinator costs of £9600 or an in kind scheme to be provided to this value.
4. Employment Training: An Employment Training Strategy to be submitted.
5. Air Quality Monitoring: A financial contribution to the sum of £25,000.
6. Project Management & Monitoring Fee: 5% of total cash contributions.

7. MAIN PLANNING ISSUES

7.01 The principle of the development

Loss of Garden Land

The NPPF (March 2012) at paragraph 53 advises that LPAs 'should consider the case for setting out policies to resist inappropriate development of residential gardens, for example

where development would cause harm to the local area.'

The London Plan (July 2011) provides further guidance on how applications for development on garden land should be treated within the London Region. The thrust of the guidance is that back gardens can contribute to the objectives of a significant number of London Plan policies and these matters should be taken into account when considering the principle of such developments. Policy 3.5 of the London Plan supports development planned presumptions against development on back gardens where locally justified by a sound local evidence base.

The Mayor's Housing Supplementary Planning Guidance, November 2012 provides further guidance on the interpretation of existing policies within the London Plan as regards garden development. Paragraph 1.2.23 advises that when considering proposals which involve the loss of gardens, regard should be taken of the degree to which gardens contribute to a communities' sense of place and quality of life (Policy 3.5), especially in outer London where gardens are often a key component of an area's character (Policies 2.6 and 2.7). The contribution gardens make towards biodiversity also needs to be considered (Policies 7.18 and 7.19) as does their role in mitigating flood risk (Policies 5.12 and 5.13). Gardens can also address the effects of climate change (Policies 5.9 - 5.11).

The Council has also adopted the Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012). Policy BE1 advises that new development, in addition to achieving a high quality of design, should enhance the local distinctiveness of the area, contribute to community cohesion and sense of place and make a positive contribution to the local area in terms of layout, form, scale and materials and seek to protect the amenity of surrounding land and buildings, particularly residential properties. Specifically, the policy advises that development should not result in the inappropriate development of gardens and green spaces that erode the character and biodiversity of suburban areas and increase flood risk.

Within the Council's emerging Local Plan: Part 2 - Development Management Policies (Proposed Submission Version, September 2014), paragraph 4.16 advises that the Council, in general will not accept proposals for development on back garden land. Policy DMH6: Garden and Backland Development states:-

'There is a presumption against the loss of back gardens due to the need to maintain local character, amenity space and biodiversity. In exceptional cases a limited scale of backland development may be acceptable, subject to the following criteria:

- rear garden land which contributes either individually or as part of a larger swathe of green space to the amenity of residents or provides wildlife habitats must be retained;
- neighbouring residential amenity and privacy of existing homes and gardens must be maintained and unacceptable light spillage avoided;
- vehicular access or car parking should not have an adverse impact on neighbours in terms of noise or light. Access roads between dwellings and unnecessarily long access roads will not normally be acceptable;
- development on backland sites must be more intimate in mass and scale and lower than frontage properties;
- Features such as trees, shrubs and wildlife habitat, which are important to character,

appearance or wildlife must be retained or re-provided.'

However, as the Development Management Policies document has not yet been adopted, only limited weight can be given to it.

It will therefore be important to assess the proposals against the various contributing assets that gardens provide towards the overall quality of the area that is appreciated by surrounding residents and it not appropriate to dismiss development proposals just because garden land is involved. These issues are considered in the relevant sections of the report.

Importantly, the GLA also do not raise any in principle objections to the scheme.

Loss of Class C3 Housing

The proposal involves the loss of Class C3 residential accommodation. Policy H3 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) states that such loss will only be permitted if replacement housing is provided on site. This scheme does not replace the Class C3 housing and therefore it is strictly contrary to saved UDP policy.

The GLA however in their Stage 1 report make clear that this application needs to be considered in the light of more up to date housing policy contained in the NPPF and London Plan as opposed to local policy H3. In national and strategic terms, the GLA advise that Class C2 care home accommodation should be considered as part of the overall provision for housing as all self-contained and non self-contained forms of residential accommodation should be included. As such, the proposal complies with policies 3.3, 3.8 and 3.14 of the London Plan and that in terms of housing delivery targets, this scheme results in a net gain of 80 units (the 85 care home units being provided minus the 5 houses lost). With a potential net increase of 80 units, the proposal would also be supported by Policy 3.14 B of the London Plan which requires loss of housing to be replaced at existing or higher densities with at least an equivalent floorspace and therefore this increase in provision, in density terms does accord with local policy H3. Furthermore, the GLA advise the net gain in housing is also supported by the process whereby care homes 'free up' conventional housing from which care home residents derive.

As regards the care home use, the application is supported by the submission of a needs assessment, together with further information in a letter dated 20/8/14. This points to the ageing population, where dementia will be more prevalent amongst an increasingly elderly population and seeks to quantify the growing demand for care home bedspace with the vicinity of the application site.

The Council's Adult Social Care Team do advise that feedback from older residents identifies a preference for living independently in the community rather than in residential care and the Council is pursuing extra care options and there are current bedspace surpluses in the Borough's care homes. However, the GLA point out that Greater London trends demonstrate that there is considerable growth in the older people population so that even if there is a current excess of supply, future demand also needs to be met.

The GLA also refer to the Council's market position statement which advises that there is likely to be a "significant increase" in the number of people aged 85 years and over with dementia in the next 30 years and that "in the absence of a medical breakthrough that results in a cure, addressing the needs of people living with dementia and preventing avoidable, premature deterioration is going to be a significant issue for both the Council

and the NHS for the foreseeable future". The GLA advise that as this proposal contains a dementia care facility with a high quality of care and living environment, clearly it would help to address the need for additional dementia care facilities in the area.

Finally on this point, the GLA refer to Annex 6 of the Mayor's Housing SPG which indicates significant pan-London and Hillingdon need for care home places and at the Pan London level, there is an existing need which is expected to increase. At Table A5.1 of the Further Alterations to the London Plan, indicative annualised benchmarks for specialist housing for older people are presented and for Hillingdon, this is 155 units per year. The proposal would therefore assist in meeting this need.

Therefore it is considered that no objections can be raised to the loss of Class C3 housing nor the principle of a care home use on this site.

Employment and Economic Benefit

This scheme also needs to be considered in the light of the expected economic benefits that this scheme would bring. The application states that approximately 90 full and part time jobs would be created which is supported by London Plan policies 4.1 and 4.12. An economic benefits assessment has also been submitted in support of the application which advises of the financial inputs that the scheme would provide to the local economy over the construction and operational phases and the scheme would save the NHS approximately £29,500 per annum.

7.02 Density of the proposed development

The Mayor's density criteria are not applicable to a care home scheme.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

The application site is not located within any existing or proposed Archaeological Priority Area nor does it have any other archaeological designation. Nevertheless, comments have been received from GLAAS which do need to be taken into consideration. They advise that due to the remains of a Roman field system in the vicinity and the good potential for preservation of buried archaeological remains within the garden areas of this site, further investigation is needed to establish if there are associated remains present, possibly the settlement itself. This investigation is currently being undertaken and the results will be reported to GLAAS before any decision is made on the application which forms part of the officer recommendation.

The application site is located adjacent to the Douay Martyrs School (Cardinal Hume Campus) which contains the locally listed predominantly single storey original early 20th century school building and also forms part of the Ickenham Village Conservation Area.

In terms of the impact on the locally listed school building, this is set back from the southern side boundary of the school site which adjoins No. 88 Long Lane and is separated and screened from the application site by large modern school buildings. The school site also benefits from good tree cover. As such, the proposed building, which itself will be set back from its northern boundary which adjoins the school and will have dense tree and shrub screening along the boundary, will not adversely affect the setting of the building.

As regards the impact on the Ickenham Village Conservation Area, the application site, although forming part of the conservation area, is located on its south eastern edge, away from its historic core. Here, the general character of the area comprises good quality, 2 storey, detached and semi-detached houses of mixed architectural style and mostly 20th century in date set within well-established gardens that include many fine trees visible from

surrounding roads that define the sylvan character of the area.

A heritage statement and a townscape and visual assessment have been submitted in support of the application. The heritage statement provides a very detailed assessment of the historical and architectural value of the existing buildings on site and the contribution they make towards the character and appearance of the conservation area. The statement concludes that the buildings are not heritage assets, being neither statutorily designated nor locally listed and they lack any special architectural or historic interest. Whilst the buildings respect the general pattern of residential development, being on green plots and set back from the road, they are not considered to make a strong positive contribution to its overall character and appearance. The townscape and visual assessment also provides very comprehensive analysis of the impact of the proposed care home on the conservation area, including a detailed site/view analysis.

The Council's Urban Design/ Conservation Officer advises that the reports are well researched and thorough and concurs with much of the assessments and their conclusions on this part of the conservation area and the heritage value of the existing buildings on site. The officer advises that the only buildings of interest on site are No. 88 Long Lane, a typical interwar two storey house, with attractive applied decorative timber framing and Dormy House, a modern house probably built in the late 1960s with an unusual green copper clad roof. Having inspected these buildings on site, the officer advises that there would be no objections raised to their demolition or to the demolition of the other buildings on site, subject to them being surveyed to English Heritage Recording Level 1. This has been conditioned.

As regards the impact of the proposed care home, the new building would generally conform with the set back from the road of the existing houses. The Council's Urban Design/ Conservation Officer advises that the Long Lane frontage in particular has been very carefully designed to reflect the architecture and materials of some of the surrounding traditionally designed "Arts and Crafts" style houses with the inclusion of gables, a traditional clay tiled pitched roof with low eaves, dormers and chimneys. As regards to the much larger footprint of the care home, the officer advises that this would be comparable with the neighbouring traditional school buildings which extend towards the rear of the site and although these are extensive, as they are single storey they do not look out of place within the local townscape. In terms of the care home, although of a different scale and bulk, the articulated layout of the proposed building would break up its overall bulk into smaller elements and being well set back from the site frontage and boundaries, the partial screening provided by existing and new planting would reduce its potential impact on the immediate townscape.

The Council's Urban Design/ Conservation Officer did raise concerns with a number of views of the building, particularly at the rear and in terms of the bulk and height of the core of the building. Revisions have been made to the proposals and in particular the scale and bulk of the roof of the building has been reduced as much as possible so that the officer does not raise any further objection to the scheme.

The GLA also considered the scheme to be acceptable in design terms, stating that "the proposal demonstrates a well thought out building layout, which responds successfully to the constraints of the site." They conclude on this aspect of the scheme in their Stage 1 Report that " the building has been designed to respond to the wider street scape along Long Lane, in line with London Plan Policy 7.4 'Local Character', forming a well-defined street facing building line while implementing a simple and traditional style of architecture

that relates successfully to its surroundings and the setting of the conservation area. The scale and massing of the proposals are therefore in line with that of the surrounding context and are supported from a strategic perspective. The intention to use high quality facing materials and detailing is welcomed and the Council is encouraged to secure key details such as window reveals, and eaves lines to ensure the highest quality of architecture is built through."

Therefore, subject to these details which is controlled by condition, the impact of the scheme on the Ickenham Village Conservation Area is acceptable.

7.04 Airport safeguarding

The proposal does not give rise to any airport safeguarding concerns.

7.05 Impact on the green belt

The site does not fall within nor is it sited close to designated Green Belt land and therefore no Green Belt issues are raised by this proposal.

7.07 Impact on the character & appearance of the area

This is dealt with in Section 7.03 above.

7.08 Impact on neighbours

Policies BE20, BE21 and BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) seek to protect the amenities of surrounding residential properties from new development in relation to loss of sunlight, dominance and loss of privacy respectively. The Council's Supplementary Planning Document HDAS: Residential Layouts provides further clarification in that it advises that buildings of two or more storeys should maintain at least a 15m separation distance from adjoining properties to avoid appearing overdominant and a 21m distance between facing habitable room windows and private amenity space (considered to be a 3m deep 'patio' area adjoining the rear elevation of a property) should be maintained to safeguard privacy.

Given the siting of the proposed care home in relation to surrounding residential properties, there would be no loss of sunlight to neighbouring properties or their gardens.

The scheme has been carefully designed so that the overall bulk of the building has been broken up with its elevations being well articulated and set back from its boundaries.

The nearest residential properties to the proposed building are located to the south west. As regards No. 96, the adjoining property on Long Lane, the nearest part of the proposed care home would be its two storied front wing sited some 9.9m from the main side elevation of No. 96 with the proposed building set back from the shared side boundary by some 6.3m. The front wing of the building would not breach a 45 degree line of sight taken from the rear elevation of No. 96 and given the separation distance and tree lined boundary, the building would not appear unduly dominant from this property. The nearest adjoining properties on Swakeleys Drive are Nos. 2A and 2B Swakeleys Drive, two bungalows whose rear elevations would directly face the proposed care home. As regards No. 2A, this would mainly adjoin one of the care home's gardens, with the nearest proposed building elevation being the corner of the side elevation of the front wing, which would be sited some 21m away. However, this is not a particularly direct relationship with the more direct line of sight being to the taller central wing of the care home which is recessed further into the site where the nearest part of this elevation would be sited some 36.4m away, sited behind the courtyard garden. As regards No. 2B, the nearest part of proposed care home would be the side elevation of the two storey rear wing, which would be domestic in scale and sited some 25.4m from the property's rear elevation with the wing set back some 11.8m from the side boundary of the application site in which space, a double line of tree planting is

proposed. It is considered that this distance which exceeds the Council's separation distance guidelines and the proposed landscaping would fully safeguard the residential amenities of this property. Notwithstanding this, it is considered that a further enhancement could be made with the provision of a green wall/living screen, details of which have been conditioned. The only other property sited close to the care home would be No. 7A Court Road which has a side elevation fronting the site boundary. The nearest elevation of the care home would be two storied and at its nearest point would be sited some 18.6m from the boundary.

Elsewhere, due to the longer rear gardens of adjoining properties, the proposed care home would be sited much further away from its neighbours, with typical distances of 69m and 85m to the rear elevations of properties in Swakeleys Drive and Court Road respectively.

These separation distances satisfy the Council's minimum separation distance of 15m for two or more storied development and the layout of the building ensures that the proposed elevations closest to neighbouring properties are domestic in scale and set well back from their boundaries allowing existing boundary screening to be maintained and enhanced.

As regards the potential for overlooking, the side elevations of the front and rear wings of the care home facing No. 96 Long Lane, 2A and 2B Swakeleys Drive and side elevation of the rear wing facing No. 7A Court Drive would not contain any windows so that no adjoining property or their patio area would be overlooked within a distance of 21m.

The proposal therefore fully complies with Council planning policy and supplementary design guidance and there would be no loss of residential amenity to surrounding neighbours by reason of loss of sunlight, dominance or loss of privacy. Noise and air quality issues are considered in Section 7.18 below.

7.09 Living conditions for future occupiers

The proposal would provide accommodation of a high standard, with good sized residents' rooms, all of which would have en-suite facilities, good outlook and natural lighting. The care home would also provide a good range of communal facilities, including restaurant, cafe, cinema, spa and provide extensive garden areas that would be separated into different areas with different characteristics and activities.

The ground floor bedrooms would also include small garden/ patio areas that would maintain the privacy of the adjoining rooms. As regards the privacy afforded between the rooms, the only area where the relationship is not ideal is between the bedroom windows on the rear elevation and rear wing. However, it is considered that the relationship could be improved with the use of screening/ angled windows which could be controlled by condition.

7.10 Traffic impact, Car/cycle parking, pedestrian safety

Policies 6.3, 6.9, 6.10 and 6.13 of the London Plan (July 2011) and saved policies AM2, AM7, AM9, AM14 and AM15 of the Hillingdon Local Plan (November 2012) are concerned with traffic generation, road capacity, vehicle and cycle parking and access to public transport.

A Transport Assessment has been submitted in support of the application which has been reviewed by the Council's Highway Engineer. The Engineer originally queried the sample sites used in the assessment to predict traffic generation and car parking provision as they were more than 5 years old and the lack of accident data. However, revised estimates of trip generation have now been submitted and although the Council's Highway Engineer considers that they are still not as robust as he would wish, but given the limited site

availability in the TRICS database, the proposed use being a care home and the scale of the proposed development, the proposals are not considered likely to result in significant vehicle movements. A record of personal injury accidents has also now been provided for the 5 year period up until 30th June 2013. The engineer advises that although an analysis of the collisions has not been provided, the records do not show a high number of collisions in the vicinity of the site and overall, the pattern of collisions do not reveal any common issues in connection with the road layout and/or vehicle speeds. As such, the Highway Engineer advises that the proposed use and car parking provision is acceptable from a highway safety perspective.

The Highway Engineer also raised an initial concern about the proposed layout of the site and the new access arrangements on Long Lane, particularly as regards the central hatched area on Long Lane and the refuse and waste storage provision on site. This aspect of the scheme has been the subject of various amendments. The Highway Engineer advises that refuse vehicles would collect from within the site and tracking diagrams demonstrate that adequate provision has been made to allow refuse vehicles to turnaround on site. The Highway Engineer also advises that amendments have been made to include revisions to the central hatching on Long Lane to facilitate the vehicles turning right to access the site and also the vehicles turning right to access the houses situated on the opposite side of the road, access works, tactile paving, short lengths of double yellow lines on Long Lane, closure of existing redundant accesses and reinstatement of footway, and other associated associated works as required. The proposed double yellow lines in proximity of the proposed access are required to facilitate service vehicle swept paths and to prohibit vehicles from parking closed to this access. The Highway Engineer advises that the access arrangements are acceptable and these have also been the subject of further public consultation.

The Highway Engineer concludes that subject to the above off-site highway works being secured through a S106 agreement and a condition attached securing 2.4m x 2.4m pedestrian visibility splays at the point of vehicle access, no objection is raised from a highways point of view. These form part of the officer recommendation.

7.11 Urban design, access and security

As regards secure by design, a condition has been added that would ensure that adequate security measures would be put in place on site.

7.12 Disabled access

The Council's Access Officer provides detailed comments as regards the access, parking and servicing provision needed to serve the development and details the measures required to satisfy Lifetime homes standards. The officer also makes reference to the need for a fire excavation strategy. These are detailed matters that can be dealt with by conditions.

7.13 Provision of affordable & special needs housing

The need for this type of proposed housing is addressed in Section 7.01 of this report.

7.14 Trees, landscaping and Ecology

Policy BE38 of the Hillingdon Local Plan: Part Two - Saved UDP Policies requires new development to retain and utilise existing landscape features of merit and to provide new planting and landscaping wherever appropriate.

A number of trees are protected by Tree Preservation Order No.438, in the garden of Dormy House. Another TPO (No. 482A) protects three trees on the rear boundary of No. 90 Long Lane whereas offsite, there are two protected trees on the front boundary of No. 96 Long Lane (TPO No. 496). The whole of the site also forms part of the the Ickenham Village

Conservation Area, which provides protection to the remaining non-TPO'd trees.

The Council's Tree/ Landscape Officer advises that the tree cover on this and adjoining sites makes a significant contribution to the landscape and visual quality of the area, contributing to its environmental quality, including air quality and biodiversity. As a consequence, justification for any tree loss is required and suitable replacement tree planting should be incorporated within a high quality landscaping scheme which improves the age profile and succession of trees on site.

An Arboricultural Report accompanies this application which includes an assessment of the quality and value of existing trees, the impact of the proposed development upon trees on and adjacent to the site, proposed methods for reducing development impacts on retained trees, proposed tree protection measures and explains the proposed tree planting strategy. The Council's Tree/ Landscape Officer advises that the submitted information accords with relevant standards (BS5837:2012) and several relevant Building Research Establishment documents (1998, 2006 and 2011).

The officer advises that the assessment has involved a total of 122 individual trees, groups and shrubs, of which one tree is considered to be 'A' grade of high quality and value, 14 are 'B' or moderate grade, 89 are 'C' or low grade and 18 are 'U' or of poor quality. Of the 78 trees that are proposed for removal, 73 are graded 'C' or 'U', trees of low/poor quality which are not generally considered to be a constraint on development, including 5 TPO'd trees. The other 5 trees to be removed are of 'B' or moderate grade of which 1 is a TPO'd tree (T51 - Liquidambar). The report also identifies that in terms of the age profile, most of the trees surveyed are 'mature' or 'early mature' with a relatively small proportion of young, or semi-mature trees that would, over time, succeed them. The officer advises that no objections are raised to the proposed tree removal, which predominantly involves low and poor quality trees and that the replacement strategy has been the subject of pre-application meetings designed to address tree and landscape issues. The officer concludes that the subsequent quality of the landscape proposals meets the stated objectives to provide a sustainable, high quality landscape which is well-integrated with the building and the needs of its occupants. Furthermore, the proposed tree planting will secure and improve the age structure / profile of the tree population and the new planting will contribute to a significant improvement to the landscape character of the Conservation Area.

On this basis the Tree/ Landscape Officer recommends approval of the scheme, subject to conditions which are included in the officer recommendation.

Ecology

As regards ecology issues, an ecological assessment has been submitted in support of the application. This includes results of a site survey undertaken in June 2014, with specific surveys undertaken in respect of badger, bats and great crested newt. The main findings are that the gardens are dominated by amenity grassed areas, bordered by amenity planting, hedgerows, fencing and scattered trees and includes built structures such as sheds and greenhouses. These habitats are generally dominated by non native/ introduced species and are of low/negligible ecological value at the site/ local level. Cotoneaster and Rhododendron were found within two properties, and it is recommended that these species are carefully removed from the site and replaced with native species and/or species of acknowledged benefit to wildlife. As regards protected species, the site does provide habitat suitable for bats and nesting birds and two buildings were recorded as supporting small summer (non-breeding) roosts for Soprano Pipistrelle and Common

Pipistrelle. The assessment advises that the demolition of these buildings will be conducted under a Natural England licence, which will include appropriate mitigation measures to safeguard bats. Replacement bat roost features will also be provided in the form of bat boxes in order to ensure no net loss of the local bat population conservation status. In addition, a sensitive lighting scheme is recommended along the site boundaries to ensure the long-term use of these features as foraging/commuting features for bats. As regards nesting birds, the report advises that common birds may utilise the trees, hedgerow and shrubs within the site for nesting and it is recommended that any clearance of potential nesting habitat be undertaken outside of the bird nesting season.

The Council's Sustainability Officer has reviewed the assessment and advises that although bats were recorded using the site, its value in their ongoing conservation status is relatively low. Usage of the two buildings was also relatively low and value as breeding roosts is also equally low. However, the demolition needs to be undertaken in accordance with a Natural England licence and the development needs to accommodate bats to ensure that their conservation status is maintained and supported. Furthermore, to ensure that the development contributes to a net biodiversity improvement, enhancement measures should be secured by condition. This forms part of the officer recommendation.

The Sustainability Officer also advises that living walls and roofs, even in the form of wire trellising systems on blank elevations which climbing plants can be trained up, can improve air quality, operate as carbon sinks and also be of importance for nature conservation. As this development does provide opportunities for the use of living walls and roofs, which will also help further screen the building from neighbouring properties, a condition is recommended.

7.15 Sustainable waste management

Policy 5.17 of the London Plan requires that all new development provide adequate facilities for the storage of waste and recycling.

The proposal makes appropriate provision for refuse and recycling close to the entrance of the site which will be screened.

7.16 Renewable energy / Sustainability

An Energy Assessment has been submitted with the application.

The Council's Sustainability Officer advises that whilst the energy assessment demonstrates the development can meet the necessary 35% reduction in CO2 emissions and generally follows London Plan guidance, the detailed design has not been finalised and therefore further information is required which can be secured by a recommended condition.

The GLA advise that further information is required in terms of the proposed external shading architectural features, and also the floor area and location of the energy centre should be provided. The GLA also advise that the sizing of the CHP system appears oversized for the building, including monthly load profiles for heat and electricity and the proportion to be met by the CHP to support the CHP sizing and carbon savings claimed. AS such, further information is required and information should also be provided on the proposed management and maintenance arrangements for the system.

The applicant's advise that the space requirements for options A and B are provided by the Energy Report which will be within the proposed basement plant room and Drw. No. e14010-GA-700-B1-1001 Rev. P3 is attached to illustrate this again. As regards the CHP details, a detailed feasibility is to be undertaken at the next design stage to select the CHP

unit. This will confirm the exact size of the CHP and the monthly profiles for heating demands and electricity. Notwithstanding this, Initial Thermal Modal Load annual profiles - Be Clean model for the Total Fuel Demand, Heating and Electricity has been submitted based on CHP units at other similar applicant's care homes which demonstrates the savings are realistic. Management/ maintenance arrangements would be agreed in due course. These issues can be secured through the use of planning conditions/and/or S106 clauses.

As regards renewable energy, the applicant advises that the total demand is made up of combined technology types additive at different times of the day or night. Each technology is therefore not competing, but contributing to carbon emission reductions. As regards to solar PVs, the applicant advises that these are not considered suitable for this project and therefore the roof design will not need to alter.

Therefore, it is considered that the outstanding issues can be dealt with by the Sustainability Officer's recommended condition which forms part of the recommendation.

7.17 Flooding or Drainage Issues

The application site is not located in an area with an identified risk of flooding, however Policy OE8 of the Saved Policies UDP and Policies 5.12 and 5.13 of the London Plan still require that developments seek to reduce surface water run-off and reduce the risk of flooding elsewhere.

The Council's Water and Flood Risk Officer advises that although the site is under a hectare and therefore does not require a Flood Risk Assessment, it is still of substantial size and Environment Agency maps indicate that there is surface water ponding on the site. The officer advises that in such circumstances, a clear drainage strategy should be submitted, which should include appropriate site investigations to identify porosity on the site and assess groundwater issues as a basement is proposed. This should then inform a drainage strategy to control surface water run off to greenfield run off rates. This would be secured by the officer's recommended condition.

7.18 Noise or Air Quality Issues

Policy OE1 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) resists the grant of planning permission for uses and associated structures which are, or are likely to become, detrimental to the character or amenities of surrounding properties or the area generally, due to various impacts, including noise generation or emission of other pollutants. Policy BE19 seeks to protect the amenity in residential areas generally.

Noise and External Lighting

An acoustic report has been submitted with the application which sets out the findings of a noise survey, assesses appropriate internal and external noise criteria and sets out the glazing/ ventilation specifications and design criteria for plant and equipment required so that the development would have no detectable effect on health and quality of life due to noise.

The Council's Environmental Health Officer has reviewed the report and advises that providing the recommendations are followed, the scheme is acceptable subject to the imposition of specific conditions relating to specify the noise rating for mechanical plant, air extraction noise scheme and the hours of deliveries. A condition is also recommended to secure details of any external lighting scheme. These conditions form part of the officer's recommendations.

Air Quality

The application site is located within an Air Quality Management Area, close to its northern boundary. An Air Quality Assessment has been submitted in support of the application which assesses the likely emissions from the two preferred options for heating plant and the cumulative effects of traffic growth from the development itself and from other potential developments in the area including the redevelopment of the former Master Brewer site and Hillingdon Circus.

The report advises that on the site, the predicted pollution levels are generally within the air quality objectives and the location is suitable for a care home. The only exception to this is an approximate 2m wide strip at the front of the site, adjacent to the Long Lane frontage where nitrogen dioxide (NO₂) levels exceed the annual mean objective in both the future baseline and future with development scenarios. However, as this exceedance is primarily caused by existing traffic levels on Long Lane and given the modest increase in concentrations likely to result from the development, mitigation at source would not be appropriate so that the scheme proposes to mitigate this impact by limiting resident and visitor access to the area by the use of soft landscaping.

The report advises that as regards the impact of emissions from the traffic generated by the development, these are imperceptible due to the limited extent of the extra trips involved.

The report then goes on to consider heating plant and advises that of the two preferred options, local impacts under Option B (2 x bio-oil fired boilers) are greater than those under Option A (2 x gas fired combined heat and power units), although neither generate new exceedances of objectives and, offsite, impacts are generally of negligible significance. Slight adverse impacts are possible with Option B, but these are limited to a zone of limited extent in the southernmost corner of the school grounds that lie adjacent to, and to the north of, the site. However, the report advises that taking into account the conservatism built into the assessment, it is considered likely that in reality, the impacts will be negligible everywhere.

The report goes on to advise that the development emissions are within the benchmarks set by the Mayor of London and it is, therefore, air quality neutral. This applies whichever option for local heating plant is selected.

The Air Quality Assessment has been reviewed by an Environmental Health Officer. The officer advises that the assessment may have underestimated the local background for nitrogen dioxide (NO₂) and notes that emission factors for future years assume a slight reduction in NO₂. As a result, the officer advises that the EU limit value of 40 mg/m³ might be exceeded further onto the site, assuming the background level has been underestimated. The officer advises that the proposed care home use would be very sensitive to poor air quality but as the building facade fronting Long Lane would be set back some 10m from the boundary, no objections are raised to the currently proposed siting of the building. The developer however should consider siting air inlets, openable windows etc, away from flues and air extraction points which is advised within a recommended informative.

The officer also notes that both heating plant options for the site satisfy the air quality neutral benchmarks set by the Mayor for a C2 use of this size, although Option A would be the preferred option as it produces lower emissions including zero particulates, would not

require the delivery and storage of bio-oil and may have lower maintenance requirements. As advised by the officer, a condition is recommended to ensure that the details of any plant, machinery and fuel burnt as part of the energy provision, together with the location of the flue(s) for the development are submitted which should include pollutant emission rates at the flue(s) with or without mitigation technologies.

The officer goes on to advise that although this scheme would be air quality neutral in terms of the Mayor's benchmark standards, it will still add to pollution in an area already suffering poor air quality and therefore a S106 contribution is required for monitoring purposes. This forms part of the officer's recommendation.

The officer concludes that there is insufficient grounds to refuse this scheme and that the recommended conditions will ensure that the development is acceptable as far as practicable on air quality grounds. The officer also notes that an Interim Travel Plan has been submitted with the application and advises that a Travel Plan should be conditioned. A Travel Plan would form part of the S106 Agreement.

7.19 Comments on Public Consultations

As regards the reasons for objecting to the proposals raised by the petitioners, these have been addressed in the officer's report.

As regards the individual objection comments, Points (i) - (iii), (vi) - (xviii), (xxi) - (xxviii), (xxx) and (xxxiv) have been dealt with in the relevant sections of the officer's report. As regards points (iv) and (v), the points raised that this would be another corporate residence that will reduce the sense of community (point iv) and only 3 family houses would be left on this stretch of road (point v) are noted, but they do not raise material planning issues of sufficient weight over and above the normal planning considerations on the impact of the development on the character of the area that could justify a refusal of permission. As regards point (xix), noise from construction traffic would be a matter for environmental health legislation. Point (xx) regarding tree planting damaging property foundations is speculation and seems unlikely. Point (xxix) is noted. Points (xxxii) and (xxxiii) do not raise material planning considerations. As regards point (xxxv) if a proposal for a change of use of the building was submitted in the future, this would need to be assessed on its planning merits. As regards point (xxxv), residents are listened to but again, there is the statutory requirement to consider all applications on their individual planning merits.

The comment in support of the application is noted.

The comments raised by the Ickenham Residents' Association and the ward councillor raising material planning considerations have also been dealt with in the officer's report.

7.20 Planning obligations

Policy R17 of the Council's Unitary Development Plan states that: 'The Local Planning Authority will, where appropriate, seek to supplement the provision of recreation open space, facilities to support arts, cultural and entertainment activities, and other community, social and education facilities through planning obligations in conjunction with other development proposals'.

A S106 Agreement would be needed to secure the following:-

- (i) Highways Works: the developer will be required to enter into a S278/S38 agreement to secure highways works including the new access onto Long Lane.
- (ii) Travel Plan
- (iii) Construction Training: A financial contribution is sought equal to Training costs: £2500

per £1m build cost + Coordinator costs of £9600 or an in kind scheme to be provided to this value.

(iv) Employment Training: An Employment Training Strategy to be submitted.

(v) Air Quality Monitoring: A financial contribution to the sum of £25,000.

(vi) Project Management & Monitoring Fee: 5% of total cash contributions.

The proposal is Mayoral or Council CIL liable.

7.21 Expediency of enforcement action

There are no enforcement issues raised by this application or the site.

7.22 Other Issues

Land Contamination

The Council's Environmental Health Officer advises that no information has been submitted with regard to land contamination with the planning application and a care home is considered to be sensitive to contamination. However, the existing housing appears to have been built on open land and an orchard appears to have been sited at the rear of the site. Given that significant land contamination issues are not anticipated on site, this can be dealt with by condition. This forms part of the officer's recommendation.

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

10. CONCLUSION

The proposed care home would involve the loss of 5 existing houses sited and have a more extensive building footprint that would take up more garden land than the existing houses. However, there are sites in the vicinity that have large building footprints which extend in depth across the site, including the immediately adjoining Douay Martyrs School site to the north. Furthermore, the scheme has been well designed so that the impacts of the built form have been minimised whilst the application site is extensive that allows the building to be set back from its boundaries which would be well screened and significant garden areas to be maintained, particularly on the more sensitive southern and western (rear) boundaries adjoining residential properties. The impacts of the scheme in terms of landscaping and tree impacts, and the considerations advised by the London Plan such as biodiversity, flood risk and climate change are acceptable and in many cases, likely to be beneficial, so that it is considered that a reason for refusal could not be justified in relation to the loss of garden space.

The loss of 5 Class C3 houses is strictly contrary to policy H3 of the saved UDP but the GLA advise that the application should be considered in terms of the more up to date policies in the NPPF and London Plan which advise that Class C2 accommodation should be included as part of the Borough's overall housing provision and therefore results in a net increase of 80 units on this site. The Council's Adult Social Care do query the need for the facility within the borough, but the GLA advise that there is significant pan - London and Hillingdon need for the facility and that this need for care home places is expected to increase.

The Council's Urban Design/ Conservation Officer raises no objections to the demolition of the houses, subject to their details being recorded, which would be controlled by condition. The proposal has also formed the subject of pre-application discussions and advice and

the scheme has undergone various revisions. The conservation officer is fully supportive of this scheme and considers it to be a high quality and well considered proposal that would not harm the character or appearance of the Ickenham Village Conservation Area.

The scheme would not harm the amenities of surrounding residential occupiers and fully complies with Council guidance. Furthermore, the Highway Officer is satisfied that the proposed parking is sufficient to serve the development and the proposed access arrangements would not prejudice highway safety.

The Mayor does not raise any in principle objections to the scheme in his Stage 1 report.

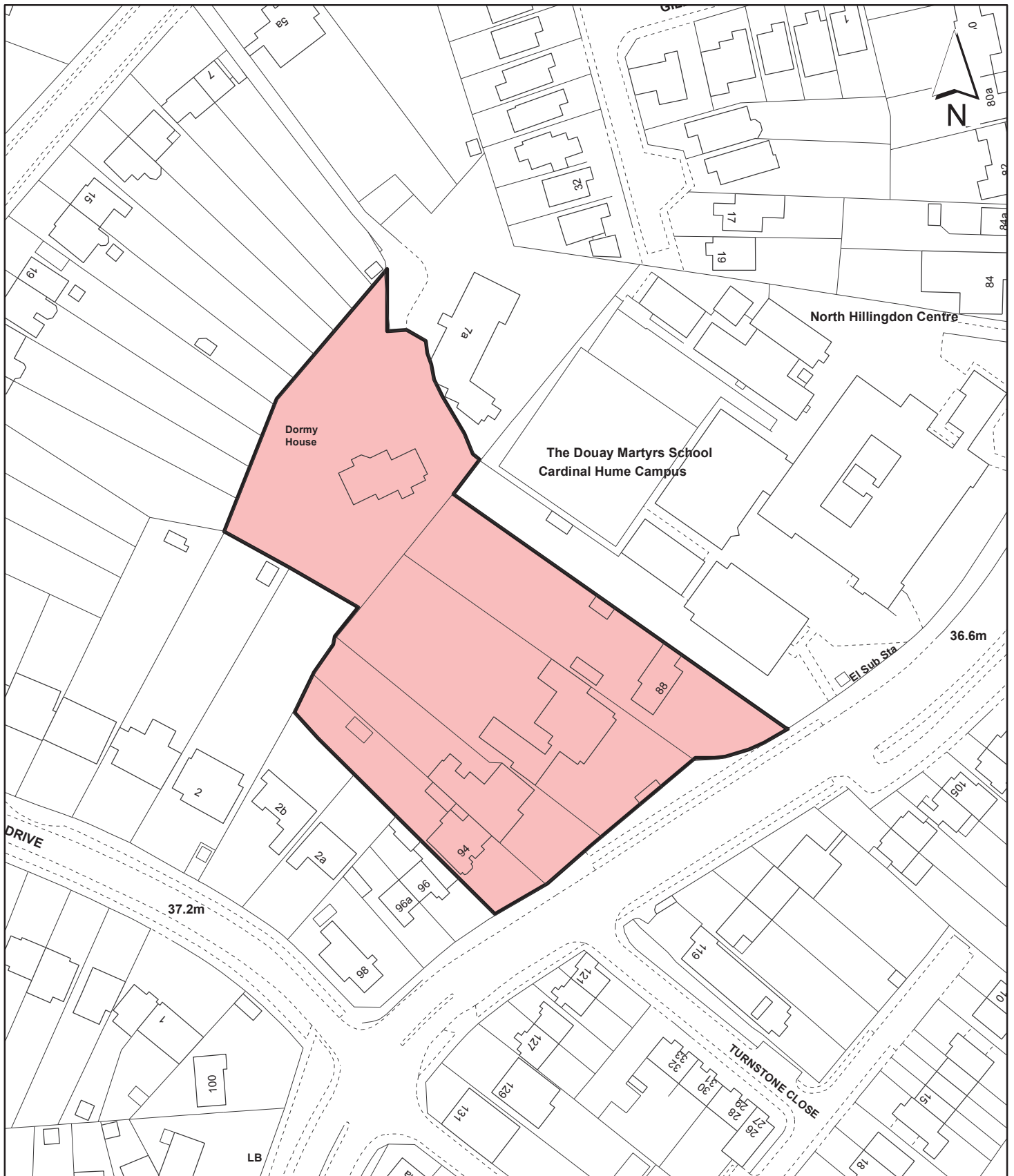
The application is recommended favourably, but further archaeological investigation is required, the results of which need to be referred to GLAAS and the application would need to be referred back to the Mayor.

11. Reference Documents

National Planning Policy Framework (March 2012)
Planning Practice Guidance (March 2014)
London Plan (July 2011)
Hillingdon Local Plan (November 2012)
HDAS: 'Accessible Hillingdon'
Consultation responses

Contact Officer: Richard Phillips

Telephone No: 01895 250230



Notes

 Site boundary

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Site Address

**88-94 Long Lane
 Ickenham**

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111

Planning Application Ref:

52129/APP/2014/2996

Scale

1:1,250

Planning Committee

Major Page 69

Date

March 2015



HILLINGDON
 LONDON

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Report of the Head of Planning, Sport and Green Spaces

- Address** HAYES AND HARLINGTON STATION STATION ROAD HAYES
- Development:** Application under Schedule 7 of the Crossrail Act 2008 for the approval of Plans and Specifications associated with the erection of a new station building, external lift shaft, footbridge with lift shafts, extension of platforms 2/3 and 4/5 with canopy modifications, new waiting room to Platform 4/5 and associated lighting.
- LBH Ref Nos:** 10057/APP/2014/4338
- Drawing Nos:** WSN1B-ALL-DRG-AEA-000270 Rev A03
WSN1B-ALL-DRG-AEA-000271 Rev A02
WSN1B-ALL-DRG-AEA-000273 Rev A02
WSN1B-ALL-DRG-AEA-000278 Rev A03
WSN1B-ALL-DRG-AEA-000280 Rev A03
WSN1B-EAR-DRG-AEA-000011 Rev A08
WSN1B-EAR-DRG-AEA-000260 Rev A02
WSN1C-EAR-DRG-BEN-000250 Rev P02
WSN1C-EAR-DRG-BEN-000251 Rev P02
WSN1C-EAR-DRG-BEN-000252 Rev P02
WSN1C-EAR-DRG-BEN-000253 Rev P02
WSN1C-EAR-DRG-BEN-000254 Rev P02
WSN1C-EAR-DRG-BEN-000255 Rev P02
WSN1C-EAR-DRG-BEN-000256 Rev P02
WSN1C-EAR-DRG-BEN-000257 Rev P02
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WSN1C-EAR-DRG-BEN-000259 Rev P02
WSN1C-EAR-SKT-BEN-000005
WSN1C-EAR-SKT-BEN-000006
WSN1C-EAR-SKT-BEN-000007
WSN1B-CNS-PLD-NCA-000012 Rev A01
Written Statement for Information HIL/3/4/NR
Design and Access statement HIL/3/4/NR

Date Plans Received: 09/12/2014 **Date(s) of Amendment(s):**

Date Application Valid: 09/12/2014

1. SUMMARY

The principle of the development has been established under the Crossrail Act 2008, however Schedule 7 requires the Local Planning Authority to approve details in relation to the erection of a new station building, external lift shaft, footbridge with lift shafts, extension of platforms 2/3 and 4/5 with canopy modifications, new waiting room to Platform 4/5 and associated lighting.

These construction arrangements contribute to improving the linkages within London and the overall regeneration objectives of the area in compliance with London Plan (July 2011) Policies, and it is recommended that the Plans and Specification submission for permanent works under Crossrail Act 2008, Schedule 7 be approved.

2. RECOMMENDATION

APPROVAL subject to the following:

INFORMATIVES

1

1. Induction loops should be specified to comply with BS 7594 and BS EN 60118-4, and a term contract planned for their maintenance.

2. Care must be taken to ensure that overspill and/or other interference from induction loops in different/adjacent areas does not occur.

3. Flashing beacons/strobe lights linked to the fire alarm should be carefully selected to ensure they remain within the technical thresholds not to adversely affect people with epilepsy.

2

We expect that as part of the development there should be access and signage improvements to enhance the connectivity between the station and the towpath.

3. CONSIDERATIONS

3.1 Site and Locality

Hayes and Harlington Station is located on Station Road to the south of Hayes Town Centre. The railway line runs east to west with the current station building located on the Station Road bridge, which crosses the railway running north to south. Access to Platforms 4 and 2/3 is gained from two sets of staircases within the Station Building.

Access to Platform 1 is gained via a staircase directly from Station Road located immediately south of the bridge parapet on the eastern flank. This staircase also provides access to the station car park. A further footbridge, to the eastern end of the station, connects Platforms 2/3 and 4/5. In addition, a subway runs from Blyth Road to the west, under Station Road, and provides further access to Station Approach.

Hayes and Harlington Station contains five Platforms. Platform 1 is the southernmost platform, followed by island Platform 2/3. The majority of Platform 4 is a single faced Platform with the remainder comprising an island platform with Platform 5.

A secondary station building is located on Platform 4 and contains a currently unused ground level platform access.

The Platform 4 canopy is an extension of the secondary station building roof. Platform 2/3 is served by a canopy extending between the eastern access staircase and existing ticket office. Within this canopy is located a platform waiting room. There is no canopy on platform 1.

The station building is locally listed and Nestles Conservation Area is located to the south of the site.

The surrounding area is mixed use in character with a mix of residential, commercial, industrial and office uses in the immediate vicinity of the site.

3.2 Proposed Scheme

The Crossrail Act 2008 provides powers for the construction and operation of Crossrail. Schedule 1 of the Act describes time 'scheduled works' that the nominated undertaker will be authorised to carry out.

The Crossrail Act gives deemed planning permission for the proposed works, subject to conditions imposed by Section 10 and Schedule 7 of the Act, which requires plans and specifications for the operations and works and construction arrangements to be approved by the Local Planning Authority. It sets out the items requiring further detailed approval for both permanent works (referred to as 'plans and specification') and temporary works (referred to as construction arrangements).

This application includes information supporting the Plans and Specifications submission in relation to a number of matters as follows:

- Construction works;
- Minor Construction Works;
- Artificial Lighting;
- Fences and Walls.

The permanent works required with this submission are as follows:

1. New Station Building

It is proposed to demolish the existing commercial building and construct a new station building at midlevel between Station Road and Station Approach. The forecourt and concourse will be at street level with Station Road with steps providing access up to this level from Station Approach. The building will be approximately 10 metres in height above Station Approach and 6.7 metres above Station Road.

2. New External Lift Shaft

An 8.4 metre high external lift shaft is proposed on the eastern elevation of the new building, which will be constructed from glass panels on the north and south elevations, and anodized metal panels on the east and west elevations. This has been incorporated into the scheme so as to provide a covered step-free access route from Station Approach to forecourt level.

3. New Footbridge with lift shafts

A new footbridge is proposed which extends 52 metres in length from the main station building across to Platform 1. These will be constructed in a similar style to that of the new station building with glazed windows along the upper sections to allow natural light to permeate the structure.

4. Platform extensions

- Platform 2/3 will be extended by 76.7m along both faces and includes maintenance access steps for staff.

- Platform 4/5 will be extended by 81.9m and widened to create an island platform of 237.0m in length.

5. New Platform 4/5 Canopy and Waiting Room

It is proposed to remove and replace the canopy and building on Platform 4. The new canopy structure will extend 58 metres in length from the new footbridge stairs along the length of the platform. The requirement for new canopies and waiting room is derived from the need to widen and expand the length of platform 4/5 to facilitate the longer Crossrail trains.

6. Modification of Platform 2/3 Canopy

As a result of the positioning of the new Platform 2/3 staircase, it is necessary to cutback the existing canopy. The canopy will be cutback by 15 metres and reinstated to provide 28 metres of coverage along the platform between the new and existing footbridges.

7. New Lighting

This will be installed along the platform extensions, canopies, new footbridge and staircases and within the new station building and canopy. Platform lighting will consist of a combination of 5 metre tall single and twin headed columns located at 6m centres. Around the station, there will be two way mounted luminaires on the external canopy columns and recessed uplighters set along the main building line internally and externally.

3.3 Relevant Planning History

10057/APP/2011/2478 Hayes And Harlington Station Station Road Hayes

Application under Schedule 7 of the Crossrail Act 2008 for approval of Construction Arrangements in respect of handling or re-use of spoil and top soil, storage sites, screening, artificial lighting, suppression of dust and measures to prevent mud on highway. The Construction Arrangements be associated with the construction of an extension to the southern most rail platform at the existing Hayes and Harlington rail station.

Decision: 07-12-2011 Approved

10057/APP/2011/2479 Hayes And Harlington Station Station Road Hayes

Application under Schedule 7 of the Crossrail Act 2008 for approval of plans and specifications for the construction of a platform extension at Hayes & Harlington Station and associated installation of fencing and lighting.

Decision: 19-01-2012 Approved

10057/APP/2014/1058 Hayes & Harlington Station Station Approach Hayes

Application under Schedule 7 of the Crossrail Act 2008 for approval of Construction Arrangements in association with Hayes and Harlington Station. The Construction Arrangements to be associated with the demolition of the existing commercial buildings on Station Road and the existing secondary station building, and construction of a new station building and canopy modifications.

Decision: 20-05-2014 Approved

Comment on Relevant Planning History

There have been a number of submissions under the Crossrail Act for associated works, which are listed above.

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

- PT1.BE1 (2012) Built Environment
 PT1.T2 (2012) Public Transport Interchanges

Part 2 Policies:

- AM13 AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): -
 (i) Dial-a-ride and mobility bus services
 (ii) Shopmobility schemes
 (iii) Convenient parking spaces
 (iv) Design of road, footway, parking and pedestrian and street furniture schemes
- BE13 New development must harmonise with the existing street scene.
- LPP 2.8 (2011) Outer London: Transport
- LPP 5.3 (2011) Sustainable design and construction
- LPP 6.2 (2011) Providing public transport capacity and safeguarding land for transport
- LPP 7.14 (2011) Improving air quality
- LPP 7.15 (2011) Reducing noise and enhancing soundscapes
- LPP 7.19 (2011) Biodiversity and access to nature
- LPP 6.4 (2011) Enhancing London's Transport Connectivity
- LPP 7.4 (2011) Local character
- LPP 7.5 (2011) Public realm
- LPP 7.6 (2011) Architecture
- NPPF1 NPPF - Delivering sustainable development
- NPPF4 NPPF - Promoting sustainable transport
- NPPF7 NPPF - Requiring good design
- OE1 Protection of the character and amenities of surrounding properties and the local area
- R16 Accessibility for elderly people, people with disabilities, women and children

5. Advertisement and Site Notice

- 5.1** Advertisement Expiry Date:- **2nd February 2015**
- 5.2** Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

649 residents and businesses were consulted on the application and four responses were received. The comments made are as follows:

1. Serious concerns with regards to vehicle access to Station Approach;
2. The plans show that the roundabout will be relocated to outside the High Point Village car park

- which will reduce the amount of space for vehicles;
3. Four spaces outside Tesco Express is not enough;
 4. Station Approach is not suitable for a drop off area due to the lack of space and at rush hour it becomes very congested and noisy;
 5. Taxi company park in the short term drop off zones and on the pavement/access point to Station Approach, which worsens the problems. This will only get worse with Crossrail;
 6. Upkeep of the area and lighting needs to be carefully considered and designated so that the area is properly maintained;
 7. The PA system/speakers need to be carefully considered so as to avoid disturbance to residents;
 8. Object to the demolition of the historic 19th Century building alongside Platform 4 and more attempt should be made to retain this as part of the new development rather than reusing the bricks.

SUPPORT

1. Excellent for progress and job creation;
2. No objection to the plans provided there is no disruption during construction
3. The site restoration scheme needs to be carefully agreed and should look at promoting pedestrianisation and cycling and more greening of the area to absorb noise;

NATURAL ENGLAND

No objection as the proposal does not affect any priority areas for Natural England.

ENVIRONMENT AGENCY

No objections or comments

CANAL AND RIVERS TRUST

No objection to the proposed new station but have the following comments to make: Given the proximity to the Grand Union Canal, we would anticipate both pedestrians and cyclists to make use of the towpath as part of their commute getting to and from the station. We would therefore expect that as part of the development there should be access and signage improvements to enhance the connectivity between the station and the towpath. If any funding is made available for access enhancements as part of the upgrade, please contact the Canal and River Trust so that the Trust can work proactively with other key stakeholders.

TfL

TfL are unable to provide a response at this time but will endeavour to respond as soon as possible.

Internal Consultees

TREES AND LANDSCAPING

LANDSCAPE CHARACTER / CONTEXT:

Site description:

- The site is occupied by Hayes & Harlington Station, which is approached from the west via Station Road and from the north by Station Approach.
- The railway line runs approximately east-west, with Hayes Town Centre situated to the north.
- A recent development of flats is situated directly to the north, occupying the former goods yard, beyond which is the Grand Union Canal.
- Land to the south of the railway is occupied by industrial / commercial development, with residential streets beyond.
- Due to the operational nature of the railway there are no trees or landscape enhancements within the curtilage of the railway.

Landscape Planning designations:

- There are no Tree Preservation Orders and no Conservation Area designations directly affecting the site.
- The Nestle Factory to the south-east of the railway lies within the Botwell Nestle Conservation Area.

PROPOSAL:

The proposal is an application under Schedule 7 of the Crossrail Act 2008 for the approval of Plans and Specifications associated with the erection of a new station building, external lift shaft, footbridge with lift shafts, extension of platforms 2/3 and 4/5 with canopy modifications, new waiting room to Platform 4/5 and associated lighting.

LANDSCAPE CONSIDERATIONS:

Saved policy BE38 seeks the retention and utilisation of topographical and landscape features of merit and the provision of new planting and landscaping wherever it is appropriate.

- No trees or other landscape features of merit will be affected by the proposal.
- Some indicative tree planting and bike storage is shown on drawing No. WSN1C_EAR_DRG_BEN_000251 Rev P.02.
- According to the Design & Access Statement, section 2 Design Rationale confirms that 'the landscaping and non-structural works within the cartilage are shown for information only and not for approval as part of this plans and specification submission'.
- It goes on to say that 'these matters will be contained in the Site Restoration Scheme, which will be agreed with the London Borough of Hillingdon at a later date'.
- In section 3, the design aims for the Emerging Public Realm Plan are set out.
- If the application is recommended for approval, it is understood that proposals for the enhancement of the public realm (hard and soft landscape) will follow. These should be complement the high profile design concept promoted by Crossrail.

RECOMMENDATIONS:

This application has been subject to pre-application discussions with planning and specialist officers.

It is understood that public realm improvements associated with the station development will follow.

ACCESS

Planning permission is sought to construct a new Hayes & Harlington Station to support the implementation of the new Crossrail train service. The work includes an external lift shaft with a feature lift, a new footbridge with lift, as well as extension and modifications to the platforms. An extension, new canopy and a waiting room is also proposed for platform 4 & 5.

The new station building is to be located North of the current building in place of the commercial building. Level access into the station from Station Road is an integral part of the design, with lift access into the station provided from Station Approach. Access for older and disabled people, and for those with young children, to all platforms would be possible via a series of lift and footbridge arrangements.

This application has been the subject of detailed pre-planning discussions and has undergone a number of iterations prior to this submission. The proposal is therefore fundamentally acceptable from an accessibility perspective, however, the following minor alteration is requested:

- 1.The artist's impression illustrates a handrail on the left when facing the Station Road entrance. An additional handrail should also be provided on the right (facing).
- 2.An alternative to minimise the risk of slips and trips, particularly by people with a visual impairment, the outer edge of the steps (where they abut the level pavement) on the Station Road entrance should ensure a contrast of no less than 30 points Light Reflectance Value (LRV).

Recommended Informatives

- 1.Induction loops should be specified to comply with BS 7594 and BS EN 60118-4, and a term

contract planned for their maintenance.

2.Care must be taken to ensure that overspill and/or other interference from induction loops in different/adjacent areas does not occur.

3.Flashing beacons/strobe lights linked to the fire alarm should be carefully selected to ensure they remain within the technical thresholds not to adversely affect people with epilepsy.

Conclusion:

No objection, subject to the above observations

ENVIRONMENTAL PROTECTION UNIT

No objection

7. MAIN PLANNING ISSUES

7.01 The principle of the development

The Crossrail Act 2008 granted the principle of a cross route railway and gives deemed planning permission for the proposed works, subject to conditions imposed by Section 10 and Schedule 7 of the Act, which requires plans and specifications for the operations and works to be approved by the Local Planning Authority.

The extent of the deemed planning permission is determined by works covered by Schedules 1 and 2 of the Act and by the limits set out in the deposited Parliamentary approved plans. The Act also provides for the scheduled works to be constructed on land within the lateral limits of deviation (LoD) shown on the deposited plans and sections and provides for an upward limit of deviation.

Schedule 7 to the Act defines the detailed planning regime which applies to the Local Planning Authority (LPA) affected by works to construct and operate Crossrail. It sets out the items requiring further detailed approval for both permanent works and construction arrangements. It also sets out the grounds in which the LPA can impose conditions. Schedule 7 is a completely separate regime to that of the Town and Country Planning Act in terms of submission details and content. Furthermore, Schedule 7 sets out the matters or operations or works that require further detailed approval from the LPA for both permanent and temporary works.

Applications made under the Crossrail Act 2008 are different from planning applications made under the Town and Country Planning Act as they have received deemed approval through Royal Assent, subject to certain details for Local Planning Authority's approval. Thus, Local Planning Authority's powers in deciding applications made under the Schedules are limited by the provisions of the Act. The Council signed up to become a 'Qualifying Authority' to have powers to consent to a wider range of detailed matters made under Schedule 7 (Planning Conditions Schedule) of the Act.

A Qualifying Authority may refuse to approve an application for 'reserved' matters as well as details of conditions that can be imposed upon applications recommended for approval, subject to the conditions set out under Schedule 7 of the Act. A Qualifying Authority can only refuse or impose conditions on a Schedule 7 application on the basis of grounds specified in the schedule, which is;

'that arrangements ought to be modified to preserve the local environment or local amenity or to reduce the prejudicial effects on road safety or on the free flow of traffic in the local area and are reasonably capable of being modified'.

The Environmental Statement (ES) deposited with the Crossrail Bill identifies the likely significant effects that will arise from the construction and operation of Crossrail, and identifies the range of mitigation measures that could be used to reduce or eliminate these effects. It serves as the Environmental Impact Assessment of the project and has been updated through the passage of the Bill in the Houses of Parliaments. The Crossrail Act made provisions that all applications cannot exceed the impacts assessed by the ES. It is the intention of the Secretary of State to carry out the project so that its impacts are as assessed in the ES.

Given such, the principle of this development has been established.

7.02 Density of the proposed development

Not applicable to the consideration of this application.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

Hayes and Harlington Station is a Locally Listed Building and it is proposed to demolish this in order to facilitate the development. Whilst the loss of the buildings are regrettable, given the alterations required to make improvements to the access, platforms, canopies and modernisation of the station, and accommodate the Crossrail trains in general, it has not been possible to retain these. The historic wall fronting Station Approach is however going to be carefully demolished and the bricks re-used to construct the crash wall along the northern part of the site. No objection is raised to this, and this proposal is considered to preserve part of the heritage and historic appearance of the site.

Nestles Conservation Area is located to the south of the application site. Given the acceptable design and scale of the proposed station building and associated platform alterations, footbridges and buildings, the proposals are not considered to have a detrimental impact on the character and appearance of the adjacent Conservation Area.

7.04 Airport safeguarding

Not applicable to the consideration of this application.

7.05 Impact on the green belt

Not applicable to the consideration of this application.

7.07 Impact on the character & appearance of the area

The station building will be constructed on a raised plinth approximately 3.2 metres above ground level and at the same level as Station Road. A new canopy is proposed to extend above the forecourt on Station Road and will be approximately 6.7 metres above the ground level.

The building is set away from the High Point Village development to the east and extends to a height of 10 metres above Station Approach. A partially glazed lift shaft is proposed at this level also to provide access to the station buildings/platforms.

In terms of the layout and siting of the proposed station, the proposed building provides a continuation from the established pattern of development within this part of Station Road and Station Approach, and its overall design, scale and orientation towards the Town Centre and Blyth Road, ensures that the building is visible within the surrounding area. Whilst the new building would be prominent in view from the surrounding street scape, it is considered that the overall size and scale of the proposed building would be appropriate for its setting and not detract from the character and appearance of more recent developments within the surrounding area.

The building would incorporate an active glazed frontage onto Station Road and is considered to enhance and complement the appearance of the surrounding area through its associated improvements. The largely glazed design of the building will allow views through the station to surrounding areas, and this design approach and proposed material palette, is not out of keeping with the evolving development in the surrounding street scene. Overall, the design of the station buildings are considered to enhance this part of Hayes Town Centre and to complement the existing recent redevelopments and long term plans for Hayes Town.

The amendments to the platforms, footbridges, canopies and ancillary station buildings are also considered to be of an acceptable design and scale, and given their location and the level differences of the site, are not deemed to have a detrimental impact on the character and appearance of the surrounding area.

7.08 Impact on neighbours

In terms of the impact of the proposal on neighbouring residents, the design, scale and siting of the buildings proposed, are not considered to have a detrimental impact on the amenities of the surrounding occupiers.

Concerns have been raised with regards to the noise from the station announcements. Crossrail have been advised of these comments and will provide a response on these matters, which will be included within the addendum to the application.

7.09 Living conditions for future occupiers

Not applicable to the consideration of this application.

7.10 Traffic impact, Car/cycle parking, pedestrian safety

Concerns have been raised with regards to the traffic congestion within Station Approach, its suitability for use in conjunction with the rail station, the lack of drop off parking spaces and proposed relocation of the roundabout.

This submission is solely to consider the proposed new station buildings, platform and canopy alterations. Works to improve the public realm, integration of transport modes, pedestrian environment, link to the town centre, cycle access and parking form part of an emerging public realm plan. This plan, which is aimed at the surrounding public realm and highways network is being designed jointly by stakeholders from the London Borough of Hillingdon, Crossrail and TfL.

The Council has made Crossrail and TfL aware of the comments in relation to the surrounding road network and it is hoped that these will be addressed within the wider plans for the area/Site Restoration Scheme.

7.11 Urban design, access and security

See section 7.07 'Impact on the Character and Appearance of the Area'

ACCESS

In terms of access to the site, this will still be retained from both Station Approach and Station Road. Significant improvements are proposed to the accessibility of the site for all users, to ensure that this is step free from both routes into the station and also to generally improve the movement in and around the station. No objections are raised to the improvements proposed, which are considered to improve the accessibility of the area.

7.12 Disabled access

The new station building is to be located further north of the current building, in place of the commercial building. Level access into the station from Station Road is an integral part of

the design, with lift access into the station provided from Station Approach. Access for older and disabled people, and for those with young children, to all platforms would be possible via a series of lift and footbridge arrangements.

The Access Officer has reviewed the application and raises no objection to the access arrangements proposed.

7.13 Provision of affordable & special needs housing

Not applicable to the consideration of this application.

7.14 Trees, landscaping and Ecology

No trees or other landscape features of merit will be affected by the proposal. The scheme has been reviewed by the Councils Trees and Landscape Officer who raises no objection to the construction of the new station buildings, platforms and canopies.

The wider public realm improvements do not form part of this application and will be submitted as part of a Site Restoration Scheme.

7.15 Sustainable waste management

Not applicable to the consideration of this application.

7.16 Renewable energy / Sustainability

Not applicable to the consideration of this application.

7.17 Flooding or Drainage Issues

Not applicable to the consideration of this application.

7.18 Noise or Air Quality Issues

Concerns have been raised with regards to the noise from station announcements and whether the new station design will improve the acoustics so that residents in High Point Village are not disturbed by these.

These concerns have been referred to Crossrail who will be providing a response on this matter. This will be reported in the addendum to the committee report.

7.19 Comments on Public Consultations

The public consultation issues and comments have been addressed in the main body of the report.

7.20 Planning obligations

Not applicable to the consideration of this application.

7.21 Expediency of enforcement action

Not applicable to the consideration of this application.

7.22 Other Issues

There are no other issues for consideration with this application.

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the

application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

Not applicable to the consideration of this application.

10. CONCLUSION

The principle of the development has been established under the Crossrail Act 2008, however Schedule 7 requires the Local Planning Authority to approve details in relation to the erection of a new station building, external lift shaft, footbridge with lift shafts, extension of platforms 2/3 and 4/5 with canopy modifications, new waiting room to Platform 4/5 and

associated lighting.

These construction arrangements contribute to improving the linkages within London and the overall regeneration objectives of the area in compliance with London Plan (July 2011) Policies, and it is recommended that the Plans and Specification submission for permanent works under Crossrail Act 2008, Schedule 7 be approved.

11. Reference Documents

Crossrail Act 2008

Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012)

Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012)

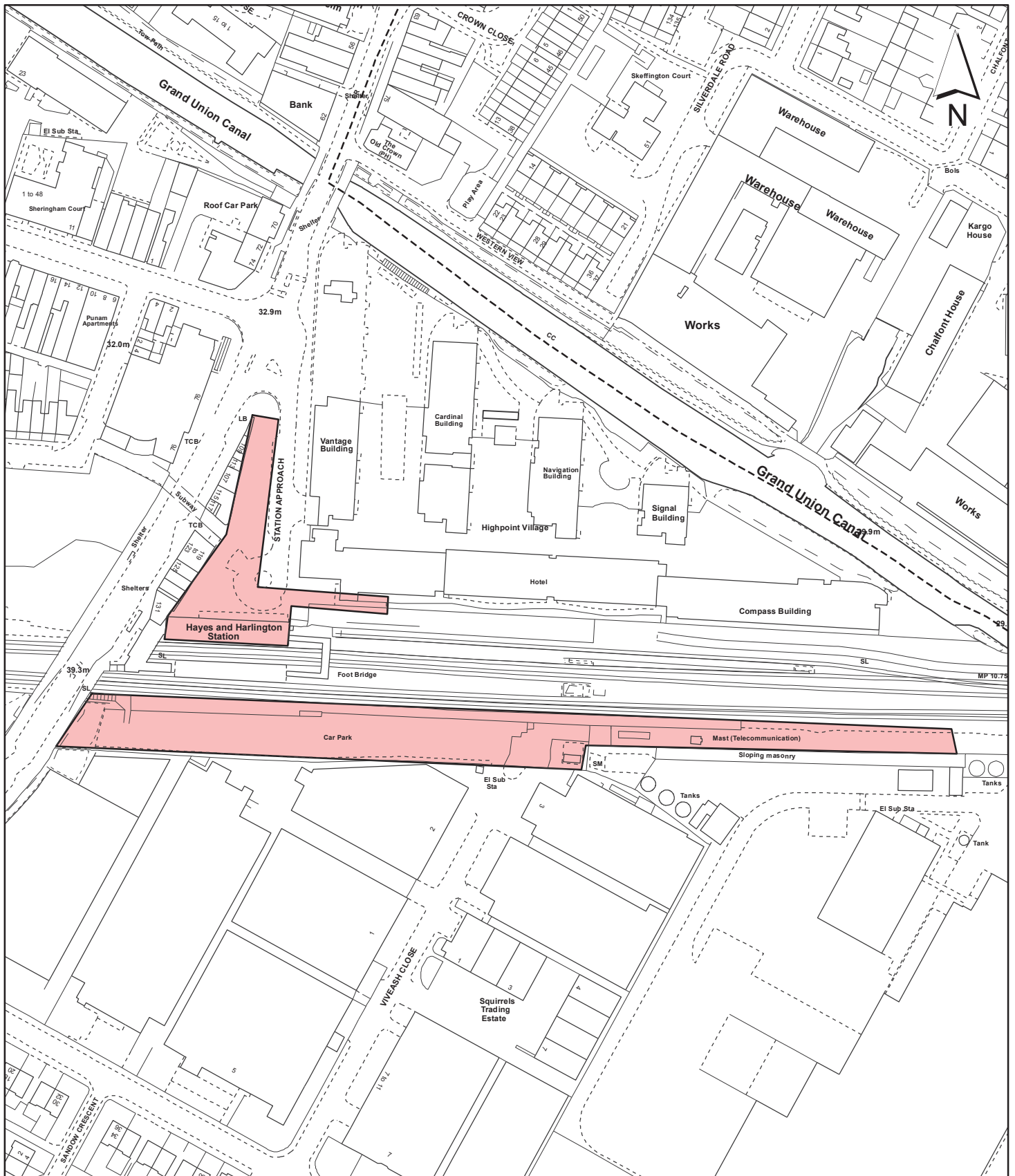
London Plan (July 2011)

National Planning Policy Framework

Hillingdon Supplementary Planning Document - Noise

Contact Officer: Charlotte Bath

Telephone No: 01895 250230



Notes

 Site boundary

For identification purposes only.

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Site Address

**Hayes and Harlington Station
Station Road
Hayes**

**LONDON BOROUGH
OF HILLINGDON**

**Residents Services
Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 250111

Planning Application Ref:

10057/APP/2014/4338

Scale

1:2,000

Planning Committee

Major Page 84

Date

March 2015



HILLINGDON
LONDON

Report of the Head of Planning, Sport and Green Spaces

- Address** PRONTO INDUSTRIAL ESTATE AND 585 - 591 UXBRIDGE ROAD HAYES
- Development:** Change of use of B1(c) floorspace to provide 12 additional residential units and associated ancillary works. (Amendment to planning permission ref: 4404/APP/2011/2079, dated 30-03-2012 (Application to replace extant planning permission ref: 4404/APP/2008/3558, dated 23-03-2009); Redevelopment of site to provide replacement Class B1(c) light industrial space and 34 two-bedroom and 9 one-bedroom flats with associated car parking, landscaping and amenity space).
- LBH Ref Nos:** 4404/APP/2014/2506
- Drawing Nos:** Noise Insulation Scheme
Email re sound insulation dated 29-08-14
Economic Viability Assessment (August 2014)
PL-01(S4) Site Plan
PL-100 Site Location Plan
PL-201 Proposed Ground Floor Plan and Elevations Block C
PL-202 Proposed Ground Floor Plan and Elevations Block D
PL-203 Proposed Ground Floor Plan and Elevations Block A2
PL-204 Proposed Ground Floor Plan and Elevations Block B
Design and Access Statement
Report on the Demand for Commercial Space on the Pronto Trading Estate

Date Plans Received: 16/07/2014

Date(s) of Amendment(s):

Date Application Valid: 17/07/2014

1. SUMMARY

The application site currently benefits from planning permission ref: 4404/APP/2011/2079, dated 30-03-2012 of the redevelopment of the site to provide replacement Class B1(c) light industrial space and 34 two-bedroom and 9 one-bedroom flats (Application to replace extant planning permission ref: 4404/APP/2008/3558, dated 23-03-2009).

This application seeks full planning permission to allow the change of use of the B1(c) floor space to 12 additional residential units (C3) and associated ancillary works.

The proposed change of use from B1(c) floor space to 12 residential units is considered to be acceptable in principle. The proposal would provide adequate amenity space and accord with the London Plan (2011) internal floor space standards.

It is considered that the proposed additional units would not have a detrimental impact on the character of the surrounding area or on the residential amenity of future occupiers of the site or neighbouring properties.

The proposal complies with Policies AM7, AM14, BE13, BE15, BE19, BE20, BE23, BE24 and LE4 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and the London Plan Policies (July 2011). It is therefore recommended that planning permission is approved.

2. RECOMMENDATION

1. That delegated powers be given to the Head of Planning and Enforcement to grant planning permission subject to the relevant conditions set out below:

A) The Council enters into an agreement with the applicant under Section 106 of the Town and Country Planning Act 1990 (as amended) and/or other appropriate legislation to secure:

1. **Affordable Housing:** A financial contribution of £120,000 towards off-site affordable housing.

2. **Project Management and Monitoring Fee:** A contribution equal to 5% of the total cash contributions is required to ensure the adequate management and monitoring of the resulting agreement.

B) That in respect of the application for planning permission, the applicant meets the Council's reasonable costs in preparation of the legal agreement and any abortive work as a result of the agreement not being completed.

C) That officers be authorised to negotiate and agree the detailed terms of the proposed agreement and conditions of approval.

D) If the Legal Agreement/s have not been finalised within 3 months, delegated authority be given to the Head of Planning and Enforcement to refuse planning permission for the following reason:

"The applicant has failed to provide measures to mitigate the impacts of the development through enhancements to services and the environment necessary as a consequence of demands created by the proposed development in respect of affordable housing. The proposal therefore conflicts with Policy R17 of the Hillingdon Local Plan: Part Two - Saved UDP Policies'.

E) That subject to the above, the application be deferred for determination by the Head of Planning and Enforcement under delegated powers, subject to the completion of the legal agreement under Section 106 of the Town and Country Planning Act 1990 and other appropriate powers with the applicant.

F) That if the application is approved, the following conditions be imposed subject to any changes negotiated by the Head of Planning and Enforcement prior to issuing the decision:

1 RES3 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990

2 RES4 Accordance with Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans,

PL-01(S4) Site Plan
PL-100 Site Location Plan
PL-201 Proposed Ground Floor Plan and Elevations Block C
PL-202 Proposed Ground Floor Plan and Elevations Block D
PL-203 Proposed Ground Floor Plan and Elevations Block A2
PL-204 Proposed Ground Floor Plan and Elevations Block B

and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and the London Plan (July 2011).

3 RES7 Materials (Submission)

No development shall take place until details of all materials and external surfaces, including details of balconies, have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with Policy BE13 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012)

4 RES9 Landscaping (car parking & refuse/cycle storage)

No development shall take place until a landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include: -

1. Details of Soft Landscaping
 - 1.a Planting plans (at not less than a scale of 1:100),
 - 1.b Written specification of planting and cultivation works to be undertaken,
 - 1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate

2. Details of Hard Landscaping
 - 2.a Refuse Storage
 - 2.b Cycle Storage
 - 2.c Means of enclosure/boundary treatments
 - 2.d Car Parking Layouts (including demonstration that 20% of all parking spaces are served by electrical charging points)
 - 2.e Hard Surfacing Materials
 - 2.f External Lighting
 - 2.g Other structures (such as play equipment and furniture)

3. Details of Landscape Maintenance
 - 3.a Landscape Maintenance Schedule for a minimum period of 5 years.
 - 3.b Proposals for the replacement of any tree, shrub, or area of surfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.

4. Schedule for Implementation

5. Other

5.a Existing and proposed functional services above and below ground

5.b Proposed finishing levels or contours

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies BE13, BE38 and AM14 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Policies 5.11 (living walls and roofs) and 5.17 (refuse storage) of the London Plan (July 2011).

5 RES10 Tree to be retained

Trees, hedges and shrubs shown to be retained on the approved plan(s) shall not be damaged, uprooted, felled, lopped or topped without the prior written consent of the Local Planning Authority. If any retained tree, hedge or shrub is removed or severely damaged during (or after) construction, or is found to be seriously diseased or dying, another tree, hedge or shrub shall be planted at the same place or, if planting in the same place would leave the new tree, hedge or shrub susceptible to disease, then the planting should be in a position to be first agreed in writing with the Local Planning Authority and shall be of a size and species to be agreed in writing by the Local Planning Authority and shall be planted in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier. Where damage is less severe, a schedule of remedial works necessary to ameliorate the effect of damage by tree surgery, feeding or groundwork shall be agreed in writing with the Local Planning Authority. New planting should comply with BS 3936 (1992) 'Nursery Stock, Part 1, Specification for Trees and Shrubs'

Remedial work should be carried out to BS BS 3998:2010 'Tree work - Recommendations' and BS 4428 (1989) 'Code of Practice for General Landscape Operations (Excluding Hard Surfaces)'. The agreed work shall be completed in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier.

REASON

To ensure that the trees and other vegetation continue to make a valuable contribution to the amenity of the area in accordance with policy BE38 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and to comply with Section 197 of the Town and Country Planning Act 1990.

6 RES16 Code for Sustainable Homes

The dwelling(s) shall achieve Level 4 of the Code for Sustainable Homes. No development shall commence until a signed design stage certificate confirming this level has been received. The design stage certificate shall be retained and made available for inspection by the Local Planning Authority on request.

The development must be completed in accordance with the principles of the design stage certificate and the applicant shall ensure that completion stage certificate has been attained prior to occupancy of each dwelling.

REASON

To ensure that the objectives of sustainable development identified in London Plan (July 2011) Policies 5.1 and 5.3.

7 RES18 Lifetime Homes/Wheelchair Units

All residential units within the development hereby approved shall be built in accordance with 'Lifetime Homes' Standards. A minimum of one of the units hereby approved shall be designed and constructed to be fully wheelchair accessible or easily adaptable for residents who are wheelchair users, as set out in the Council's Supplementary Planning Document 'Accessible Hillingdon'.

REASON

To ensure that sufficient housing stock is provided to meet the needs of disabled and elderly people in accordance with London Plan (July 2011) Policies 3.1, 3.8 and 7.2

8 RES22 Parking Allocation

No unit hereby approved shall be occupied until a parking allocation scheme has been submitted to, and approved in writing by, the Local Planning Authority. Thereafter the parking shall remain allocated for the use of the units in accordance with the approved scheme and remain under this allocation for the life of the development.

REASON

To ensure that an appropriate level of car parking provision is provided on site in accordance with Policy AM14 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and Chapter 6 of the London Plan (July 2011).

9 RES24 Secured by Design

The dwelling(s) shall achieve 'Secured by Design' accreditation awarded by the Hillingdon Metropolitan Police Crime Prevention Design Adviser (CPDA) on behalf of the Association of Chief Police Officers (ACPO). No dwelling shall be occupied until accreditation has been achieved.

REASON

In pursuance of the Council's duty under section 17 of the Crime and Disorder Act 1998 to consider crime and disorder implications in exercising its planning functions; to promote the well being of the area in pursuance of the Council's powers under section 2 of the Local Government Act 2000, to reflect the guidance contained in the Council's SPG on Community Safety By Design and to ensure the development provides a safe and secure environment in accordance with London Plan (July 2011) Policies 7.1 and 7.3.

10 RES17 Sound Insulation

Development shall not begin until a scheme for protecting the proposed development from road traffic noise has been submitted to and approved in writing by the Local Planning Authority. All works which form part of the scheme shall be fully implemented before the development is occupied and thereafter shall be retained and maintained in good working order for so long as the building remains in use.

REASON

To ensure that the amenity of the occupiers of the proposed development is not adversely affected by (road traffic) (rail traffic) (air traffic) (other) noise in accordance with policy OE5 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and London Plan (July 2011) Policy 7.15.

11 NONSC High Level Windows

All windows indicated to be high level windows on the approved plans shall be positioned so that the bottom sill is a minimum of 1.7m above final floor levels.

REASON

To prevent overlooking to adjoining properties in accordance with policy BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012)

INFORMATIVES

1 I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 I53 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

AM7	Consideration of traffic generated by proposed developments.
AM14	New development and car parking standards.
BE13	New development must harmonise with the existing street scene.
BE15	Alterations and extensions to existing buildings
BE19	New development must improve or complement the character of the area.
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
LE4	Loss of existing industrial floorspace or land outside designated Industrial and Business Areas
HDAS-LAY	Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006
LPP 3.11	(2011) Affordable housing targets
LPP 3.12	(2011) Negotiating affordable housing (in) on individual private residential and mixed-use schemes
LPP 3.5	(2011) Quality and design of housing developments
NPPF	National Planning Policy Framework

3 I15 Control of Environmental Nuisance from Construction Work

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between

the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance 'The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

4 I43 Keeping Highways and Pavements free from mud etc

You are advised that care should be taken during the building works hereby approved to avoid spillage of mud, soil or related building materials onto the pavement or public highway. You are further advised that failure to take appropriate steps to avoid spillage or adequately clear it away could result in action being taken under the Highways Act 1980.

5 I47 Damage to Verge - For Council Roads:

The Council will recover from the applicant the cost of highway and footway repairs, including damage to grass verges.

Care should be taken during the building works hereby approved to ensure no damage occurs to the verge or footpaths during construction. Vehicles delivering materials to this development shall not override or cause damage to the public footway. Any damage will require to be made good to the satisfaction of the Council and at the applicant's expense.

For further information and advice contact - Highways Maintenance Operations, Central Depot - Block K, Harlington Road Depot, 128 Harlington Road, Hillingdon, Middlesex, UB3 3EU (Tel: 01895 277524).

6

You are advised that the development hereby approved represents chargeable development under the Mayor's and Hillingdon's Community Infrastructure Levy (CIL). The actual Community Infrastructure Levy will be calculated at the time your development is first permitted and a separate liability notice will be issued by the Local Planning Authority. Should you require further information please refer to the Council's Website www.hillingdon.gov.uk/index.jsp?articleid=24738

3. CONSIDERATIONS

3.1 Site and Locality

The application site is located on the southern side of Uxbridge Road, Hayes. 593-595 Uxbridge Road is located to the west of the site with 583 Uxbridge Road to the east. At the rear of the site, to the east, is Rosedale Park and allotment gardens are located to the south and west.

3.2 Proposed Scheme

The application site currently benefits from planning permission for a mixed residential and commercial use located within seven blocks (ref: 4404/APP/2011/2079) (Application to replace extant planning permission ref: 4404/APP/2008/3558, dated 23-03-2009). Blocks A2, C and D comprise of B1(c) floor space on the ground floors and residential units on the upper floors, whilst Block B solely provides B1(c) floor space. Blocks A1, E and F are for residential use only.

This current application seeks to vary the previous planning permission to allow for the change of use of the B1(c) floor space in Blocks A2, B, C and D to 12 additional residential units (C3), along with associated ancillary works.

There would be no change to Blocks A1, E and F.

3.3 Relevant Planning History

4404/APP/2008/3558 Pronto Industrial Estate And 585 - 591 Uxbridge Road Hayes

Demolition of existing buildings and redevelopment of site to provide replacement Class B1(c) light industrial space and 34 two-bedroom and 9 one-bedroom flats with associated car parking, landscaping and amenity space.

Decision: 23-03-2009 Approved

4404/APP/2011/2079 Pronto Industrial Estate And 585 - 591 Uxbridge Road Hayes

Application to replace an extant planning permission ref: 4404/APP/2008/3558 dated 23/03/2009. Demolition of existing buildings and redevelopment of site to provide replacement Class B1(c) light industrial space and 34 two-bedroom and 9 one-bedroom flats with associated car parking, landscaping and amenity space.

Decision: 14-02-2012 Approved

4404/APP/2013/1650 Pronto Industrial Estate And 585 - 591 Uxbridge Road Hayes

Part 2 Part 3 storey building to be used for light industrial (Use Class B1(c)) purposes (revised design of Block B approved in 4404/APP/2011/2079).

Decision: 17-06-2014 Approved

4404/PRC/2014/18 Pronto Industrial Estate And 585 - 591 Uxbridge Road Hayes

Change of use of B1(c) floor space into 12 residential units (C3)

Decision: 17-06-2014 NO

Comment on Relevant Planning History

Planning permission for Class B1(c) light industrial space and 34 two-bedroom and 9 one-bedroom flats was granted in March 2012 under planning application ref: 4404/APP/2011/2079. This permission replaced a previous planning permission (ref: 4404/APP/2008/3558), granted in March 2009, for the same development. The current approved 43 unit scheme and the proposed 12 unit scheme would result in a total of 55 residential units on the site.

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

PT1.BE1 (2012) Built Environment

Part 2 Policies:

AM7 Consideration of traffic generated by proposed developments.

AM14 New development and car parking standards.

BE13 New development must harmonise with the existing street scene.

BE15 Alterations and extensions to existing buildings

BE19 New development must improve or complement the character of the area.

BE20 Daylight and sunlight considerations.

BE21 Siting, bulk and proximity of new buildings/extensions.

BE23 Requires the provision of adequate amenity space.

BE24 Requires new development to ensure adequate levels of privacy to neighbours.

LE4 Loss of existing industrial floorspace or land outside designated Industrial and Business Areas

HDAS-LAY Residential Layouts, Hillingdon Design & Access Statement, Supplementary Planning Document, adopted July 2006

LPP 3.11 (2011) Affordable housing targets

LPP 3.12 (2011) Negotiating affordable housing (in) on individual private residential and mixed-use schemes

LPP 3.5 (2011) Quality and design of housing developments

NPPF National Planning Policy Framework

5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- **2nd September 2014**

5.2 Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

Consultation letters were sent to 20 local owners/occupiers and a site notice was displayed. No objections have been received.

One letter of support has been received, making the following comments:

- i) commercial developments should not be among residential properties
- ii) there is a need for housing
- iii) there should be tree screening to prevent overlooking to neighbouring property

NATS:

The proposed development has been examined from a technical safeguarding aspect and does not conflict with our safeguarding criteria. Accordingly, NATS (En Route) Public Limited Company ("NERL") has no safeguarding objection to the proposal.

TfL:

The site is on Uxbridge Road which is part of the Strategic Road Network. TfL request that 20% of the residential parking has active electric charging points and a further 20% have passive charging points in order to comply with London Plan standards. TfL request that cycle parking is provided in conformity with London Plan standards and is located within the curtilage of the buildings. Subject to the above being addressed, TfL have no objections to the application.

Internal Consultees

Access Officer:

No objection; a condition to be applied requiring a minimum of one unit to be designed and fitted to the specifications prescribed in our Accessible Hillingdon SPD. The usual Lifetime Homes Standards Condition should also be attached to any approval notice.

Environmental Protection Unit:

Following a request for additional information in regards to noise and sound insulation and the submission of a Noise Insulation Scheme, there is no objection to the proposal.

Highways:

Further to reviewing the above, it is considered that the traffic generation associated with the proposed residential apartments would not have a material impact along the adjacent highway, above that of the extant B1(c) uses at the site. In addition, it is noted that car parking will be allocated at 1 space per apartment and cycle parking at 1.2 spaces per apartment, which is considered acceptable to serve the proposals. In terms of the layout of the site, this has been agreed under the extant planning consent.

As a result, an objection is not raised in relation to the highway and transportation aspects of the proposals.

Section 106 Officer:

Following discussions with the applicant, a financial contribution of £120,000 towards off-site affordable housing is considered acceptable and agreed in principle.

Trees/Landscape Officer:

Site description:

- The site lies within a residential area on the south side of Uxbridge Road. Formerly used as a transport depot the site is currently being developed in accordance with planning approval ref. 2011/2079 and 2013/1650 for a mixed use development.

Landscape Planning designations:

- The development proposals include approved layout and landscape proposals.

Landscape constraints/opportunities:

- See above

PROPOSAL:

The proposal is to change the use of the B1(c) floor space to provide 12 additional residential units

and associated ancillary works.

LANDSCAPE CONSIDERATIONS:

Saved policy BE38 seeks the retention and utilisation of topographical and landscape features of merit and the provision of new planting and landscaping wherever it is appropriate.

· According to the Design & Access Statement (3.6) 'it is not proposed to alter the scale, or footprint or position of the approved buildings....minor changes are proposed to the site layout and are limited to the provision of patios/terraces, landscaping and small alterations to provide an improved parking arrangement.'

· If the application is recommended for approval, landscape conditions should be imposed to ensure that the proposals preserve and enhance the character and local distinctiveness of the surrounding natural and built environment.

RECOMMENDATIONS:

No objection, subject to the above observations and COM9 (parts 1, 2, 4, 5 and 6).

7. MAIN PLANNING ISSUES

7.01 The principle of the development

Policy LE4 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) states that proposals involving the loss of existing industrial floor space outside of designated industrial and business areas will normally only be permitted if:

- i) the existing use seriously affects amenity, through disturbance to neighbours, visual intrusion or an adverse impact in the character of an area; or
- ii) the site is unsuitable for industrial redevelopment because of the size, shape, location or lack of vehicular access; or
- iii) there is no realistic prospect of the land being used for industrial and warehousing purposes in the future; or
- iv) they are in accordance with the Council's regeneration policies for an area.

In order to comply with Policy LE4 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012), the applicant is required to demonstrate that there have been sufficient attempts to market the B1(c) units and to provide an assessment of current supply and demand for commercial units in the area.

The applicant has submitted a report which demonstrates that there have been numerous attempts to market the B1(c) commercial units since October 2011, which have been unsuccessful. Although there has been some initial interest from potential occupiers, these have not been pursued due to concerns over potential conflicts between the commercial and residential uses on the site, particularly for the buildings with commercial on the ground floor and residential above.

The report also demonstrates that there are a large number of commercial B1(c) units available to rent within the local area. These units include both older stock historic/redundant buildings and newly refurbished/purpose built commercial spaces. These units are still available on the open market due to the lack of demand.

Further there have been significant changes in planning policy since the original grant of permission. The National Planning Policy Framework makes clear at paragraph 22 that 'planning policies should avoid the long term protection of sites allocated for employment use where there is no reasonable prospect of a site being used for that purpose. Land allocations should be regularly reviewed. Where there is no reasonable prospect of a site

being used for the allocated employment use, applications for alternative uses of land or buildings should be treated on their merits having regard to market signals and the relative need for different land uses to support sustainable local communities'.

In this case the applicant has demonstrated that there is no reasonable prospect of the light industrial units being let and the National Planning Policy Framework requires the consideration of different land uses to support sustainable communities. The provision of additional housing to meet housing objectives is therefore consistent with the National Planning Policy Framework.

Therefore, subject to compliance with the relevant policies of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012), there is no objection in principle to the change of use of the B1(c) floor space into 12 residential units (C3).

7.02 Density of the proposed development

Not applicable to this application.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

Not applicable to this application.

7.04 Airport safeguarding

NATS raises no objection to the proposal in regards to airport safeguarding.

7.05 Impact on the green belt

Not applicable to this application.

7.07 Impact on the character & appearance of the area

Policies BE13 and BE19 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) require developments to harmonise with the existing street scene or other features in the area and ensure that oversized and badly located additions are avoided.

Both the original planning application (ref: 4404/APP/2008/3558) and the replacement planning application (ref: 4404/APP/2011/2079) considered the impact of the proposed mixed commercial and residential development on the character and appearance of the area. The majority of the site is located behind the rear gardens of 581-603 Uxbridge Road and so would not be highly visible from the public highway. The design, size, scale and siting of the proposed blocks were considered to be acceptable.

The current application would not result in any change to the size, scale and siting of the proposed blocks. As the proposal seeks to change the use of the ground floors of Blocks A2, C and D from commercial to residential, there have been minor changes to the external appearance of the blocks, and these are considered to be acceptable. The proposal would result in the conversion of the solely commercial Block B into residential use, and so the external appearance of the building would be changed in order to ensure that Block B is in keeping with the residential appearance of the site.

The proposal is considered to comply with Policies BE13 and BE19 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7.08 Impact on neighbours

The original planning application (ref: 4404/APP/2008/3558) looked at the impact of the proposed mixed commercial and residential development on neighbouring properties. It was considered that the proposed scheme would not have a significantly detrimental impact on the residential amenity of the neighbouring properties. The current application would replace the B1(c) floor space with 12 additional residential units. The development

would therefore be solely used for residential purposes and so would have less of an impact on residential amenity of neighbours than the previously approved mixed use scheme.

7.09 Living conditions for future occupiers

i. External Amenity Space

Policy BE23 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) requires new developments to "provide or maintain external amenity space which is sufficient to protect the amenity of the occupants of the proposed and surrounding buildings, and which is useable in terms of its shape and siting." The HDAS 'Residential Layouts' SPD states that 20sq.m of external amenity space should be provided for one-bed flats and 25sq.m of external amenity space for two-bed flats.

The amount of external amenity space provided for the previously approved 43 residential units (1,300sq.m) exceeded the recommended standard set out in the HDAS 'Residential Layouts' SPD. The current proposal is required to provide an additional 280sq.m of external amenity space for the 12 residential units. Each of the new residential units would be provided with private amenity space in the form of balconies, patios and gardens. An additional 134sq.m of amenity space has been provided as part of the current application.

It is noted that the site neighbours an existing area of public open space, Rosedale Park, and a pedestrian access between the development and the park was approved as part of the original planning permission (ref: 4404/APP/2008/3558). There is also a children's play area and public open space located nearby, on the opposite side of Uxbridge Road.

It is therefore considered that the proposal would provide adequate levels of external amenity space for the 12 additional residential units without impacting on the amenity provision of the 43 units originally approved, thereby complying with Policy BE23 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

ii. Internal Space Standards

Policy 3.5 of the London Plan (July 2011) requires dwellings to provide an acceptable amount of internal floor space; the standard floor space for a one-bed two persons dwelling is 50sq.m and 70sq.m for a two-bed four persons dwelling. The proposed residential units would comply with the recommended floor space standards set out in the London Plan.

iii. Daylight/Sunlight/Privacy & Overlooking

Policies BE20 and BE21 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and HDAS: 'Residential Layouts' seek to ensure that new developments maintain and allow adequate levels of daylight and sunlight to penetrate into and between them. Furthermore these policies state that planning permission will not be granted for new developments which by reason of their siting, bulk and proximity, would result in a significant loss of residential amenity.

Policy BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) requires the design of new developments to protect the privacy of neighbouring dwellings. The Council's HDAS: 'Residential Layouts' sets out a minimum of 15m separation distance between adjoining dwellings. Furthermore, Paragraph 4.12 of the HDAS 'Residential Layouts' requires a 21m distance separation between habitable rooms to ensure no loss of privacy would occur.

The original planning application (ref: 4404/APP/2008/3558) considered the layout of the proposed blocks to be acceptable in regards to their siting, bulk and proximity and would

maintain adequate levels of daylight/sunlight to residential units. The siting, bulk and proximity of the six blocks would not be impacted upon by the proposed change of use of the B1(c) commercial floor space, and so there would be no change to the amount of sunlight/daylight received by the approved and proposed residential units.

Adequate levels of privacy between the approved residential units were achieved through the careful siting of high level windows where the separation distances were less than 15m. The converted ground floor units would be provided with high level windows in-line with those located on the upper floors, in order to ensure adequate levels of privacy are retained.

Block B was originally solely for commercial use. The proposed scheme would convert this block into five residential units. In order to ensure the proposal does not create an issue of overlooking of the rear garden of 593-595 Uxbridge Road from Block B, high level windows would be introduced on the western elevation of Block B and the existing tree planting would be retained. Additional tree planting would be added to the eastern site boundary in order to reduce overlooking of the rear garden of 583 Uxbridge Road from the balconies on Block B.

It is therefore considered that the proposed change of use of B1(c) commercial floor space to 12 additional residential units complies with Policies BE20, BE21 and BE24 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7.10 Traffic impact, Car/cycle parking, pedestrian safety

Policy AM7 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) considers whether the traffic generated by proposed developments is acceptable in terms of the local highway and junction capacity, traffic flows and conditions of general highway or pedestrian safety. Policy AM14 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) seeks to ensure that all development is in accordance with the Council's adopted Car Parking Standards.

The Council's Car Parking Standards require the provision of one parking space per residential dwelling. A total of 59 parking spaces would be provided on site and would include disabled and visitor parking spaces. It is proposed that the parking spaces would be allocated to each residential unit.

Electric vehicle charging points should be provided in accordance with the standards set out in the London Plan (July 2011).

The scheme is also required to provide one cycle parking space to each residential unit. 69 cycle spaces would be provided within 8 covered and secure cycle stores located around the site.

The Council's Highways Engineer raises no objection to the proposal.

It is considered that the proposed scheme would provide adequate provision of car and cycle parking for both the previously approved and the proposed residential dwellings, in compliance with the standards set out in Policy AM14 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

Access into and within the site would remain as previously approved. The amount of traffic generated by sole residential use is likely to be slightly lower than the amount of traffic generated by a mixed use development.

7.11 Urban design, access and security

Urban Design:

See Section 7.07 of this report.

Access:

There would be no change to the previously approved access into the site.

Security:

The proposal would need to comply with the principles of Secure by Design. This can be dealt with by way of a Secure by Design condition on any consent granted.

7.12 Disabled access

The Council's Access Officer considers the proposal to be acceptable, provided the proposed units comply with the Lifetime Homes standards. This can be secured by way of a Lifetime Homes condition on any consent granted.

7.13 Provision of affordable & special needs housing

Affordable Housing

London Plan (2011) Policies 3.11 and 3.12 require developments to provide 35% affordable housing on-site, taking into account individual circumstances including development viability.

The original planning application (ref: 4404/APP/2008/3558) was supported by a financial viability assessment demonstrating that it was not feasible to provide any affordable housing within the redevelopment. At the time of the 2012 permission (ref: 4404/APP/2011/2079) it was considered that there had not been significant changes in the economic situation since the original permission was granted, and that the cost modelling previously undertaken would remain valid. As such, the planning application (ref: 4404/APP/2011/2079) was approved without requiring affordable housing provision.

The applicant has provided a financial viability assessment as part of this application in order to demonstrate the feasibility of providing on-site affordable housing. The report was prepared in accordance with the relevant guidance and methodologies and has been validated by an independent consultant for the Council. The outcome of this report was that it is not feasible for the development to provide any affordable housing units on the site.

Notwithstanding the viability report, there have been extensive discussions between officers and the applicant in relation to the specific circumstances of this proposal. As a result, it has been agreed with the applicant that they will provide a financial contribution of £120,000 towards off-site affordable housing. This can be secured by way of a Section 106 Legal Agreement. Therefore, although there will no affordable housing provided on site, it is considered that the financial contribution towards off-site affordable housing would be acceptable in accordance with London Plan (2011) Policies 3.11 and 3.12.

Technically a valid financial viability assessment has been submitted and the development is not viable. However, because of the historical background to this application, it could be argued that the Three Dragons Toolkit was not designed for the scenario that applies on this case. A lengthy negotiation therefore occurred to achieve a meaningful off-site contribution. The sum agreed would equate to two off-site units. Given the fallback position that exists and the length of negotiations to reach the outcome of the agreed contribution, it is not considered that a higher contribution could be negotiated.

7.14 Trees, landscaping and Ecology

Policy BE38 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012)

seeks to retain and utilise topographical and landscape features of merit and provide new planting and landscaping when appropriate.

The landscaping for the mixed residential and commercial development was considered acceptable under the previously approved applications (refs: 4404/APP/2008/3558 and 4404/APP/2011/2079). The proposed scheme would convert the B1(c) commercial floor space into 12 residential units, and so there would be some changes to the landscaping in order to reflect the full residential use of the site. Additional landscaping would be provided around Blocks B, C and D to increase residential amenity and to soften the appearance of the buildings.

The Council's Trees/Landscape Officer raises no objection to the proposed scheme subject to a suitable landscaping condition on any consent granted.

The proposed scheme complies with Policy BE38 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7.15 Sustainable waste management

The approved planning permission (ref: 4404/APP/2011/2079) provided twenty refuse and recycling bins within five bin stores located around the site. The proposal would retain the approved amount of refuse storage and is considered to be acceptable.

7.16 Renewable energy / Sustainability

The approved planning permission (ref: 4404/APP/2011/2079) was considered to satisfy the requirements of the various London Plan policies relating to renewable energy and sustainability. As there have been no material changes in planning policy since the above planning permission was granted, it is considered that the proposed scheme would be acceptable in terms of renewable energy and sustainability. A condition shall be added to any consent granted requiring the proposed residential units to achieve Level 4 of the Code for Sustainable Homes.

7.17 Flooding or Drainage Issues

The site does not lie within a designated flood risk zone. Accordingly, the consideration of flood risk is not relevant to the current proposal.

The approved planning permission (ref: 4404/APP/2011/2079) for the mixed residential and commercial development was considered acceptable in terms of sustainable drainage. It is considered that the proposed change of use of the B1(C) commercial floor space to residential would not result in significant drainage issues that cannot be dealt with by sustainable drainage measures.

7.18 Noise or Air Quality Issues

Not applicable to this application.

7.19 Comments on Public Consultations

No objections were received during the public consultation. One letter of support has been received.

7.20 Planning obligations

The proposed scheme would result in the conversion of existing B1(c) floor space to 12 additional residential units and so would be Mayoral CIL Liable. The London Borough of Hillingdon falls within Charging Zone 2 of the Mayoral CIL which requires a flat rate fee of £35 per square metre for each net additional square metre added to the site as part of the development.

The proposal would also be liable under the London Borough of Hillingdon's Community

Infrastructure Levy (CIL), which was introduced in August 2014. The charging schedule requires a fee of £95 per square metre for residential developments.

7.21 Expediency of enforcement action

Not applicable to this application.

7.22 Other Issues

None

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application.

Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

None

10. CONCLUSION

The application site currently benefits from planning permission ref: 4404/APP/2011/2079, dated 30-03-2012 of the redevelopment of the site to provide replacement Class B1(c) light industrial space and 34 two-bedroom and 9 one-bedroom flats (Application to replace extant planning permission ref: 4404/APP/2008/3558, dated 23-03-2009).

This application seeks full planning permission to allow the change of use of the B1(c) floor space to 12 additional residential units (C3) and associated ancillary works.

The proposed change of use from B1(c) floor space to 12 residential units is considered to be acceptable in principle. The proposal would provide adequate amenity space and accord with the London Plan (2011) internal floor space standards.

It is considered that the proposed additional units would not have a detrimental impact on the character of the surrounding area or on the residential amenity of future occupiers of the site or neighbouring properties.

The proposal complies with Policies AM7, AM14, BE13, BE15, BE19, BE20, BE23, BE24 and LE4 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012) and the London Plan Policies (July 2011). It is therefore recommended that planning permission is approved.

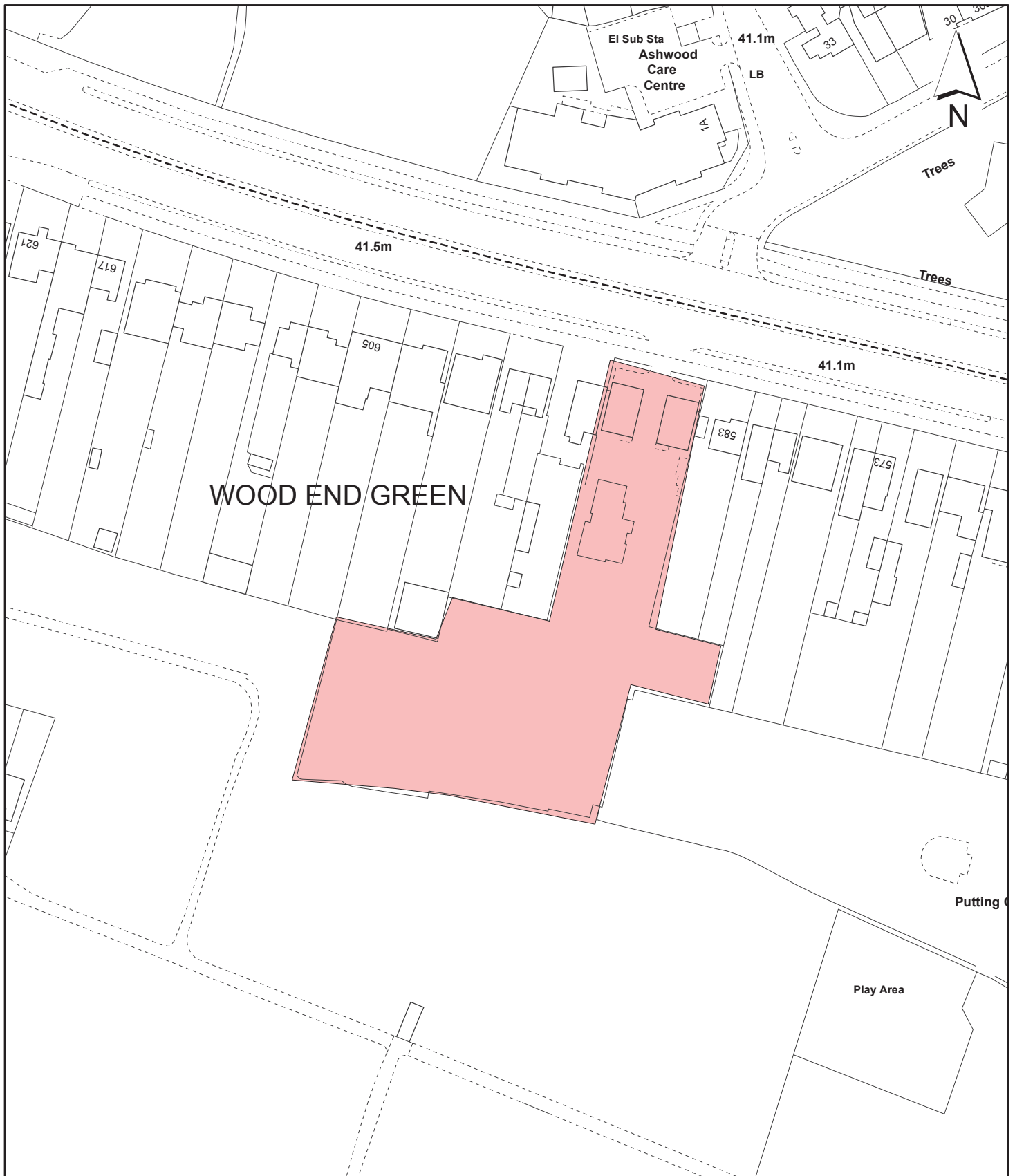
11. Reference Documents

Hillingdon Local Plan: Part One - Strategic Policies (November 2012)

Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012)

Contact Officer: Katherine Mills

Telephone No: 01895 250230



Notes

 Site boundary

For identification purposes only.
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Site Address

**Pronto Industrial Estate
 & 585 - 591 Uxbridge Road
 Hayes**

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111

Planning Application Ref:

4404/APP/2014/2506

Scale

1:1,250

Planning Committee

Major Page 103

Date

March 2015



HILLINGDON
 LONDON

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Report of the Head of Planning, Sport and Green Spaces

- Address** 1 NOBEL DRIVE HARLINGTON
- Development:** Conversion and extension of existing office building to form a 200 bedroom hotel with banqueting suite, conference facilities, and rooftop restaurant, including a seven-storey extension to rear, a three storey addition at roof level and single-storey side extension, together with the creation of a new vehicle access, and alterations to car parking and landscaping.
- LBH Ref Nos:** 46214/APP/2014/2827
- Drawing Nos:**
- 01-Location Plan
 - 02-Ex Site Plan
 - 03-Ex GF plan
 - 04-Ex FF plan
 - 05-Ex SF plan
 - 06-Ex Third flr plan
 - 07-Ex Roof plan
 - 08-Ex South and East Elevation
 - 09-Ex North and West Elevations
 - 10-Ex Sections A-A and B-B
 - Aether_AQ_assessment_NobelDr_Hillingdon_July14_Fir
 - Appendices A to C
 - Appendix D
 - Bus-tracking
 - Car-tracking
 - DESIGN & ACCESS STATEMENT B
 - GEA-Ground Contamination Report
 - Heathrow Market Report - July 2014
 - Heathrow Top Energy statement
 - Heathrow Top Hotel - PLanning Statement 080814
 - Interim Travel Plan - Nobel Drive Heathrow 26 June
 - Refuse vehicle-tracking
 - Service vehicle-tracking
 - _AB Heritage-Archeaology Report-JUL 2014_FINA
 - ae1449-01a
 - ae1449-02deck-04-prop first flr plan
 - PL02B
 - PL03A
 - PL04A
 - PL05A
 - PL06A
 - PL07A
 - PL08A
 - PL09A
 - PL10A
 - PL11A
 - PL12A
 - PL13A
 - PL14A
 - CHP Calcs sheet

HT energy statement - Addendum 1 & 2

Date Plans Received:	08/08/2014	Date(s) of Amendment(s):	19/02/2015
Date Application Valid:	03/09/2014		08/08/2014 23/02/2015

1. SUMMARY

The application seeks planning permission for the change of use of the existing building from office (Use Class B1(a)) to a Hotel (Use Class C1) including banqueting, restaurant and conference facilities, including a seven-storey 'L-shaped' extension to the rear, a three-storey extension at roof level of the existing building and a single storey extension to the western side of the existing building.

Given the site's context the scheme raises no adverse amenity issues to residential neighbours nor does the new building's massing and outlook prejudice the existing or proposed developments on the adjacent sites. The car parking provision and highway access arrangements are considered consistent with planning policy and acceptable including the arrangements for service delivery and guest drop off/collection.

The height of the proposal is consistent with the surrounding development and in visual appearance terms the treatment of the elevations is considered appropriate and the proposed landscaping to street adequate.

In summary the scheme, subject to appropriate conditions and planning obligations, is considered to comply with relevant London Plan and Hillingdon Local Plan Part 1 and Part 2 policies. The GLA has raised several issues in their Stage 1 comments relating to design and landscaping, inclusive access, renewable energy, local training, transport and parking.

It is anticipated that these matters have either been resolved or can be resolved in the lead up to and during the referral process, but delegated powers are sought to determine the application as appropriate following Stage 2 considerations by the Mayor.

2. RECOMMENDATION

That delegated powers be given to the Head of Planning and Enforcement to grant planning permission subject to the relevant conditions set out below:

A) That the Council enters into an agreement with the applicant under Section 106 of the Town and Country Planning Act 1990 (as amended) and/or other appropriate legislation to secure:

1. Highways: to secure all necessary works (including new access points and relocation or removal of on-street parking bays including associated works) and

the provision of a Travel Plan (including £20,000.00 Bond), including Sustainable Transport Measures (such as a hopper bus service), and a Construction Logistics Plan and a Delivery & Service Plan.

2. Construction Training: either a financial contribution, or an in-kind scheme delivered during the construction phase of the development, should be secured (in either event the 'obligation' should be delivered equal to the formula of £2,500 for every £1 million build cost plus £9600 Coordinator Costs).

3. Air Quality: in line with the SPD and given the site is located in an air quality management area then a contribution in the sum of £25,000.

4. Hospitality Training

5. An Employment Training Strategy

6. Carbon Fund offsite contribution of £12,600.00 (@£60/tCO₂ for 30years) to offset against the shortfall (which equates to 7 tonnes of carbon per year)

7. Project Management and Monitoring Fee: a financial contribution equal to 5% of the total cash contributions towards the management and monitoring of the resulting agreement.

B) That in respect of the application for planning permission, the applicant meets the Council's reasonable costs in preparation of the Legal Agreement and any abortive work as a result of the agreement not being completed.

C) That officers be authorised to negotiate and agree the detailed terms of the proposed agreement and conditions of approval.

D) That if any of the heads of terms set out above have not been agreed and the S106 legal agreement has not been finalised before 16/03/2015, or such other date as agreed by the Head of Planning and Enforcement delegated authority be given to the Head of Planning and Enforcement to refuse planning permission for the following reason:

'The applicant has failed to provide contributions towards the improvement of services and the environment as a consequence of demands created by the proposed development (in respect of construction training, off site highways impacts, carbon savings as well as air quality). The proposal therefore conflicts with Policies R17, AM7 and OE1 of the adopted Local Plan and the Council's Planning Obligations SPD and Air Quality SPG.'

E) That subject to the above, the application be deferred for determination by the Head of Planning and Enforcement under delegated powers, subject to the completion of the legal agreement under CIL Regulations and other appropriate powers with the applicant.

F) That the applicant shall pay the required levy on the additional floorspace created under the Council's Community Infrastructure Levy.

G) That if the application is approved, the following conditions be imposed:

1 COM3 Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON

To comply with Section 91 of the Town and Country Planning Act 1990.

2 COM4 Accordance with Approved Plans

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans, numbers PL14A, PL13A, PL12A, PL11A, PL10A, PL09A, PL08A, PI07A, PL06A, PL05A, PL04A, PL03A, PL02B, ae1449-01a and ae1449-02deck-04-prop and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and the London Plan (July 2011).

3 COM5 General compliance with supporting documentation

The development hereby permitted shall not be occupied until the following has been completed in accordance with the specified supporting plans and/or documents:

Heathrow Top Hotel - Planning Statement 080814,
GEA-Ground Contamination Report,
Design & Access Statement Rev.B.

Thereafter the development shall be retained/maintained in accordance with these details for as long as the development remains in existence

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and the London Plan (July 2011).

4 NONSC Facilities for People with Disabilities

The hotel use hereby approved shall not commence until the development has been provided with a layout and facilities which meet the requirements set out in BS 8300:2009 (Code of Practice on Design of buildings and their approaches to meet the needs of disabled people), including:-

a. the development shall be Disability Discrimination Act compliant ensuring that step and ramp free access are provided within the primary access areas and landscaped amenity spaces;

b. sufficient numbers of lifts able to accommodate wheelchair users;

c. 10% of all rooms as a percentage of the total number of bedrooms should be accessible by disabled users with:

i. 5% of these rooms being fully accessible from the outset and equipped with all fittings necessary to facilitate use by people with limited mobility including interconnecting doors and fully accessible bathrooms;

ii. 5% of the rooms being standard accessible rooms suitable for wheelchair circulation

and include ensuite wetrooms and interconnecting doors to accommodate for carers of disabled guests or larger families

d. 5% of all rooms should be standard rooms with interconnecting doors to twin rooms suitable for families, the elderly and guests with limited mobility

e. 20 ensuite bathrooms to be specified to BS 8300:2009 standard.

The layout and all facilities which form part of the scheme shall be retained and maintained in good working order for so long as the building remains in use.

REASON

To ensure that London's visitor infrastructure is accessible and welcoming to all sections of the population, including older and disabled people in accordance with policy AM13 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and policies 3.1, 3.8 and 7.2 of the London Plan (2011).

5 NONSC Air Quality - Energy Provision

Before the development is commenced details of any plant, machinery and fuel burnt, as part of the energy provision and the location of the flue at the development shall be submitted to the LPA for approval. This shall include pollutant emission rates at the flue with or without mitigation technologies. The use of ultra low NOx emission gas-fired CHPs and boilers is recommended. The development should as a minimum be 'air quality neutral' and demonstrably below the building emissions benchmark, with a low emission CHP.

REASON:

To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

6 NONSC Air Extract System

No air extraction system shall be used on the premises until a scheme for the control of noise and odour emanating from the site has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of measures as may be approved by the LPA. Thereafter, the scheme shall be implemented and maintained in full compliance with the approved measures.

REASON:

To safeguard the amenity of the occupants of surrounding properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

7 NONSC Extract Systems

No hot cooking shall take place within the premises until full details, with calculations, of the proposed fume/ odour extraction system have been submitted to and approved in writing by the Local Planning Authority, unless otherwise agreed in writing. The submission shall include details of:

- (i) The extract fan, silencers, anti-vibration mounts, high velocity cowl, correctly sized carbon filter and electrostatic precipitator systems and any other items of plant;
- (ii) The velocity of air flowing through the cooker hood, the carbon filters, electrostatic precipitator and at the duct termination;
- (iii) The retention time of gases in the carbon filters;

(iv) A maintenance schedule;

Before commencement of the approved ground floor use, the approved extraction system shall be installed on site in accordance with the approved details and shall be retained and maintained thereafter. Any variations thereafter shall be agreed in writing by the Local Planning Authority.

REASON

To safeguard the amenity of the future occupiers of the development and of occupiers of adjacent premises in accordance with Policy OE1 Hillingdon Local Plan: Part 2 Saved UDP Policies (November 2012)

8 COM15 Sustainable Water Management

Prior to the commencement of the development, a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how it incorporates sustainable urban drainage in accordance with the hierarchy set out in Policy 5.15 of the London Plan and will:

- i. provide information on all Suds features including the method employed to delay and control the water discharged from the site and:
 - a. calculations showing storm period and intensity and volume of storage required to control surface water and size of features to control that volume.
 - b. any overland flooding should be shown, with flow paths depths and velocities identified as well as any hazards, (safe access and egress must be demonstrated).
 - c. measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - d. how they or temporary measures will be implemented to ensure no increase in flood risk from commencement of construction.
- ii. and demonstrate capacity into the receiving foul sewer network
- iii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime. Including appropriate details of Inspection regimes, appropriate performance specification, remediation and timescales for the resolving of issues.
- iv. provide details of the body legally responsible for the implementation of the management and maintenance plan.
The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:
 - v. incorporate water saving measures and equipment.
 - vi. provide details of water collection facilities to capture excess rainwater;
 - vii. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (Nov 2012) Policy 5.12 Flood Risk Management of the London Plan (July 2011) and National Planning Policy Framework (March 2012) and the Planning Practice Guidance (March 2014). To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

9 NONSC Amplified Music/ Control of site noise rating level

No music and/or other amplified sound from the premises shall be audible inside surrounding residential premises between 2300 hours and 0700 hours. At all other times, the rating level of the noise emitted from the site shall be at least 5dB below the existing background noise level. The noise levels shall be determined at the boundary of the nearest residential property. The measurements and assessment shall be made in accordance to the latest British Standard 4142, 'Method for rating industrial noise affecting mixed residential and industrial areas'.

REASON

In the interests of residential amenity in compliance with Policy OE1 of the Hillingdon Unitary Development Plan Saved Policies September 2007.

10 COM27 Traffic Arrangements - submission of details

Development shall not begin until details of all traffic arrangements (including where appropriate carriageways, footways, turning space, safety strips, sight lines at road junctions, kerb radii, car parking areas and marking out of spaces, loading facilities, alterations of existing access and means of surfacing) have been submitted to and approved in writing by the Local Planning Authority. The approved development shall not be occupied until all such works have been constructed in accordance with the approved details. Thereafter, the parking areas, sight lines and loading areas must be permanently retained and used for no other purpose at any time. Disabled parking bays shall be a minimum of 4.8m long by 3.6m wide, or at least 3.0m wide where two adjacent bays may share an unloading area.

REASON

To ensure pedestrian and vehicular safety and convenience and to ensure adequate off-street parking, and loading facilities in compliance with Policy AM14 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Chapter 6 of the London Plan (July 2011).

11 NONSC Construction Management Strategy (Aircraft Safety)

Development shall not commence until a construction management strategy has been submitted to and approved in writing by the Local Planning Authority covering the application site and any adjoining land which will be used during the construction period. Such a strategy shall include the following matters:

- details of the area(s) subject to construction activity and the storage of materials and equipment
- details of cranes and other tall construction equipment (including the details of obstacle lighting) - Such schemes shall comply with Advice Note 4 'Cranes and Other Construction Issues'(available at www.aoa.org.uk/operations & safety/safeguarding. asp).
- details of temporary lighting - Such details shall comply with Advice Note 2 'Lighting Near Aerodromes' (available at www.aoa.org.uk/operations & safety/safeguarding. asp).

- height of storage areas for materials or equipment.

The approved strategy (or any variation approved in writing by the Local Planning Authority) shall be implemented for the duration of the construction period.

REASON

To ensure that construction work and construction equipment on the site and adjoining land does not breach the Obstacle Limitation Surface (OLS) surrounding Heathrow Airport and endanger aircraft movements and the safe operation of the aerodrome, and; to ensure the development does not endanger the safe movement of aircraft or the operation of Heathrow Airport through interference with communication, navigational aids and surveillance equipment.

12 NONSC Car Parking for guests only

The car parking facilities provided at the hotel shall be used by hotel guests and conference/ banqueting facilities visitors only and strictly for the duration of their stay at the hotel. Prior to occupation of the hotel, a car parking management strategy shall be submitted to demonstrate how this will be managed and to ensure the efficient operation of the car park, especially at peak demand periods. The approved strategy shall be implemented as soon as the hotel is brought into use and the strategy shall remain in place thereafter. Any changes to the strategy shall be agreed in writing by the Local Planning Authority.

REASON

To ensure suitable parking provision is provided on the site, in accordance with policies AM2 and AM7 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Chapter 6 of the London Plan (July 2011).

13 COM30 Contaminated Land

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use.

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement.

(ii) If during development or works contamination not addressed in the submitted remediation scheme is identified, an addendum to the remediation scheme must be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a

verification report submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

(iv) Before any part of the development is occupied, site derived soils and imported soils shall be independently tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

REASON

To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

14 NONSC Archaeology

A) No development shall take place until the applicant (or their heirs and successors in title) has secured the implementation of a programme of archaeological investigation in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the local planning authority in writing.

B) No development or demolition shall take place other than in accordance with the Written Scheme of Investigation approved under Part (A).

C) The development shall not be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under Part (A), and the provision made for analysis, publication and dissemination of the results and archive deposition has been secured.

REASON

Heritage assets of archaeological interest are expected to survive on the site. The planning authority wishes to secure the provision of appropriate archaeological investigation, including the publication of results.

15 NONSC Energy Efficiency

Prior to the commencement of development a detailed energy assessment shall be submitted to and approved in writing by the Local Planning Authority.

The assessment shall clearly set out the baseline energy performance of the development, and the detailed measures to reduce CO2 emissions in accordance with the approved energy strategy submitted with the planning application. The assessment shall provide detailed specifications of the measures and technology set out in outline energy assessment. In particular, the assessment shall provide full details of the proposed CHP unit, including inputs and outputs, location and how it connects to the development. The assessment shall also include full details of any further low or zero carbon technology including type, specification, and detailed plans and elevations where appropriate.

Finally, the assessment must include appropriate measuring and monitoring proposals and how the information will be submitted to the Local Planning Authority to demonstrate

CO2 reductions are being met.

The development must proceed in accordance with the approved scheme.

REASON

To ensure appropriate carbon savings are delivered in accordance with London Plan Policy 5.2.

16 NONSC Building Height

No building or structure of the development hereby permitted shall exceed 50 metres AOD.

REASON

Development exceeding this height would penetrate the Obstacle Limitation Surface (OLS) surrounding Heathrow Airport and endanger aircraft movements and the safe operation of the aerodrome.

17 COM7 Materials (Submission)

No development shall take place until details of all materials and external surfaces have been submitted to and approved in writing by the Local Planning Authority. Thereafter the development shall be constructed in accordance with the approved details and be retained as such.

Details should include information relating to make, product/type, colour and photographs/images.

REASON

To ensure that the development presents a satisfactory appearance in accordance with policy BE13 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

18 COM31 Secured by Design

The building shall achieve 'Secured by Design' accreditation awarded by the Hillingdon Metropolitan Police Crime Prevention Design Adviser (CPDA) on behalf of the Association of Chief Police Officers (ACPO). No building shall be occupied until accreditation has been achieved.

REASON

In pursuance of the Council's duty under section 17 of the Crime and Disorder Act 1998 to consider crime and disorder implications in exercising its planning functions; to promote the well being of the area in pursuance of the Council's powers under section 2 of the Local Government Act 2000, to reflect the guidance contained in the Council's SPG on Community Safety By Design and to ensure the development provides a safe and secure environment in accordance with policies 7.1 and 7.3 of the London Plan (July 2011).

19 COM9 Landscaping (car parking & refuse/cycle storage)

No development shall take place until a landscape scheme has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include:

1. Details of Soft Landscaping

1.a Planting plans (at not less than a scale of 1:100),

1.b Written specification of planting and cultivation works to be undertaken,

1.c Schedule of plants giving species, plant sizes, and proposed numbers/densities where appropriate

2. Details of Hard Landscaping
 - 2.a Refuse Storage (which shall be covered and secure)
 - 2.b Cycle Storage (including secure storage for at least 20 cycles)
 - 2.c Means of enclosure/boundary treatments
 - 2.d Car Parking Layouts (including the provision 83 car parking spaces 64 parking spaces allocated for the hotel and 19 spaces for the banqueting/ conference rooms, including at least 8 disabled parking spaces, and demonstration that at least 10 parking spaces are served by electrical charging points)
 - 2.e Hard Surfacing Materials (including a kerbed access route from Bath Road to the main entrance of the building and crossover alongside the new vehicular access fronting Nobel Drive)
 - 2.f External lighting including for the car parking areas
 - 2.g Other structures (such as furniture)

3. Living Walls and Roofs

3.a A scheme for the inclusion of living walls, roofs and screens, in particular at roof level and at first floor level within the roof terrace. The scheme shall provide details of the types of living material to be used and the locations and methods of maintenance where necessary

4. Details of Landscape Maintenance

- 4.a Landscape Maintenance Schedule for a minimum period of 5 years.
- 4.b Proposals for the replacement of any tree, shrub, or area of surfing/seeding within the landscaping scheme which dies or in the opinion of the Local Planning Authority becomes seriously damaged or diseased.

5. Schedule for Implementation

6. Other

- 6.a Existing and proposed functional services above and below ground
- 6.b Proposed finishing levels or contours

Thereafter the development shall be carried out and maintained in full accordance with the approved details.

REASON

To ensure that the proposed development will preserve and enhance the visual amenities of the locality and provide adequate facilities in compliance with policies BE13, BE38 and AM14 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and policies 5.11 and 5.17 of the London Plan (July 2011).

20 COM10 Tree to be retained

Trees, hedges and shrubs shown to be retained on the approved plan shall not be damaged, uprooted, felled, lopped or topped without the prior written consent of the Local Planning Authority. If any retained tree, hedge or shrub is removed or severely damaged during construction, or is found to be seriously diseased or dying another tree, hedge or shrub shall be planted at the same place or, if planting in the same place would leave the new tree, hedge or shrub susceptible to disease, then the planting should be in a position to be first agreed in writing with the Local Planning Authority and shall be of a size and species to be agreed in writing by the Local Planning Authority and shall be planted in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier. Where damage is less severe, a schedule of remedial works necessary to ameliorate the effect of damage by tree surgery, feeding or

groundwork shall be agreed in writing with the Local Planning Authority. New planting should comply with BS 3936 (1992) 'Nursery Stock, Part 1, Specification for Trees and Shrubs'

Remedial work should be carried out to BS BS 3998:2010 'Tree work - Recommendations' and BS 4428 (1989) 'Code of Practice for General Landscape Operations (Excluding Hard Surfaces)'. The agreed work shall be completed in the first planting season following the completion of the development or the occupation of the buildings, whichever is the earlier.

REASON

To ensure that the trees and other vegetation continue to make a valuable contribution to the amenity of the area in accordance with policy BE38 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and to comply with Section 197 of the Town and Country Planning Act 1990.

21 COM8 Tree Protection

No site clearance or construction work shall take place until the details have been submitted to, and approved in writing by, the Local Planning Authority with respect to:

1. A method statement outlining the sequence of development on the site including demolition, building works and tree protection measures.
2. Detailed drawings showing the position and type of fencing to protect the entire root areas/crown spread of trees, hedges and other vegetation to be retained shall be submitted to the Local Planning Authority for approval. No site clearance works or development shall be commenced until these drawings have been approved and the fencing has been erected in accordance with the details approved. Unless otherwise agreed in writing by the Local Planning Authority such fencing should be a minimum height of 1.5 metres.

Thereafter, the development shall be implemented in accordance with the approved details. The fencing shall be retained in position until development is completed.

The area within the approved protective fencing shall remain undisturbed during the course of the works and in particular in these areas:

- 2.a There shall be no changes in ground levels;
- 2.b No materials or plant shall be stored;
- 2.c No buildings or temporary buildings shall be erected or stationed.
- 2.d No materials or waste shall be burnt; and
- 2.e No drain runs or other trenches shall be dug or otherwise created, without the prior written consent of the Local Planning Authority.

REASON

To ensure that trees and other vegetation can and will be retained on site and not damaged during construction work and to ensure that the development conforms with policy BE38 Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012)

22 NONSC Bird Hazard Management Plan

Development shall not commence until a Bird Hazard Management Plan has been submitted to and approved in writing by the Local Planning Authority. The submitted plan shall include details of:

- Management of any flat/shallow pitched/green roofs on buildings within the site which

may be attractive to nesting, roosting and "loafing" birds. The management plan shall comply with Advice Note 8 'Potential Bird Hazards from Building Design' attached * See para below for information *

The Bird Hazard Management Plan shall be implemented as approved on completion of the development and shall remain in force for the life of the building. No subsequent alterations to the plan are to take place unless first submitted to and approved in writing by the Local Planning Authority.

REASON

It is necessary to manage the flat roofs in order to minimise its attractiveness to birds which could endanger the safe movement of aircraft and the operation of Heathrow Airport.

23 NONSC Fire Evacuation Plan

Prior to the first occupation of the development hereby approved a comprehensive fire emergency plan that demonstrates how disabled people will be safeguarded from fire and enabled to evacuate the building shall be submitted to and approved in writing by the Local Planning Authority.

REASON

To ensure that adequate facilities are provided for people with disabilities in accordance with policy AM13 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and policies 3.1, 3.8 and 7.2 of the London Plan (2011).

INFORMATIVES

1 I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 I53 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

AM13	AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): - (i) Dial-a-ride and mobility bus services (ii) Shopmobility schemes (iii) Convenient parking spaces (iv) Design of road, footway, parking and pedestrian and street furniture schemes
AM14	New development and car parking standards.
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.

AM8	Priority consideration to pedestrians in the design and implementation of road construction and traffic management schemes
BE13	New development must harmonise with the existing street scene.
BE15	Alterations and extensions to existing buildings
BE18	Design considerations - pedestrian security and safety
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
OE1	Protection of the character and amenities of surrounding properties and the local area
OE11	Development involving hazardous substances and contaminated land - requirement for ameliorative measures
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
T2	Location of tourist accommodation and conference facilities
T4	Hotels, guest houses and other tourist accommodation - location, amenity and parking requirements
LPP 2.6	(2011) Outer London: vision and strategy
LPP 2.7	(2011) Outer London: economy
LPP 2.8	(2011) Outer London: Transport
LPP 4.1	(2011) Developing London's economy
LPP 4.5	(2011) London's Visitor Infrastructure
LPP 5.1	(2011) Climate Change Mitigation
LPP 5.10	(2011) Urban Greening
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.21	(2011) Contaminated land
LPP 5.3	(2011) Sustainable design and construction
LPP 5.4	(2011) Retrofitting
LPP 5.7	(2011) Renewable energy
LPP 6.1	(2011) Strategic Approach
LPP 6.10	(2011) Walking
LPP 6.11	(2011) Smoothing Traffic Flow and Tackling Congestion and reducing traffic
LPP 6.13	(2011) Parking
LPP 6.5	(2011) Funding Crossrail and other strategically important transport infrastructure
LPP 6.9	(2011) Cycling
LPP 7.1	(2011) Building London's neighbourhoods and communities
LPP 7.13	(2011) Safety, security and resilience to emergency
LPP 7.14	(2011) Improving air quality
LPP 7.15	(2011) Reducing noise and enhancing soundscapes
LPP 7.2	(2011) An inclusive environment
LPP 7.3	(2011) Designing out crime
LPP 7.4	(2011) Local character
LPP 7.6	(2011) Architecture

LPP 8.2 (2011) Planning obligations

3 I14C Compliance with Building Regulations Access to and use of

You are advised that the scheme is required to comply with either:

- The Building Regulations 2000 Approved Document Part M 'Access to and use of buildings', or with
- BS 8300:2001 Design of buildings and their approaches to meet the needs of disabled people - Code of practice. AMD 15617 2005, AMD 15982 2005.

These documents (which are for guidance) set minimum standards to allow residents, workers and visitors, regardless of disability, age or gender, to gain access to and within buildings, and to use their facilities and sanitary conveniences.

You may also be required make provisions to comply with the Disability Discrimination Act 1995. The Act gives disabled people various rights. Under the Act it is unlawful for employers and persons who provide services to members of the public to discriminate against disabled people by treating them less favourably for any reason related to their disability, or by failing to comply with a duty to provide reasonable adjustments. This duty can require the removal or modification of physical features of buildings provided it is reasonable.

The duty to make reasonable adjustments can be effected by the Building Regulation compliance. For compliance with the DDA please refer to the following guidance: -

- The Disability Discrimination Act 1995. Available to download from www.opsi.gov.uk
- Disability Rights Commission (DRC) Access statements. Achieving an inclusive environment by ensuring continuity throughout the planning, design and management of building and spaces, 2004. Available to download from www.drc-gb.org.
- Code of practice. Rights of access. Goods, facilities, services and premises. Disability discrimination act 1995, 2002. ISBN 0 11702 860 6. Available to download from www.drc-gb.org.
- Creating an inclusive environment, 2003 & 2004 - What it means to you. A guide for service providers, 2003. Available to download from www.drc-gb.org.

This is not a comprehensive list of Building Regulations legislation. For further information you should contact Building Control on 01895 250804.

4 I15 Control of Environmental Nuisance from Construction Work

Nuisance from demolition and construction work is subject to control under the Control of Pollution Act 1974, the Clean Air Act 1993 and the Environmental Protection Act 1990. You should ensure that the following are complied with:

- (i) Demolition and construction works should only be carried out between the hours of 0800 and 1800 on Monday to Friday and between the hours of 0800 and 1300 on Saturday. No works should be carried out on Sundays, Public or Bank Holidays;
- (ii) All noise generated during such works should be controlled in compliance with British Standard 5228, and use "best practicable means" as defined in section 72 of the Control of Pollution Act 1974;
- (iii) Measures should be taken to eliminate the release of dust, odours and other

emissions caused by the works that may create a public health nuisance. Guidance on control measures is given in "The control of dust and emissions from construction and demolition: best practice guidelines", Greater London Authority, November 2006; and (iv) No bonfires that create dark smoke or cause nuisance to local residents should be allowed at any time.

You are advised to consult the Council's Environmental Protection Unit to seek prior approval under Section 61 of the Control of Pollution Act 1974 if you anticipate any difficulty in carrying out the works other than within the normal working hours set out above, and by means that would minimise disturbance to adjoining premises. For further information and advice, contact the Environmental Protection Unit, 3S/02 Civic Centre, High Street, Uxbridge, Middlesex UB8 1UW (tel. 01895 250155).

5

The written scheme of investigation will need to be prepared and implemented by a suitably qualified archaeological practice in accordance with English Heritage Greater London Archaeology guidelines. It must be approved by the planning authority before any on-site development related activity occurs.

6 I28 **Food Hygiene**

The Council's Commercial Premises Section should be consulted prior to the use of the premises so as to ensure compliance with the Food Safety Registration Regulations 1990, Hygiene (General) Regulations 1970, The Food Act 1984, The Health and Safety at Work Act 1974 and any other relevant legislation. Contact: - Commercial Premises Section, 4W/04, Civic Centre, High Street, Uxbridge, UB8 1UW (Telephone 01895 250190).

7 I58 **Opportunities for Work Experience**

The developer is requested to maximise the opportunities to provide high quality work experience for young people (particularly the 14 - 19 age group) from the London Borough of Hillingdon, in such areas as bricklaying, plastering, painting and decorating, electrical installation, carpentry and landscaping in conjunction with the Hillingdon Education and Business Partnership.

Please contact: Mr Peter Sale, Chief Executive Officer, Hillingdon Training Ltd: contact details - c/o Hillingdon Training Ltd, Unit A, Eagle Office Centre, The Runway, South Ruislip, HA4 6SE Tel: 01895 671 976 email: petersale@hillingdontraining.co.uk

8 I60 **Cranes**

Given the nature of the proposed development it is possible that a crane may be required during its construction. The applicant's attention is drawn to the requirement within the British Standard Code of Practice for the safe use of Cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. This is explained further in Advice Note 4, 'Cranes and Other Construction Issues' (available at www.aoa.org.uk/publications/safeguarding.asp)

9

The development is close to the aerodrome and the approach to the runway. We draw attention to the need to carefully design lighting proposals. This is further explained in Advice Note 2, 'Lighting near Aerodromes' (available at [http://www.aoa.org.uk/operation & safety/safeguarding.htm](http://www.aoa.org.uk/operation&safety/safeguarding.htm)). Please note that the Air Navigation Order 2005, Article 135 grants the Civil Aviation Authority power to serve notice to extinguish or screen lighting

which may endanger aircraft.

10

The development is close to the airport and the landscaping which it includes may attract birds which in turn may create an unacceptable increase in birdstrike hazard. Any such landscaping should, therefore, be carefully designed to minimise its attractiveness to hazardous species of birds.

Your attention is drawn to Advice Note 3, 'Potential Bird Hazards: Amenity Landscaping and Building Design' (available at [http://www.aoa.org.uk/operation & safety/safeguarding.htm](http://www.aoa.org.uk/operation&safety/safeguarding.htm)).

11

The Bird Hazard Management Plan must ensure that flat/shallow pitched roofs be constructed to allow access to all areas by foot using permanent fixed access stairs ladders or similar. The owner/occupier must not allow gulls, to nest, roost or loaf on the building. Checks must be made weekly or sooner if bird activity dictates, during the breeding season. Outside of the breeding season gull activity must be monitored and the roof checked regularly to ensure that gulls do not utilise the roof. Any gulls found nesting; roosting or loafing must be dispersed by the owner/occupier when detected or when requested by BAA Airside Operations staff. In some instances it may be necessary to contact BAA Airside Operations staff before bird dispersal takes place. The owner/occupier must remove any nests or eggs found on the roof.

The breeding season for gulls typically runs from March to June. The owner/occupier must obtain the appropriate licences where applicable from Natural England before the removal of nests and eggs.

12

You are advised that the development hereby approved represents chargeable development under the Community Infrastructure Levy. The applicant will be liable to pay the Community Infrastructure Levy on commencement of this development. A separate liability notice will be issued by the Local Planning Authority, however you are advised that it is your responsibility to notify the Local Planning Authority of the anticipated commencement date and any changes in liability through submission of the appropriate forms.

Should you require further information please refer to the Council's Website (<http://www.hillingdon.gov.uk/index.jsp?articleid=24738>).

3. CONSIDERATIONS

3.1 Site and Locality

The application site comprises a roughly rectangular shaped plot located on the north side of Bath Road in Harlington. The existing four-storey office building on the site sits towards the southern half of the site, fronting Bath Road and Nobel Drive. The office building has been vacant since February 2013 and was last occupied by the Border Agency.

The site is bounded to the east by Nobel Drive, which is accessed at both ends via Bath Road and provides access to both commercial and residential properties. Beyond Nobel Drive to the east is a large 1960s built single-storey building which is used as a bowling alley. To the north are three-storey residential properties, beyond which is land falling within

the Green Belt.

The site is bounded to the west by the 4/5 storey Holiday Inn Hotel, and to the south by the A4 dual carriageway, beyond which are relatively small scale commercial and industrial buildings within the Heathrow Airport boundary.

The north side of Bath Road is largely characterised by commercial office buildings and hotels, and there are several other hotels, including the Ibis, Holiday Inn, Sheraton, Marriott and Radisson Edwardian, within the vicinity. The southern side of Bath Road is more characterised by smaller scale commercial and industrial buildings, and large car parks associated with Heathrow Airport.

The site falls within the developed area as shown on the Hillingdon Unitary Development Plan Proposals Map. Bath Road is designated as a Strategic Route and open land beyond the residential properties to the north is designated as Green Belt.

3.2 Proposed Scheme

The application seeks planning permission for the change of use of the existing building from office (Use Class B1(a)) to a Hotel (Use Class C1). The proposal seeks to extend and convert the existing office building into a 200 bed hotel, together with conference/banqueting facilities (for up to 600 people) at first floor level, a restaurant at roof level and associated amendments to landscaping and car parking.

Three extensions are proposed to the original building; a seven-storey 'L-shaped' extension to the rear to provide the conference/banqueting facility at first floor level, with additional rooms, with undercroft parking; a three-storey extension at roof level of the existing building, to provide two floors of hotel rooms and one floor for the restaurant; and a single storey extension to the western side of the existing building to provide the back-of-house facilities.

Although the main car parking area is to remain some existing parking will be lost, with the extensions constructed above the existing car parking area. Deliveries and servicing would take place to the rear of the existing building, with a coach/taxi drop-off area proposed in a new atrium entrance area between the existing building and the proposed rear extension. The vehicle access is proposed to the rear of the site, to enable circulation and drop-off facilities to function whilst the exit would be to Nobel Drive.

3.3 Relevant Planning History

Comment on Relevant Planning History

There is no planning history that could be relevant in the determination of this application.

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

Part 2 Policies:

AM13 AM13 Increasing the ease of movement for frail and elderly people and people with

	disabilities in development schemes through (where appropriate): -
	(i) Dial-a-ride and mobility bus services
	(ii) Shopmobility schemes
	(iii) Convenient parking spaces
	(iv) Design of road, footway, parking and pedestrian and street furniture schemes
AM14	New development and car parking standards.
AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM8	Priority consideration to pedestrians in the design and implementation of road construction and traffic management schemes
BE13	New development must harmonise with the existing street scene.
BE15	Alterations and extensions to existing buildings
BE18	Design considerations - pedestrian security and safety
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
OE1	Protection of the character and amenities of surrounding properties and the local area
OE11	Development involving hazardous substances and contaminated land - requirement for ameliorative measures
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
T2	Location of tourist accommodation and conference facilities
T4	Hotels, guest houses and other tourist accommodation - location, amenity and parking requirements
LPP 2.6	(2011) Outer London: vision and strategy
LPP 2.7	(2011) Outer London: economy
LPP 2.8	(2011) Outer London: Transport
LPP 4.1	(2011) Developing London's economy
LPP 4.5	(2011) London's Visitor Infrastructure
LPP 5.1	(2011) Climate Change Mitigation
LPP 5.10	(2011) Urban Greening
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.21	(2011) Contaminated land
LPP 5.3	(2011) Sustainable design and construction
LPP 5.4	(2011) Retrofitting

- LPP 5.7 (2011) Renewable energy
- LPP 6.1 (2011) Strategic Approach
- LPP 6.10 (2011) Walking
- LPP 6.11 (2011) Smoothing Traffic Flow and Tackling Congestion and reducing traffic
- LPP 6.13 (2011) Parking
- LPP 6.5 (2011) Funding Crossrail and other strategically important transport infrastructure
- LPP 6.9 (2011) Cycling
- LPP 7.1 (2011) Building London's neighbourhoods and communities
- LPP 7.13 (2011) Safety, security and resilience to emergency
- LPP 7.14 (2011) Improving air quality
- LPP 7.15 (2011) Reducing noise and enhancing soundscapes
- LPP 7.2 (2011) An inclusive environment
- LPP 7.3 (2011) Designing out crime
- LPP 7.4 (2011) Local character
- LPP 7.6 (2011) Architecture
- LPP 8.2 (2011) Planning obligations

5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- **15th October 2014**

5.2 Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

The scheme was advertised in the local press on 25th September 2014. In addition, 128 neighbouring households, amenity groups, and local businesses were notified of the proposal on 22th September 2014 with the consultation period ending on 13th October 2014. No responses have been received raising concerns with the proposed development scheme.

NATS

The proposed development has been examined from a technical safeguarding aspect and does not conflict with our safeguarding criteria. Accordingly, NATS (En Route) Public Limited Company ("NERL") has no safeguarding objection to the proposal.

GLASS

Recommend Archaeological Condition

The report confirms that the planning application lies in an area where heritage assets of archaeological interest are recorded although the precise nature of remains within the site is uncertain. The report draws attention to the potential for remains 19th to 20th buildings, including part of a greyhound track, but I do not consider these to be of significant archaeological interest. The potential for as yet unrecorded prehistoric (and Roman) buried remains is significant as they would form part of the extensive Heathrow plateau landscapes which are the justification for the Archaeological Priority Zone, and notably is only c500m from the nationally important site of Caesar's Camp - an Iron Age hillfort containing a temple.

The National Planning Policy Framework (Section 12) and the London Plan (2011 Policy 7.8) emphasise that the conservation of archaeological interest is a material consideration in the planning process. Paragraph 128 of the NPPF says that applicants should submit desk-based assessments, and where appropriate undertake field evaluation, to describe the significance of heritage assets and how they would be affected by the proposed development. This information should be supplied to inform the planning decision. If planning consent is granted paragraph 141 of the NPPF says that applicants should be required to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) and to make this evidence publicly available.

The proposed development is relatively small-scale compared to the mineral extraction and airport developments which have taken place around Heathrow and remains are likely to have been damaged by modern redevelopment so the potential for contributing to understanding of landscape-scale patterns is likely to be comparatively modest although the possibility of individual discoveries of higher importance cannot be discounted. I note that the proposed development involves a western ground level extension to the existing building, a raised northern extension and landscaping. Unfortunately the assessment does not clearly map the extent of proposed groundworks and I remain doubtful as to whether the northern extension and related landscaping can really be constructed without significant impact on archaeological remains which may survive beneath the car park; and therefore whether the proposal for a watching brief would provide adequate mitigation.

On balance I consider development would not cause sufficient harm to justify refusal of planning permission provided that a condition is applied to require an investigation to be undertaken to advance understanding. The archaeological interest should be conserved by attaching a condition as follows:

REASON: Heritage assets of archaeological interest are expected to survive on the site. The planning authority wishes to secure the provision of appropriate archaeological investigation, including the publication of results.

Condition

A) No development shall take place until the applicant (or their heirs and successors in title) has secured the implementation of a programme of archaeological investigation in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the local planning authority in writing.

B) No development or demolition shall take place other than in accordance with the Written Scheme of Investigation approved under Part (A).

C) The development shall not be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under Part (A), and the provision made for analysis, publication and dissemination of the results and archive deposition has been secured.

Informative:

The written scheme of investigation will need to be prepared and implemented by a suitably qualified archaeological practice in accordance with English Heritage Greater London Archaeology guidelines. It must be approved by the planning authority before any on-site development related activity occurs

Depending upon clarification of the extent and nature of groundworks (which will need to be specified in the written scheme of investigation referred to above) I envisage that the archaeological fieldwork would comprise either a watching brief as suggested in the desk-based assessment or an archaeologically controlled 'strip, map and sample' excavation of the areas of impact.

Heathrow

The proposed development has been examined from an aerodrome safeguarding perspective and could conflict with safeguarding criteria unless any planning permission granted is subject to the conditions detailed below:

Height Limitation on Buildings and Structures

No building or structure of the development hereby permitted shall exceed 50m AOD.

Reason: Development exceeding this height would penetrate the Obstacle Limitation Surface (OLS) surrounding Heathrow Airport and endanger aircraft movements and the safe operation of the aerodrome.

Submission of a Bird Hazard Management Plan

Development shall not commence until a Bird Hazard Management Plan has been submitted to and approved in writing by the Local Planning Authority. The submitted plan shall include details of:

- Management of any flat/shallow pitched/green roofs on buildings within the site which may be attractive to nesting, roosting and "loafing" birds. The management plan shall comply with Advice Note 8 'Potential Bird Hazards from Building Design' attached * See para below for information *

The Bird Hazard Management Plan shall be implemented as approved on completion of the development and shall remain in force for the life of the building. No subsequent alterations to the plan are to take place unless first submitted to and approved in writing by the Local Planning Authority.

Reason: It is necessary to manage the flat roofs in order to minimise its attractiveness to birds which could endanger the safe movement of aircraft and the operation of Heathrow Airport.

The Bird Hazard Management Plan must ensure that flat/shallow pitched roofs be constructed to allow access to all areas by foot using permanent fixed access stairs ladders or similar. The owner/occupier must not allow gulls, to nest, roost or loaf on the building. Checks must be made weekly or sooner if bird activity dictates, during the breeding season. Outside of the breeding season gull activity must be monitored and the roof checked regularly to ensure that gulls do not utilise the roof. Any gulls found nesting; roosting or loafing must be dispersed by the owner/occupier when detected or when requested by BAA Airside Operations staff. In some instances it may be necessary to contact BAA Airside Operations staff before bird dispersal takes place. The owner/occupier must remove any nests or eggs found on the roof.

The breeding season for gulls typically runs from March to June. The owner/occupier must obtain the appropriate licences where applicable from Natural England before the removal of nests and eggs.

Submission of a Construction Management Strategy - England

Development shall not commence until a construction management strategy has been submitted to and approved in writing by the Local Planning Authority covering the application site and any adjoining land which will be used during the construction period. Such a strategy shall include the following matters:

- details of the area(s) subject to construction activity and the storage of materials and equipment
- details of cranes and other tall construction equipment (including the details of obstacle lighting) - Such schemes shall comply with Advice Note 4 'Cranes and Other Construction Issues'(available at www.aoa.org.uk/operations & safety/safeguarding. asp).
- details of temporary lighting - Such details shall comply with Advice Note 2 'Lighting Near Aerodromes' (available at www.aoa.org.uk/operations & safety/safeguarding. asp).

- height of storage areas for materials or equipment..

The approved strategy (or any variation approved in writing by the Local Planning Authority) shall be implemented for the duration of the construction period.

Reason: To ensure that construction work and construction equipment on the site and adjoining land does not breach the Obstacle Limitation Surface (OLS) surrounding Heathrow Airport and endanger aircraft movements and the safe operation of the aerodrome.

And

Reason: To ensure the development does not endanger the safe movement of aircraft or the operation of Heathrow Airport through interference with communication, navigational aids and surveillance equipment.

We would also make the following observations:

Lighting

The development is close to the aerodrome and the approach to the runway. We draw attention to the need to carefully design lighting proposals. This is further explained in Advice Note 2, 'Lighting near Aerodromes' (available at [http://www.aoa.org.uk/operation & safety/safeguarding.htm](http://www.aoa.org.uk/operation%20&%20safety/safeguarding.htm)). Please note that the Air Navigation Order 2005, Article 135 grants the Civil Aviation Authority power to serve notice to extinguish or screen lighting which may endanger aircraft.

Landscaping

The development is close to the airport and the landscaping which it includes may attract birds which in turn may create an unacceptable increase in birdstrike hazard. Any such landscaping should, therefore, be carefully designed to minimise its attractiveness to hazardous species of birds. Your attention is drawn to Advice Note 3, 'Potential Bird Hazards: Amenity Landscaping and Building Design' (available at [http://www.aoa.org.uk/operation & safety/safeguarding.htm](http://www.aoa.org.uk/operation%20&%20safety/safeguarding.htm)).

We, therefore, have no aerodrome safeguarding objection to this proposal, provided that the above conditions are applied to any planning permission.

GLA

The GLA state that London Plan policies on tourism/ leisure, employment, urban design, inclusive design, sustainable development and transport are relevant to this application.

The application broadly complies with these policies but there are a few issues that must be addressed, as follows:-

i. Hotel Use and Loss of Office Space: Principle of the development: In summary, the principle of converting and consolidating the existing office space to provide a hotel use is supported provided that the new use outweighs any harm it may cause to the loss of office space;

ii. Employment and Training: The applicant should confirm and secure as part of the s106 agreement that the scheme will provide apprenticeships and on-the-job training for local young people;

iii. Urban Design: The applicant should submit a landscaping design identifying pedestrian routes towards the hotel entrance, reduce the dominance of the vehicular access arrangements along the eastern frontage of the building and a more refined building line/ facade treatment should be introduced along the east frontage of the building.

iv. Inclusive design: The applicant should submit an inclusive design statement that ensures the development complies with inclusive design policy 7.2 of the London Plan. A hotel accessibility plan

should also be provided and secured as part of a condition.

v. Sustainable development/ energy: Further information is required to confirm that the development has been modelled under part L 2013 and the efficiency measures proposed. A BRUKL sheet including efficiency measures alone should be provided. The development should be designed to allow a connection to a district heating source should one become available in the future. Further information should be provided on the optimisation of the CHP and the location of the plant room. Further information is required on the solar PV array area and installed capacity. A scaled roof plan should also be provided illustrating the proposed installation. Additional measures aimed at achieving further carbon reductions should also be investigated.

vi. Transport: Electric vehicle charging points should be increased. Short stay cycle parking should be provided. The submitted interim travel plan should be revised to include a baseline mode split and resubmitted to be secured via s106. The submission of a delivery and servicing plan (DSP) and a full construction logistics plan (CLP) should be secured by condition. CIL payments should also be secured.

OFFICER COMMENT: The officer's comments on issues raised by the GLA response are provided below:

i) Loss of employment land: The stage 1 response raised no issues in principle, but indicated that Hillingdon Council should, nonetheless, ensure that the potential loss of employment premises is acceptable in terms of the borough's supply of employment floorspace. This has been considered in detail by officers and is addressed within the Principle section of the committee report. In this instance the loss of employment land is considered acceptable.

ii) Employment and Training: The stage 1 response urged the Council to secure apprenticeships and on-the-job training for local young people. An employment strategy has been secured as part of the section 106 agreement.

iii) Urban design: The GLA Stage 1 response indicated that the scheme should seek to provide clear pedestrian routes towards the hotel entrance, reduce the dominance of the vehicular access arrangements along the eastern frontage of the extended building and provide a more refined building line/ facade treatment along the east frontage of the building. Following amendments to the landscaping strategy and to the north eastern facade to provide a continuous consolidated frontage at ground floor level, officers consider that the amended scheme would address the issues raised. The one way circulation arrangement under the main body of the northern extension will require an entry access along Nobel Drive. However, this is a single unidirectional entry point and, provided it is built with adequate surfacing materials it is not considered to dominate this frontage. A condition is recommended to that effect.

iv. Inclusive design: The GLA Stage 1 response indicated that the applicant should submit an inclusive design statement and a hotel accessibility plan. The applicant has submitted an inclusive design statement in compliance with policy 7.2 of the London Plan and a condition is imposed requiring the development complies with inclusive design principles.

v. Sustainable development/ energy: The GLA Stage 1 response indicated that further details of the energy strategy and carbon reduction measures were required. The applicant has submitted these details and implementation will be secured by way of conditions. The applicant has confirmed that any shortfall in carbon reductions will be mitigated with a monetary contribution as part of the section 106 agreement.

vi. Transport: The GLA Stage 1 response requested an increase in short stay cycle parking and electric vehicle charging points and requested that various matters be secured by condition and

legal agreement. The application has been amended to provide; a) short stay cycle racks at the front of the hotel entrance, b) to increase provision of electric vehicle charging points from five to ten; and, c) to provide eight disabled parking bays.

Conditions and S106 obligations meeting the above requirements have been incorporated.

Internal Consultees

Highways Engineer
Site and Transport Network

The site is located at the north-west corner of the junction of Nobel Drive and A4 Bath Road. Transport for London is the highway authority for the A4. The site has a PTAL rating of 4 (good). The site is in close proximity to Terminal 1-3, Heathrow Airport. There are pay and display car parking bays along Nobel Drive adjacent to the site.

Parking

The proposal will result in reduction of the existing from 151 car park spaces to 81 spaces. This provision approximately corresponds with 66 spaces for the hotel and 19 spaces for the banqueting suites based on a benchmark standard of 1 space for three bedrooms and 1/50sqm for function rooms. Furthermore experience from the Ramada Hotel in Long Drive, Ruislip indicates that 1 space per 5 bedrooms and 40 spaces for banqueting would allow for greater scope for marketing the banqueting events. This is considered acceptable given the shared use of the car park and can be agreed through the parking allocation and management plan.

Eight parking bays will be provided for disabled users and four bays with provision for electric vehicle charging points. In addition, there will be provision for three coach parking bays and taxi parking / drop-off.

Cycle parking provision for 20 cycles will be provided and meets Council's standards.

The proposals make provision for six motorcycle parking and meets with Hillingdon's Local Plan part 2 standard rate of 1 space per 20 car parking spaces.

Traffic Generation and Impact

When considering the vehicular traffic generation and the impacts of this development, the Transport Assessment (TA) indicates traffic impact would not be significant and materially less than the existing use on the site. A total of 39 vehicular trips (arrival and departures) are envisaged during the morning and evening peak periods. Given the reduction in car parking, the good accessibility of the site and its proximity to Heathrow Airport, no traffic modelling is considered necessary and the proposed development could be considered to have an insignificant traffic impact over and above existing use.

Access and Layout

The site will be served by vehicle accesses off Nobel Drive with a one-way internal traffic circulation.

The preferred arrangement is for entry via the new access on Nobel Drive and exit from the roundabout at the north of the site. This arrangement will result in no loss of on-street parking bays but will require relocation of four on-street car parking bays.

Highway works to alter the splitter island along Nobel Drive adjacent to the roundabout and the formation of the new site access off Nobel Drive will be required. Vehicular swept paths with 300mm safety margin will be required to support the detailed design of these works for prior approval by Hillingdon Council.

Construction Traffic

In case of any permission, a Construction Logistics Plan can be secured by way of a planning condition or s106 agreement. This should include (but not limited to):

- Construction traffic generation by development;

- Access routes;
- Contractor parking;
- Deliveries to avoid highway network peak hours and traffic sensitive hours;
- Construction staff travel plan

Travel Plan

The Council's travel plan officer should be consulted to comment on the travel plan. A full travel plan to take account of any necessary adjustments can be secured and maintained through a planning condition and/or s106 agreement as appropriate.

Conditions and S106 Obligations

The issues above regarding on-street car parking, motorcycle parking and access layout should be appropriately covered through planning conditions. The highways/transport obligations listed below should be covered within the S106 agreement.

1. Access works.
2. Part carriageway and footway resurfacing and associated works along Nobel Drive.
3. Removing/relocating on-street car parking bays, amending parking restrictions and associated costs.
4. Travel Plan.
5. Car parking allocation and management scheme.
6. Construction Logistics Plan.
7. Refuse and delivery management scheme.

EPU

Conversion and extension of existing office building to form a 200 bedroom hotel with banqueting suite, conference facilities, and rooftop restaurant, including a seven-storey extension to rear, a three storey addition at roof level, and single-storey side extension, together with the creation of a new vehicle access, and alterations to car parking and landscaping.

Due to the proximity of residential premises to the North of the site we would recommend that the following conditions are attached.

Hours of operation for the restaurant conference and banqueting facilities, up until 11pm may be suitable as this is when aircraft noise reduces

A Noise management plan to demonstrate how noise from the restaurant and banqueting facilities will be managed.

N11B Noise affecting residential property

The rating level of noise emitted from the plant and/or machinery hereby approved shall be at least 5 dB below the existing background noise level. The noise levels shall be determined at the nearest residential property. The measurements and assessment shall be made in accordance with British Standard 4142 "Method for rating industrial noise affecting mixed residential and industrial areas".

REASON

To safeguard the amenity of the surrounding area in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

N12 Air extraction system noise and odour (~ old N12)

No air extraction system shall be used on the premises until a scheme for the control of noise and odour emanating from the site has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of measures as may be approved by the LPA. Thereafter, the scheme shall be implemented and maintained in full compliance with the

approved measures.

REASON:

To safeguard the amenity of the occupants of surrounding properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

INF 20 Control of environmental nuisance from construction work (~ Informative 20)

Nuisance from demolition and construction work is subject to control under the Control of Pollution Act 1974, the Clean Air Act 1993 and the Environmental Protection Act 1990. You should ensure that the following are complied with:

- (i) Demolition and construction works should only be carried out between the hours of 0800 and 1800 on Monday to Friday and between the hours of 0800 and 1300 on Saturday. No works should be carried out on Sundays, Public or Bank Holidays;
- (ii) All noise generated during such works should be controlled in compliance with British Standard 5228, and use "best practicable means" as defined in section 72 of the Control of Pollution Act 1974;
- (iii) Measures should be taken to eliminate the release of dust, odours and other emissions caused by the works that may create a public health nuisance. Guidance on control measures is given in "The control of dust and emissions from construction and demolition: best practice guidelines", Greater London Authority, November 2006; and
- (iv) No bonfires that create dark smoke or cause nuisance to local residents should be allowed at any time.

You are advised to consult the Council's Environmental Protection Unit to seek prior approval under Section 61 of the Control of Pollution Act 1974 if you anticipate any difficulty in carrying out the works other than within the normal working hours set out above, and by means that would minimise disturbance to adjoining premises. For further information and advice, contact the Environmental Protection Unit, 3S/02 Civic Centre, High Street, Uxbridge, Middlesex UB8 1UW (tel. 01895 250155)

Land Contamination

The following information was submitted with the application for air quality:

- Air Quality Assessment for the proposed hotel development at 1 Nobel Drive, Bath Road, Hillingdon, dated July 2014

The following information was submitted with the application for land contamination:

- Desk Study Report 1 Nobel Drive, Heathrow, Middlesex by GEA Ltd, dated August 2014

Air Quality

The application site is located within the Hillingdon AQMA, declared for Nitrogen Dioxide (NO₂). The air quality assessment was undertaken using DMRB, therefore only considers traffic impacts on the site, and indicates impact from the airport has been considered as a result of using measured background data (DEFRA mapped background data for the site was not used as it was considered to be an over estimate). The assessment was considered to be conservative by the consultants as the DMRB model output was over predicting compared to monitoring information. At the four conservative locations considered at the site (i.e. not on the building facade) the NO₂ levels are all above 40 µg/m³, and considerably over that at the locations adjacent to the Bath Road.

Based on the limited air quality assessment, it indicates the proposed new use will only have a negligible impact on on-site receptor locations. No off site sensitive receptors were considered in the assessment. The assessment indicates the new development will have 83 car parking spaces (slightly fewer than the existing use) and coach parking. It indicates only a slight increase in trip generation compared to the current use, based on the assumption that public transport would be used or nearby car parking would be used as indicated in the Transport Assessment. Pick up and drop off by taxi at the front of the site is referred to in this document, although it is not clear if these

were counted in the trip generation. The air quality assessment also refers to a Transport Plan, by which I assume they mean the Draft Travel Plan in Appendix F which does not appear to have been provided, as a means of mitigation. The Transport Assessment indicates 4 spaces will have electric charging points. It is advisable to include a Travel Plan condition on any planning permission given including having some car or taxi parking spaces with fast electric charging points.

As the development is in and may cause increases in an area already suffering poor air quality the following are requested:

Section 106

Section 106 obligation up to £25,000 should be sought for contribution to the air quality monitoring network in the area.

The EU limit value for NO₂ at the facade of the building would only need to be met if staff are likely to live in at the hotel, for example. It is not clear if this is the case. I would also add it should be considered relevant if cabin crew are to stay at the hotel. The air quality assessment assumes that the NO₂ levels will fall below the limit value on the higher floors. Given the proximity of the site to the airport, this may not necessarily be the case.

The air quality assessment did not take into consideration the emissions from the on-site CHP and boilers in the assessment. However, it has considered building emissions in order to determine that the development is 'air quality neutral', although this appears to be limited on the assumption of gas being used on site to provide hot water only. The building emissions benchmark is met based on the limited assessment, which also indicates they have gone with 'worst-case' because they have assumed <70 mg/kWh standard, but ultra low NO_x boilers may be used. The BREEAM pre-assessment indicates only two points are being picked up for NO_x emissions, therefore it does not appear ultra low NO_x boilers are going to be used. In addition, the Sustainability and Energy Statement indicates the possible use of an appropriately sized CHP backed by high efficiency gas fired water heaters. The likely building emission standards are unclear based on the available information. The following condition is recommended for inclusion on any permission that may be given to ensure, as a minimum the building emissions benchmark is met and the CHP emission requirement for air quality neutral is also met.

Air Quality Condition 2 - Details of Energy Provision

Before the development is commenced details of any plant, machinery and fuel burnt, as part of the energy provision and the location of the flue at the development shall be submitted to the LPA for approval. This shall include pollutant emission rates at the flue with or without mitigation technologies. The use of ultra low NO_x emission gas-fired CHPs and boilers is recommended. The development should as a minimum be 'air quality neutral' and demonstrably below the building emissions benchmark, with a low emission CHP.

REASON:

To safeguard the amenity of neighbouring properties in accordance with policy OE1 of the Hillingdon Unitary Development Plan.

Notes: This condition relates to the operational phase of residential and commercial development and is intended for the protection of future residents in a designated AQMA and Smoke Control Area. Advice on the assessment of CHPs is available from EPUK at: http://www.iaqm.co.uk/text/guidance/epuk/chp_guidance.pdf. An area up to a distance of 10 times the appropriate stack height needs to be assessed. Guidance on air quality neutral is available at: <https://www.london.gov.uk/priorities/planning/consultations/draft-sustainable-design-and-construction>. They should contact Planning Specialists if they have any queries.

Land Contamination Officer

A desk study has been submitted with regard to land contamination. It did not identify any risk to the site. It does not appear as if the Council were consulted as part of the desk study, although it does state the site is located on historical worked ground, and former greyhound track. Council records indicate a landfill at this location although limited ground investigation for the site does not indicate fill as such. The standard contaminated land condition is recommended for inclusion in any permission given. The site investigation should be carried out taking into consideration the proposed design of the extension. In the absence of information about the foundation design and gas protection measures incorporated into the existing building, it is recommended that ground gas monitoring forms a part of the investigation. The soils condition is also recommended, and soil testing needs to be carried out to demonstrate the development is suitable for use. It is advisable the source of the soil is agreed with the LPA prior to any importing of soil to the site.

Contaminated Land Condition

(i) The development hereby permitted shall not commence until a scheme to deal with contamination has been submitted in accordance with the Supplementary Planning Guidance Document on Land Contamination and approved by the Local Planning Authority (LPA). The scheme shall include all of the following measures unless the LPA dispenses with any such requirement specifically and in writing:

(a) A desk-top study carried out by a competent person to characterise the site and provide information on the history of the site/surrounding area and to identify and evaluate all potential sources of contamination and impacts on land and water and all other identified receptors relevant to the site;

(b) A site investigation, including where relevant soil, soil gas, surface and groundwater sampling, together with the results of analysis and risk assessment shall be carried out by a suitably qualified and accredited consultant/contractor. The report should also clearly identify all risks, limitations and recommendations for remedial measures to make the site suitable for the proposed use; and

(c) A written method statement providing details of the remediation scheme and how the completion of the remedial works will be verified shall be agreed in writing with the LPA prior to commencement, along with details of a watching brief to address undiscovered contamination.

(ii) If during development works contamination not addressed in the submitted remediation scheme is identified, the updated watching brief shall be submitted and an addendum to the remediation scheme shall be agreed with the LPA prior to implementation; and

(iii) All works which form part of the remediation scheme shall be completed and a comprehensive verification report shall be submitted to the Council's Environmental Protection Unit before any part of the development is occupied or brought into use unless the LPA dispenses with any such requirement specifically and in writing.

REASON: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems and the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

Condition to minimise risk of contamination from garden and landscaped areas

Before any part of the development is occupied, site derived soils and imported soils shall be independently tested for chemical contamination, and the results of this testing shall be submitted and approved in writing by the Local Planning Authority. All soils used for gardens and/or landscaping purposes shall be clean and free of contamination.

Note: The Environmental Protection Unit (EPU) must be consulted for their advice when using this condition.

REASON

To ensure that the occupants of the development are not subject to any risks from soil contamination in accordance with policy OE11 of the Hillingdon Unitary Development Plan Saved Policies (September 2007).

References:

Hillingdon's Land Contamination Supplementary Planning Guidance (SPG)
http://www.hillingdon.gov.uk/media/pdf/p/e/land_contamination.pdf

Waste

Hotel

I would estimate the waste arising from the development to be 50000 litres

The client for the building work should ensure that the contractor complies with the Duty of Care requirements, created by Section 33 and 34 of the Environmental Protection Act.

Conclusion: acceptable.

Accessibility Officer

The site, located adjacent to Heathrow Airport, is rectangular and covers an area of 0.62 Ha, and is occupied by a substantial 4 storey office building. The proposal is for the conversion of the existing office building into a hotel comprising 200 bedrooms and a 600 person banqueting/conference facility.

The design of this development application demonstrates some commitment to the principles of access and inclusion. The detail provided should be relative to the scale and significance of the development.

The revised Design & Access Statement is fine and no further changes are needed. However, the layout of the ensuite bathrooms are not correctly illustrated on plan. A condition should be imposed requiring that 20 ensuite bathrooms to be specified to BS 8300:2009 are provided.

Conclusion: acceptable, subject to the attachment of appropriate conditions.

Urban design

I have noted these revisions and the application proposal as a whole. I confirm that I do not have any objection to the scheme on urban design grounds, whilst large, is considered to be well designed and not inappropriate in this location.

If you intend to recommend this application for approval, please can you ensure that the following items are covered by condition:

- all external materials, colours and finishes
- glazing details, including the screens to the atrium and foyer area and projecting windows
- the design of the canopies and fascias to main frontage, principal entrance and at roof level
- roof top garden planting and landscape proposals for the site
- details of boundary enclosures
- details of hand rails and screens to balconies and terraced areas
- location and type of PVs etc if they are to be included

Floodwater

The site is indicated to have some surface water ponding on the site and the redevelopment of the site should reduce the surface water run off from the site in line with the requirements of the London Plan of a minimum of 50%. It should also be demonstrated that there is appropriate and sufficient foul infrastructure to support this redevelopment.

Therefore I request the following condition:

Prior to commencement, a scheme for the provision of sustainable water management shall be submitted to, and approved in writing by the Local Planning Authority. The scheme shall clearly demonstrate how it incorporates sustainable urban drainage in accordance with the hierarchy set out in Policy 5.15 of the London Plan and will:

- i. provide information on all Suds features including the method employed to delay and control the water discharged from the site and:
 - a. calculations showing storm period and intensity and volume of storage required to control surface water and size of features to control that volume.
 - b. any overland flooding should be shown, with flow paths depths and velocities identified as well as any hazards, (safe access and egress must be demonstrated).
 - c. measures taken to prevent pollution of the receiving groundwater and/or surface waters;
 - d. how they or temporary measures will be implemented to ensure no increase in flood risk from commencement of construction.
- ii. and demonstrate capacity into the receiving foul sewer network
- iii. provide a management and maintenance plan for the lifetime of the development of arrangements to secure the operation of the scheme throughout its lifetime. Including appropriate details of Inspection regimes, appropriate performance specification, remediation and timescales for the resolving of issues.
- iv. provide details of the body legally responsible for the implementation of the management and maintenance plan.

The scheme shall also demonstrate the use of methods to minimise the use of potable water through water collection, reuse and recycling and will:

- v. incorporate water saving measures and equipment.
- vi. provide details of water collection facilities to capture excess rainwater;
- vii. provide details of how rain and grey water will be recycled and reused in the development.

Thereafter the development shall be implemented and retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure that surface water run off is controlled to ensure the development does not increase the risk of flooding contrary to Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (Nov 2012) Policy 5.12 Flood Risk Management of the London Plan (July 2011) and National Planning Policy Framework (March 2012) and the Planning Practice Guidance (March 2014). To be handled as close to its source as possible in compliance with Policy 5.13 Sustainable Drainage of the London Plan (July 2011), and conserve water supplies in accordance with Policy 5.15 Water use and supplies of the London Plan (July 2011).

106 Officer

Description

Conversion and extension of existing office building to form a 200 bedroom hotel with banqueting suite, conference facilities, and rooftop restaurant, including a seven-storey extension to rear, a three storey addition at roof level, and single-storey side extension, together with the creation of a new vehicle access, and alterations to car parking and landscaping.

- 200 bed hotel
- roof top restaurant
- 7 story rear extension
- 3 story addition at roof level
- single story side extension

HEADS OF TERMS

1. Highways Works: S278/S38 for required Highways Works

2. Construction Training: A financial contribution to the sum of: Training costs: £2500 per £1m build cost plus Coordinator Costs - £9,600 per phase or an in kind scheme to be provided.
3. Air Quality Monitoring: A financial contribution to the sum of £25,000.
4. Travel Plan: to include £20,000 Bond
5. Employment Training Strategy
6. Hospitality Training
7. Hoppa Bus Provision
8. Project Management & Monitoring Fee: A financial contribution equal to 5% of the total cash contributions.

Landscape

COMMENTS (Re: Revised D&AS, dated 07/11/2014 and post-application discussions)

- The amended scheme includes the removal of the Yew hedge which defines the boundary of the surface level car park at the north end of the site. The current proposal is to remove the hedge and provide a more architectural edge. Illustrations indicate planting at higher levels on terraces and a roof garden.
- A Landscape Strategy drawing No. AE1449-01, by Anne Evans, dated November 2014, includes a brief tree assessment and identifies 6No. trees (from a total of 15No.) which are to be removed from the south and south-east site edges. There are 2No. 'B' grade trees and 4No. 'C' grade trees identified to be removed. The trees 'to be retained' include 3No. 'A' grade specimens and the rest are 'B' category. There is no objection to the tree (retention /removal) strategy.
- Much of the shrubby structural planting along the Bath Road frontage is to be retained with key areas refreshed with new and enhanced tree and shrub planting.
- The strategy acknowledges the need for the planting and management regime to conform to the CAA's anti-bird strike guidelines.
- The Landscape Strategy drawing indicates the retention of the hedge and tree planting along the northern part of the site. As noted above, this detail has now been superseded but will feature new tree planting and roof top planting clearly illustrated within the D&AS.

Recommendations

No objection subject to conditions COM8, COM9 (parts 1,2,3,4,5 and6) and COM10.

Sustainability Officer

The amended information provided at this stage is satisfactory, although there is a sudden leap in savings largely as a result of an improved CHP. The information on the CHP, specifications, inputs, outputs, etc... need to be finalised and information is still required on how it will connect to the site. To that end, a condition is recommended but I am satisfied at this stage.

There is also a shortfall which equates to 7tonnes per year, which will require an offsite contribution of £12,600 (@£60/tCO₂ for 30years).

Condition

Prior to the commencement of development a detailed energy assessment shall be submitted to and approved in writing by the Local Planning Authority.

The assessment shall clearly set out the baseline energy performance of the development, and the detailed measures to reduce CO₂ emissions in accordance with the approved energy strategy submitted with the planning application. The assessment shall provide detailed specifications of the measures and technology set out in outline energy assessment. In particular, the assessment shall provide full details of the proposed CHP unit, including inputs and outputs, location and how it connects to the development. The assessment shall also include full details of any further low or zero carbon technology including type, specification, and a detailed plans and elevations where appropriate.

Finally, the assessment must include appropriate measuring and monitoring proposals and how the information will be submitted to the Local Planning Authority to demonstrate CO2 reductions are being met.

The development must proceed in accordance with the approved scheme.

REASON

To ensure appropriate carbon savings are delivered in accordance with London Plan Policy 5.2.

7. MAIN PLANNING ISSUES

7.01 The principle of the development

The proposal seeks to convert and extend the existing office building to provide a 200 bed hotel with conference/banqueting facilities (for up to 600 people) and a restaurant with a terrace at roof level.

The National Planning Policy Framework (NPPF) supports sustainable economic growth and sets out a presumption in favour of sustainable development by planning for prosperity, planning for people and planning for places. The primary objective of development management is to foster the delivery of sustainable development, and significant weight should be attached to the benefits of economic and housing growth. The National Planning Policy Framework (NPPF) also requires the Council to make the most efficient use of land by maximising the re-use of previously developed land and the conversion of existing buildings.

In justifying the loss of office space the applicant has submitted an Office Market Overview Report prepared by Strutt & Parker (29th July 2014) with a sequential test and sufficient evidence to demonstrate that the site whilst vacant for a period in excess of 12 months has been marketed without success. It is also clear from the levels of the deterioration of the buildings confirmed with the visit to the site that the site has been vacant for approximately two years although it is noticeable that the site itself has been well secured and is well maintained.

The extent of the marketing information submitted to the Council as part of this application is considered genuine and demonstrates that the site is no longer viable or sought for office related uses. Therefore, the principle of redeveloping these business premises, requiring the loss of the existing use is acceptable as further compatible employment generating uses will be provided within the site.

Notwithstanding this, the London Plan 2011 identifies a need for a net increase of 40,000 hotel rooms across London, and accordingly the provision of new visitor facilities is encouraged by Policy 4.5. The London Vision for Tourism document also identifies a need for additional hotel accommodation in London, the majority of which would be required by 2016.

The Background Technical Report Tourism Study (January 2007) of the Hillingdon Development Framework also identifies the Heathrow area as an areas where further visitor accommodation could be established even though the area is largely constrained both by competition from other land uses (especially airport related activities) and land use designations including Green Belt and employment land.

Strategic Policy E2 of the Local Plan Part 1 and Policy T4 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) encourage appropriate hotel and conference facility provision in the borough. In particular, Policy E2 seeks to encourage an increase in

hotel provision, amongst other locations, within the Heathrow perimeter and other sustainable Locations. Policy T2 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) encourages the provision of a range of accommodation and conference facilities on sites easily accessible from Heathrow airport, provided the development does not conflict with the aim to maintain and improve the environment.

The present policy framework indicates support for establishing this land use within this part of the borough. As such, providing site specific issues can be met no objections are raised to the principle of providing a hotel on this site.

7.02 Density of the proposed development

The density matrix under Table 3.2 of the London Plan 2011 is only applicable to residential uses falling within Class C3. In the case of the proposed scheme incorporating a 200 bed hotel with conferencing facilities and a restaurant it will be the height and bulk of the development that will be crucial in determining whether an acceptable design response for this site and its immediate setting has been achieved.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

English Heritage have confirmed that whilst the site lies in an area where archaeological remains may be anticipated, they have no objections to the proposed development subject to an appropriate condition requiring the applicant to secure the implementation of a programme of archaeological work in accordance with a written scheme of investigation. The Council's Conservation Officer has raised no objection to the proposal.

7.04 Airport safeguarding

BAA Safeguarding, National Air Traffic Services (NATS) and Heathrow Airport have been consulted and confirmed that the proposed development would not conflict with airport safeguarding criteria subject to conditions.

7.05 Impact on the green belt

Policy OL5 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) seeks to protect the Green Belt from nearby developments which may prejudice its visual amenity. Whilst the proposed extensions to the building would be visible from the Green Belt to the north, the existing two-storey and three-storey residential properties in Marlborough Crescent and David Close provide a buffer between this land and the proposed building. Seen in context with other large buildings along Bath Road, and Heathrow Airport beyond, it is not considered that the proposed development would have any significant visual impact on the Green Belt.

7.07 Impact on the character & appearance of the area

Policies BE13 and BE19 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) seeks to ensure that new development will complement and improve the character and amenity of the area.

This scheme has been subject to considerable discussion with the applicant and revised plans have been submitted that are considered to address all the issues raised. The design of the extensions are appropriate to the site and surrounds as the proposed design, materials, bulk and massing are proportionate and provide a continuous and harmonious relationship with the existing building whilst the retention of the gap with the western boundary and the provision of set backs at roof level will ensure that the scheme integrates successfully with the surrounding area.

The curved frontage provides a street presence for the conference facilities and amendments to the scheme ensure that the provision of a continuous but permeable frontage with glazed aluminium fins alongside the northern section of the Nobel Drive

frontage will provide an adequate but also effective screening to the undercroft floor car park, thereby addressing the concerns raised by the Greater London Authority. Furthermore, the applicant has demonstrated that an acceptable and operational relationship can be established with the 560 bed hotel scheme allowed (with ref.38807/APP/2008/3493) at the Airport Bowl site on the opposite side of Nobel Drive.

Accordingly, the Council's Conservation and Urban Design Officer has raised no objection to the proposal and stated that the building albeit "large" is "well designed and not inappropriate in this location".

Conditions are recommended to ensure that the details of all external materials, colours and finishes, glazing details, including the screens to the atrium and foyer area and projecting windows, the design of the canopies and fascias to main frontage, principal entrance and at roof level, roof top garden planting and landscape proposals for the site, details of boundary enclosures details of hand rails and screens to balconies and terraced areas location and type of PVs etc if they are to be included.

The proposal is therefore considered to be in accordance with Policy BE1 (Built Environment) of the Hillingdon Local Plan: Part One - Strategic Policies (November 2012) and Policies BE13 and BE19 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

7.08 Impact on neighbours

Policies BE20, BE21 and BE24 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) seeks to ensure that new developments do not have a detrimental impact on the residential amenity of neighbouring occupiers through loss of light, dominance or loss of privacy.

It is not considered that there would be a material loss of amenity to neighbouring properties, as the proposed extensions would be sited a sufficient distance away from adjoining properties. The proposed extensions are not considered to be over-dominant when viewed in conjunction with the existing building, and the wider context of the site, with numerous large commercial buildings, and as such, is unlikely to impact on the visual amenities of neighbouring properties from loss of light, over-dominance or loss of privacy.

The applicant has carried out a daylight and sunlight assessment in order to assess the impact of the proposed development on its surroundings, particularly in respect of the Holiday Inn hotel to the west and the residential properties in 1-19 Marlborough Crescent to the north. The report shows that the proposed development will have a negligible impact on the residential properties as it would not result in a decrease in the lighting levels below the recommended percentages of annual daylight or sunlight hours received at these properties below the limits stated in the BRE Report.

A number of bedroom windows on the eastern side of the Holiday Inn hotel will experience a material reduction in the levels of daylight but will still receive the recommended percentage of annual sunlight hours. This is not considered to be an issue given the transient nature of the hotel residents. As such, the impact of the proposed development on these properties, in terms of daylight and sunlight, is considered to be acceptable.

Policy OE1 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) states permission will not be granted for uses which are likely to become detrimental to the character or amenities of surrounding properties. Given the nearby neighbouring residential properties, it is considered that suitable noise conditions should be included on any grant of

permission to ensure the scheme will not give cause to noise annoyance to surrounding properties.

The proposed development is considered to be consistent with policies BE20, BE21 and BE24 of the UDP.

7.09 Living conditions for future occupiers

The proposal is for a hotel and there will be no permanent future occupiers. There are no policies or standards that relate to hotel accommodation.

7.10 Traffic impact, Car/cycle parking, pedestrian safety

The site has a PTAL of 4 (good) with a bus stops just outside the site. There are currently 151 car parking spaces on site for the extant office use. The 200 bed hotel proposal will reduce these to 83 parking spaces with 64 parking spaces allocated for the hotel and 19 spaces for the banqueting/ conference rooms. Eight parking bays will be provided for disabled users and ten bays will be provided with electric vehicle charging points. In addition, there will be provision for one coach parking bay with a further two dedicated coach drop off points together with taxi parking / drop-off. Six motorcycle bays and 20 covered cycle parking spaces and shower facilities for staff are to be provided. An additional four short term cycle racks will be provided at the hotel entrance. The access and circulation arrangements, with a clockwise one way system through the site and under the main body of the buildings, are considered acceptable.

The applicant has submitted a transport assessment to justify the reduction in parking spaces provided within the site, which is a relatively limited number of parking spaces. It argues that, due to the site's high accessibility to public transport, associated with its close proximity to Heathrow, and the international clientele it would attract, that a vast number of guests would travel via public transport or coach, rather than by private car, and that the proposal would generate an insignificant traffic impact over the existing use.

A dedicated shuttle bus would be provided between the hotel and all airport terminals at a frequency of every 15 minutes throughout the day and every 30 minutes throughout the evening and night. In addition most hotels along the Bath Road corridor are served by the Heathrow Airport Hotel Hoppa Bus, which runs regularly between the Hotels and the Heathrow terminals. The applicant has agreed to link the hotel to the Hoppa Bus and this is secured via the S106.

Given the site's good public transport links, its close proximity to Heathrow, which would provide a significant proportion of the hotel's customers, and the proposed measures that would be taken to encourage the use of sustainable modes of transport, as outlined in the Transport Assessment and Interim Travel Plan, as well as the provision of a dedicated shuttle bus, it is considered that the proposed parking is acceptable.

In terms of staff, no car parking facilities would be provided and, as such, staff would be expected to travel by public transport, bicycle or walk. This part of Bath Road is served by the London Cycle Network, and the site is in close proximity to several residential areas, which are within walking distance. In addition 4 public bus routes stop within close proximity to the site. Showers and changing rooms with lockers would be provided for staff of the hotel who cycle to work.

The extant permission for a 560 bed hotel scheme allowed at the Airport Bowl on the opposite side of Nobel Drive proposes to set up Nobel Drive with a shared surface arrangement between the junction with Bath Road (A4) and the roundabout to the north west corner of the site, thereby converting this part of Nobel Drive into a 'mews' style

arrangement with traffic calming measures and a shared surface for cars, cyclists and pedestrians. This scheme would provide appropriate landscape with hard and soft features that would enhance the public realm at the main public facade to the hotel.

Whilst, the currently proposed scheme would not compromise the implementation of these improvements the new vehicular exit to Nobel Drive and the swept paths, in particular for larger vehicles, indicate that some existing parking bays would need to be relocated or could be lost. The applicant successfully demonstrated that, with minor alterations to the splitter island, the affected parking bays can be relocated closer to the roundabout (further to the north) without any loss of on-street parking.

Notwithstanding this, the records of the Parking Services operating the Pay and Display Machines in Nobel Drive show that there is not much demand for these parking spaces and, in this instance, the applicant is satisfied to either; a) secure the works and costs associated with the relocation of the on-street parking bays; or, b) the works and costs of their removal, through an appropriate s106 agreement.

The Council's Highway Officer has been consulted on the application and raises no objection to the scheme, subject to a S106 requirement that a 'hopper bus' service be used and a travel plan provided, and conditions are included to ensure the access to the building is kerbed, and that details are submitted to ensure access for refuse vehicles.

As such, it is not considered that the proposed development would have a detrimental effect on the existing highway in accordance with the aims of Policies AM2 and AM7 of the Local Plan.

The GLA's concerns have been addressed with increases in the numbers of cycle parking spaces and electric vehicle charging points.

A condition is imposed requiring that the car parking facilities provided at the hotel shall be used by hotel staff and guests only and strictly for the duration of their stay.

7.11 Urban design, access and security

In relation to the appearance, layout and scale of the proposal, the proposed building's bulk and scale is considered appropriate in the context of the Heathrow perimeter and would be well integrated within the urban pattern of the area. The proposal seeks to increase the height of the existing building whilst enlarging the building's footprint. However, due to the design, scale and position of the enlargements the building would retain its prominent address to Bath Road.

From an urban design point of view, the proposed extensions are considered to be an improvement with the lower elements presented with curved facade facing north and west which will contribute to a signature building at an appropriate scale. It is also considered that the amended continuous curved facade of the building would create a softer and more sympathetic built edge than the original open finish to the northern corner of the building.

Externally, the extensions have been amended and carefully designed to integrate with the main building whilst adopting a common system of large expanses of external glazing and panel rendering.

Accordingly, the proposed appearance, layout and scale of the proposal is considered to be acceptable.

The Council's Urban Design Officer is supportive of the overall design and quality of the proposed development and it is anticipated that the building will integrate reasonably well within its surrounds.

With regard to access arrangements, the main vehicle access is from Nobel Drive around to the eastern elevation of the building and its vehicle main entrance. A segregated area will be provided under the body of ten building for vehicles to set down passengers to both hotel and conference facilities. Hotel guests can access the hotel building by foot through the route which passes within the front landscaped area and alongside the front parking area.

7.12 Disabled access

Additional information was requested with regard to level access into the building, wheelchair access evenly distributed between the proposed elements of the hotel building and manoeuvring areas within the bathrooms, to allow for wheelchair use.

The applicant has submitted amended plans showing these requirements have been met and the scheme is therefore considered to comply with the Disability Discrimination Act, Part M of the Building Regulations, BS8300:2009, BS8300:2009+A1:2010 and BS.9999:2008.

Subject to an appropriate condition it is considered that the proposal would provide an inclusive environment for future users in accordance with Policies R16 and AM13 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and Policies 7.1 and 7.2 of the London Plan.

7.13 Provision of affordable & special needs housing

There are no requirements for commercial schemes to provide or contribute towards affordable or special needs housing provision in the borough.

7.14 Trees, landscaping and Ecology

Policy BE38 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) seeks the retention and utilisation of topographical and landscape features of merit and the provision of new planting and landscaping wherever it is appropriate.

The site as it stands is largely devoid of vegetation, albeit there are some trees that will be required to be removed to enable the development. An accurate site survey has been submitted with the application which plots trees on to the site, the tree species, and their quality and spread. None of the trees in the vicinity are protected by a Tree Preservation Order or by Conservation Area designations. In terms of landscaping, the applicant's objectives include a restricted redefinition of the existing green buffer fronting Bath Road to provide a small sitting out area with a sizeable retention of the existing planting, existing trees and the sloped lawn. The Council's Landscape Officer considers the removal of these trees is not a constraint on development due to their poor condition and landscape value.

With regard to the site layout and treatment for roof terraces in terms of planting, security measures (which include balustrades sufficiently set back from the edges of the building) and materials the scheme provides an improvement over the existing expanse of hard standing and boundary treatment with details provided of indicative hard and soft landscaping. Subject to the relevant planning conditions in respect of landscape maintenance and revisions/further detail on the planting plan the scheme is considered to provide a satisfactory landscape arrangement that complies with Policy BE38 of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012).

ECOLOGY:

The site is currently almost entirely given over to hardstanding. In view of this and the safeguarding measures detailing within the application documents in respect to trees and shrubs on neighbouring sites it is not considered the scheme will have an adverse impact on the areas ecology.

7.15 Sustainable waste management

London Plan policies 5.16 and 5.17 requires adequate provision of refuse and recycling facilities for new development and for their location to be appropriate in terms of enabling ease of collection from the site. The bin and recycle area includes a 40m³ waste compactor and is positioned in an enclosed area centrally located to the west of the site with access for staff with access from the one way system for the refuse collectors.

The level of waste and recycling store provision, its location and means of collection by refuse vehicles is considered to comply with the Council's requirements. However, the Council's waste manager recommends that there is daily waste collection and it is therefore recommended that a condition be imposed to ensure the collection of waste is carried out at times to avoid impacts on the neighbouring area on a daily basis. Subject to such a condition, the scheme is considered satisfactory and complies with the standards set out in London Plan policies 5.16 and 5.17.

7.16 Renewable energy / Sustainability

The applicant has submitted an amended Sustainability Strategy with the application which shows that a number of measures have been incorporated into the design of the hotel to reduce its energy demand. Most notable is the extensive external glazed envelope which would help to maintain a comfortable climate within the site, significantly reducing heating and cooling needs. In addition, the orientation and massing of the hotel, and the integration of a centrally located light well, maximises the amount of natural light which can penetrate the building and also allows natural light to rooms and restaurants located on upper floors, thus reducing the need for artificial lighting.

The Energy Statement demonstrates that the applicant has investigated the use of a number of renewable energy sources in order to reduce carbon emissions. A significant reduction would be achieved through the use of a CHP unit, which would provide heating and associated electricity for the hotel. A further reduction would be achieved through the use of photovoltaic panels.

Notwithstanding the above mentioned energy conservation measures, there is also a shortfall which equates to 7 tonnes of carbon per year below London Plan standards. This would require an offsite contribution towards a carbon fund of £12,600 (@£60/tCO₂ for 30years). The applicant has formally agreed to this contribution, which is to be secured in the S106 legal agreement.

7.17 Flooding or Drainage Issues

The site does not fall within a flood zone and no issues relating to flooding have been identified.

London Plan policies 5.12 and 5.13 require that development proposals should use sustainable urban drainage systems (SUDS) unless there are good reasons for not doing so. Policy EM6 Flood Risk Management in Hillingdon Local Plan: Part 1- Strategic Policies (Nov 2012) requires that surface water run off is controlled to ensure the development does not increase the risk of flooding. The scheme would need to investigate rainwater harvesting, including the provision of a ground storage tank, and permeable paving to

replace the existing large expanse of non permeable hard standing.

Subject to conditions, the proposal is considered to comply with the intentions of the Hillingdon Local Plan: Part One and Part Two Saved UDP Policies (November 2012) in respect to water management and London Plan policies 5.12 and 5.13.

The Council's Floodwater Management Officer raises no objection to the scheme, subject to the application of the relevant SUDS and sustainable water management planning condition.

7.18 Noise or Air Quality Issues

- Air Quality

The site is located within an Air Quality Management Area and, as such, an Air Quality Assessment has been submitted. In the opinion of the Council's Air Quality Officer a number of measures have been put forward, such as the shuttle bus system and dedicated cycle parking, to off-set the impact on air quality. Nonetheless there will still be an increase in nitrogen dioxide in an area that is already suffering from very poor air quality.

It is therefore considered that the applicant, given the scale of development, should contribute towards the upkeep and maintenance of a nearby air quality monitoring. A contribution of £25,000 is suggested to enable continued monitoring at the site. It is considered that the applicant should also be encouraged to investigate the use of an electric or alternative fuel bus for the shuttle runs.

- Noise

Given that the proposed building is a hotel, the Council's Noise Officer has confirmed that internal noise levels are not an issue. However, should approval be granted a condition to ensure external noise levels do not exceed 5dB below the background noise level should be added.

7.19 Comments on Public Consultations

No objections have been raised during the public consultation.

7.20 Planning obligations

The Community Infrastructure Levy Regulation 2010 (Regulations issued Pursuant to the 2008 Act) and the NPPF have put three tests on the use of planning obligations into law. It is unlawful (since 6th April 2010) to request planning obligations that do not meet the following tests:

- i. necessary to make the development acceptable in planning terms
- ii. directly related to the development, and
- iii. fairly and reasonable related in scale and kind to the development

The effect of the Regulations is that the Council must apply the tests much more strictly and is only to ask for planning obligations that are genuinely necessary and directly related to a development. Should planning obligations be requested that do not meet the policy tests the Council would have acted unlawfully and could be subject to a High Court challenge.

In this instance, on the basis of the NPPF and the Community Infrastructure Levy Regulation 2010, it is only considered reasonable to request contributions towards the following:

Non-monetary contributions:

. Highways Works S278/S38 to include the costs associated with the re-location or otherwise removal of the on-street parking in Nobel Drive, including the alterations to the traffic management order, removal and/ or re-location of parking bays and removal or re-location of ticket machine(s)

. A full and formal Travel Plan with associated £20,000.00 bond and including Sustainable Transport Measures (such as a hopper bus service) is required to be submitted and agreed in writing by the LPA before occupation of the development. Thereafter, the Travel Plan is required to be reviewed annually to monitor and if required, update and/or amend the document to the satisfaction of the LPA, in order that its aims and objectives are achieved.

. A Construction Logistics Plan

. A Delivery & Service Plan.

. A full Employment Training Strategy

. Hospitality Training

Monetary contributions:

. Air Quality: a contribution in the sum of £25,000.00 is sought

. Construction Training: either a contribution equal to the formula (£2,500.00 for every £1m build cost + £9,600.00 per phase or an in kind scheme) or an in-kind training scheme equal to the financial contribution delivered during the construction period of the development with the preference being for an in-kind scheme to be delivered.

. Carbon Fund: offsite contribution of £12,600.00 (@£60/tCO₂ for 30years) to offset against the shortfall (which equates to 7 tonnes of carbon per year)

. Project Management & Monitoring Fee: a contribution equal to 5% of the total cash contributions secured from the scheme to enable the management and monitoring of the resulting agreement, is sought.

The proposal would also be liable for the London Borough of Hillingdon CIL and the Mayor of London's CIL, as the scheme provides extensions in excess of 100m². This would be collected by the Council after implementation (if permission were to be granted) and could be subject to surcharges for failure to assume liability, submit a commencement notice and late payment, or and indexation in line with the construction costs index.

7.21 Expediency of enforcement action

There are no enforcement issues with this site.

7.22 Other Issues

None.

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so

far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

None

10. CONCLUSION

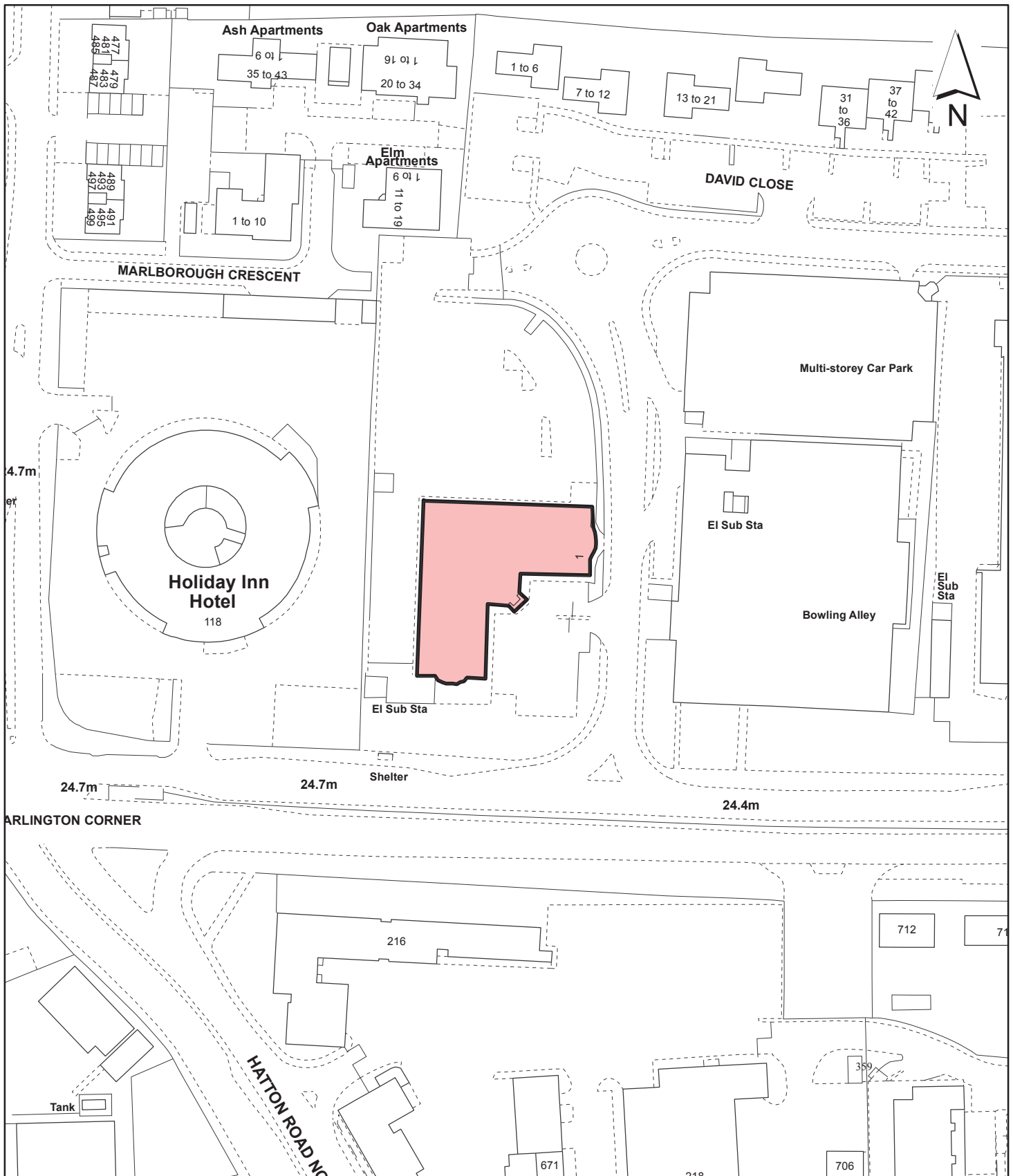
The principle of the proposed development is considered to be acceptable. The size and scale of the resulting building is considered to be appropriate for this location, and the overall building design would enhance the character and appearance of the street scene. The parking is considered to be acceptable and it is not considered that it would have any significant detrimental visual impacts on the residential amenity of neighbouring properties. The proposal complies with relevant UDP and London Plan policies and, as such, approval is recommended.

11. Reference Documents

Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012)
Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012)
London Plan (July 2011)
National Planning Policy Framework
Hillingdon Supplementary Planning Document - Accessible Hillingdon
Hillingdon Supplementary Planning Document - Noise
Hillingdon Supplementary Planning Document - Planning Obligations
Hillingdon Supplementary Planning Guidance - Air Quality
Hillingdon Supplementary Planning Guidance - Community Safety by Design
Hillingdon Supplementary Planning Guidance - Land Contamination

Contact Officer: Tiago Jorge

Telephone No: 01895 250230



Notes

 Site boundary

For identification purposes only.
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Site Address

**1 Nobel Drive
 Harlington**

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111

Planning Application Ref:

46214/APP/2014/2827

Scale

1:1,250

Planning Committee

Major Page 148

Date

March 2015



HILLINGDON
 LONDON

Report of the Head of Planning, Sport and Green Spaces

Address WEST DRAYTON GARDEN VILLAGE PORTERS WAY WEST DRAYTON

Development: Reserved matters (appearance and landscaping) in compliance with conditions 2 and 3 for Phase 5 (Block A) (82 residential units) of planning permission ref: 5107/APP/2009/2348, dated 01/10/2010, for the proposed mixed used redevelopment of the Former NATS Site.

LBH Ref Nos: 5107/APP/2014/4304

Drawing Nos:

- 1363-D2720-rev06 Porters Way Street Elevation
- 1363-D2721-rev07 Boulevard Street Elevator
- 1363-D2707-rev07 Elevation HH: Proposec
- 1363-D2706-rev07 Elevation GG: Proposec
- 1363-D2705-rev07 Elevation FF: Proposed
- 1363-D2704-rev07 Elevation EE: Proposed
- 1363-D2703-rev07 Elevation DD: Proposed
- 1363-D2702-rev08 Elevation CC: Proposed
- 1363-D2701-rev07 Elevation BB: Proposed
- 1363-D2700-rev08 Elevation AA: Proposec
- 1363-D2751-rev06 Detail Elevations 2
- 1363-D2750-rev06 Detail Elevations 1
- 1363-D2752-rev06 Detail Elevations 3
- 1363-D1100-rev01 Location Plan: Existing
- 1363-D2100-rev09 Ground Floor Plan: Proposed
- 1363-D2102-rev09 Second Floor Plan: Proposed
- 1363-D2104-rev09 Fourth Floor Plan: Proposed
- 1363-D2301-rev00 Typical Wheelchair Flats 2
- 1363-D2200-rev00 Typical Lifetime Homes Plans 1
- 1363-D2201-rev00 Typical Lifetime Homes Plans 2
- 1363-D2300-rev00 Typical Wheelchair Flats 1
- Design and Access Statement Phase 5: Block A
- Materials Schedule
- INL-E3685-300 Rev M Drainage Strategy
- INL19476-11C Landscape Proposals
- INL19476-12D Landscape Proposals
- 1363-D2103-rev10 Third Floor Plan: Proposed
- Landscape Management and Maintenance Plan
- Landscape Specification Rev A
- 1363-D2101-rev10 First Floor Plan: Proposed

Date Plans Received: 08/12/2014

Date(s) of Amendment(s): 08/12/2014

Date Application Valid: 11/12/2014

16/02/2015

1. SUMMARY

Outline Planning Permission for the comprehensive, mixed-use re-development of the former NATS site was granted 1 October 2010 (LBH Ref: No. 5107/APP/2009/2348). The outline application was for consideration of 'Means of Access', 'Layout' and 'Scale'. Matters which were reserved were 'Appearance' and 'Landscaping'.

Condition 2 of this consent relates to the approval of Appearance and Landscaping 'Reserved Matters'.

Condition 3 requires the approval of the details of the landscaping and appearance of individual phases of the development. Condition 3 also describes the extent of the information required.

This application relates to Phase 5, Block A, the final phase of the Drayton Garden Village development. It is situated in the south-west corner of West Drayton Garden Village and fronts onto Porters Way to the south. Phase 4 (Blocks B and C) is located north of the application site. To the west lies the Parkwest residential scheme, by St George, and the east boundary fronts onto the main spine road into the estate.

The proposal is for a block of flats comprising 82 units, 85 parking spaces (including undercroft parking) and the detailed landscaping of the application site. The 82 units would comprise of:

- 2 studio flats
- 27 one-bed flats
- 6 one-bed wheelchair accessible flats
- 25 two-bed four person flats
- 3 two-bed four person wheelchair accessible flats
- 18 three-bed five person flats
- 1 three-bed five person wheelchair accessible flats

Block A would be part 3 and part 4 storeys. Each unit would be provided with private amenity space and the block would have a communal garden located on a first floor podium above the rear parking court.

2. **RECOMMENDATION**

APPROVAL subject to the following:

1 RES4 **Accordance with Approved Plans**

The development hereby permitted shall not be carried out except in complete accordance with the details shown on the submitted plans,

- 1363-D1100-rev01 Location Plan: Existing
- 1363-D2100-rev09 Ground Floor Plan: Proposed
- 1363-D2101-rev10 First Floor Plan: Proposed
- 1363-D2102-rev09 Second Floor Plan: Proposed
- 1363-D2103-rev10 Third Floor Plan: Proposed
- 1363-D2104-rev09 Fourth Floor Plan: Proposed
- 1363-D2200-rev00 Typical Lifetime Homes Plans 1
- 1363-D2201-rev00 Typical Lifetime Homes Plans 2
- 1363-D2300-rev00 Typical Wheelchair Flats 1
- 1363-D2301-rev00 Typical Wheelchair Flats 2
- 1363-D2700-rev08 Elevation AA: Proposed
- 1363-D2701-rev07 Elevation BB: Proposed
- 1363-D2702-rev08 Elevation CC: Proposed
- 1363-D2703-rev07 Elevation DD: Proposed
- 1363-D2704-rev07 Elevation EE: Proposed
- 1363-D2705-rev07 Elevation FF: Proposed
- 1363-D2706-rev07 Elevation GG: Proposed
- 1363-D2707-rev07 Elevation HH: Proposed

1363-D2720-rev06 Porters Way Street Elevation
1363-D2721-rev07 Boulevard Street Elevation
1363-D2750-rev06 Detail Elevations 1
1363-D2751-rev06 Detail Elevations 2
1363-D2752-rev06 Detail Elevations 3
INL19476-11C Landscape Proposals
INL19476-12D Landscape Proposals
INL-E3685-300 Rev M Drainage Strategy

and shall thereafter be retained/maintained for as long as the development remains in existence.

REASON

To ensure the development complies with the provisions of the Hillingdon Local Plan: Part Two Saved UDP Policies (November 2012) and the London Plan (July 2011).

2 RES5 General compliance with supporting documentation

The development hereby permitted shall not be occupied until it has been completed in accordance with the specified supporting plans and/or documents:

Materials Schedule
Landscape Management and Maintenance Plan
Landscape Specification Rev A

Thereafter the development shall be retained/maintained in accordance with these details for as long as the development remains in existence.

REASON

To ensure that the development complies with the objectives of Policies BE13 and BE38 of the Hillingdon Local Plan: Part Two - Saved UDP Policies (November 2012).

INFORMATIVES

1 I52 Compulsory Informative (1)

The decision to GRANT planning permission has been taken having regard to all relevant planning legislation, regulations, guidance, circulars and Council policies, including The Human Rights Act (1998) (HRA 1998) which makes it unlawful for the Council to act incompatibly with Convention rights, specifically Article 6 (right to a fair hearing); Article 8 (right to respect for private and family life); Article 1 of the First Protocol (protection of property) and Article 14 (prohibition of discrimination).

2 I53 Compulsory Informative (2)

The decision to GRANT planning permission has been taken having regard to the policies and proposals in the Hillingdon Unitary Development Plan Saved Policies (September 2007) as incorporated into the Hillingdon Local Plan (2012) set out below, including Supplementary Planning Guidance, and to all relevant material considerations, including the London Plan (July 2011) and national guidance.

AM2	Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
AM7	Consideration of traffic generated by proposed developments.
AM8	Priority consideration to pedestrians in the design and implementation of road construction and traffic management schemes

AM9	Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
AM13	AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): - (i) Dial-a-ride and mobility bus services (ii) Shopmobility schemes (iii) Convenient parking spaces (iv) Design of road, footway, parking and pedestrian and street furniture schemes
AM14	New development and car parking standards.
BE13	New development must harmonise with the existing street scene.
BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE22	Residential extensions/buildings of two or more storeys.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
H4	Mix of housing units
H5	Dwellings suitable for large families
OE1	Protection of the character and amenities of surrounding properties and the local area
OE5	Siting of noise-sensitive developments
OE7	Development in areas likely to flooding - requirement for flood protection measures
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
OE11	Development involving hazardous substances and contaminated land - requirement for ameliorative measures
LPP 2.6	(2011) Outer London: vision and strategy
LPP 2.8	(2011) Outer London: Transport
LPP 3.1	(2011) Ensuring equal life chances for all
LPP 3.3	(2011) Increasing housing supply
LPP 3.4	(2011) Optimising housing potential
LPP 3.5	(2011) Quality and design of housing developments
LPP 3.6	(2011) Children and young people's play and informal recreation (strategies) facilities
LPP 3.7	(2011) Large residential developments
LPP 3.8	(2011) Housing Choice
LPP 3.9	(2011) Mixed and Balanced Communities
LPP 3.11	(2011) Affordable housing targets
LPP 3.12	(2011) Negotiating affordable housing (in) on individual private residential and mixed-use schemes
LPP 5.1	(2011) Climate Change Mitigation
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.3	(2011) Sustainable design and construction
LPP 5.7	(2011) Renewable energy

LPP 5.10	(2011) Urban Greening
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 6.1	(2011) Strategic Approach
LPP 6.7	(2011) Better Streets and Surface Transport
LPP 6.10	(2011) Walking
LPP 6.13	(2011) Parking
LPP 7.1	(2011) Building London's neighbourhoods and communities
LPP 7.2	(2011) An inclusive environment
LPP 7.3	(2011) Designing out crime
LPP 7.4	(2011) Local character
LPP 7.5	(2011) Public realm
LPP 7.6	(2011) Architecture
LPP 7.14	(2011) Improving air quality
LPP 7.15	(2011) Reducing noise and enhancing soundscapes

3 115 Control of Environmental Nuisance from Construction Work

Nuisance from demolition and construction works is subject to control under The Control of Pollution Act 1974, the Clean Air Acts and other related legislation. In particular, you should ensure that the following are complied with:-

A. Demolition and construction works which are audible at the site boundary shall only be carried out between the hours of 08.00 and 18.00 hours Monday to Friday and between the hours of 08.00 hours and 13.00 hours on Saturday. No works shall be carried out on Sundays, Bank or Public Holidays.

B. All noise generated during such works shall be controlled in compliance with British Standard Code of Practice BS 5228:2009.

C. Dust emissions shall be controlled in compliance with the Mayor of London's Best Practice Guidance' The Control of dust and emissions from construction and demolition.

D. No bonfires that create dark smoke or nuisance to local residents.

You are advised to consult the Council's Environmental Protection Unit (www.hillingdon.gov.uk/noise Tel. 01895 250155) or to seek prior approval under Section 61 of the Control of Pollution Act if you anticipate any difficulty in carrying out construction other than within the normal working hours set out in (A) above, and by means that would minimise disturbance to adjoining premises.

3. CONSIDERATIONS

3.1 Site and Locality

The wider application site was previously occupied by the National Air Traffic Services (NATS) as the main air traffic control centre for the southern England and London airports. Since 2008, the NATS operation has relocated to the new London Area Control Centre at Swanwick, near Fareham in Hampshire. The application site, measuring some 12.6ha, is now a major construction site that also includes an increasing number of completed and newly occupied homes together with an increasingly extensive and well landscaped public realm.

This Reserved Matters application relates to Phase 5, Block A (82 residential units), the final phase of the Drayton Garden Village development. It is situated in the south-west corner of West Drayton Garden Village and fronts onto Porters Way to the south. Phase 4 (Blocks B and C) is located north of the application site. To the west lies the Parkwest residential scheme, by St George, and the east boundary fronts onto the main spine road into the estate.

The site is located within a PTAL level of 1b.

3.2 Proposed Scheme

Outline Planning Permission for the comprehensive, mixed-use re-development of the former NATS site was granted 1 October 2010 (LBH Ref: No. 5107/APP/2009/2348).

The Committee has already approved a number of reserved matters applications for sites within phases 1 and 2. This application seeks approval for the Appearance and Landscaping of Phase 5, comprising 82 flats located within an apartment block (Block A), 85 parking spaces (including undercroft parking) and the detailed landscaping of the application site.

Proposed Housing

- 2 studio flats
- 27 one-bed flats
- 6 one-bed wheelchair accessible flats
- 25 two-bed four person flats
- 3 two-bed four person wheelchair accessible flats
- 18 three-bed five person flats
- 1 three-bed five person wheelchair accessible flats

Block A would be part 3 and part 4 storeys.

Materials

- i) Facing Brickwork - Hammersmith London Stock (Yellow)
- ii) Render - White and Grey
- iii) Roof - Flat roof System
- iv) Rainwater goods - UPVC
- v) Windows & French Doors - white colour UPVC
- vii) Galvanised steel balconies - black
- viii) Galvanised metal railings - black

Amenity Space

Each unit would be provided with private amenity space and the block would have a communal garden located on a first floor podium above the rear parking court. Additional communal amenity space would be provided within this Phase. Phase 5 (Block A) is located south-east of Spring Green, one of two primary open spaces on the wider NATS site, which provides a children's play area.

Car Parking Provision

This Phase would provide 85 parking spaces, of which 10 will be for disabled parking. 5 parking spaces would have electric charging points. 84 cycle spaces would be provided within 3 cycle stores located on the ground floor of the apartment block. The parking would be located west of Block A and would be partly located underneath the first floor amenity podium.

Accessible Housing

All of the residential units would be constructed to Lifetime Homes standards. The original masterplan required a total of 10% wheelchair accessible units to be provided throughout the whole of the NATS site, which equates to 78 wheelchair accessible units. 10 wheelchair accessible units would be provided within this part of the site, whilst the remaining wheelchair accessible units would be provided within other phases. The scheme for Phase 5 would provide 10 disabled parking spaces.

Site Access

All vehicular and pedestrian access points to the site would be via the permanent on-site roads and pavements linking Porters Way to the separate areas of the development.

Landscaping

A comprehensive schedule of hard landscaping materials has been provided together with detailed supporting information in respect of the soft landscape planting. These are of an acceptable quality and range.

3.3 Relevant Planning History

5107/APP/2009/2348 Former National Air Traffic Services (Nats) Headquarters Porters Wa

Proposed mixed-use redevelopment comprising: 773 dwellings comprising 12no. studios, 152 no. 1-bedroom flats, 316no. 2-bedroom flats, 21no. 2-bedroom houses, 23no. 3-bedroom flats, 181 no. 3-bedroom houses, 59no. 4-bedroom houses and 9no. 5-bedroom houses; Class D1 Primary Healthcare facility including room for joint community use (up to 1085sqm gea); Class C2 Nursir Home (up to 3630sqm gea); Classes A1-A3 Shop units to complement Mulberry Parade (up to 185sqm gea, depending on size of Primary Healthcare facility); Class B1 Business units including site management office (up to 185sqm gea); Energy Centre (up to 220sqm gea) with combined heat and power unit; foul water pumping station; associated access roads from Porter Way (and excluding all access including pedestrian and bicycle access from Rutters Close); 1085 car parking spaces; cycle parking; public open space areas; cycleways and footpaths; and landscaping works (Outline Application)

Decision: 01-10-2010 Approved

5107/APP/2014/4424 West Drayton Garden Village Porters Way West Drayton

Details of traffic and car parking arrangements, demolition and construction management plan, access, energy strategy, Bird Hazard Management Plan, waste storage, security measures, CCTV, ingress of polluted air, levels, rainwater / grey water recycling, foul and surface water, construction logistics plan, programme of archaeological work, drainage strategy, Ecological Mitigation and Management Plan, badger survey, and tree protection, in compliance with conditions 5, 6, 7, 8, 9, 10, 13, 14, 17, 20, 30, 45, 48, 50, 51, 52, 53, 61 relating to phase 5 Bloc A of planning permission ref:5107/APP/2009/2348 dated 19/11/2009 (Mixed development)

Decision:

5107/APP/2015/18 West Drayton Garden Village Porters Way West Drayton

Details pursuant to condition 58 (contamination) for Phase 5 Block A (82 residential units) of permission ref: 5107/APP/2009/2348, dated 19/11/2009 (redevelopment of former NATS site to mixed development including 773 dwellings)

Decision: 05-02-2015 Approved

Comment on Relevant Planning History

The most relevant planning history for the application site is the Outline Planning Permission (LBH Ref: 5107/APP/2009/2348 granted 1 October 2010), reserving matters of Landscaping and Appearance.

Condition 2 of this consent relate to the approval of Appearance and Landscaping 'Reserved Matters' while Condition 3 requires the approval of the details of the landscaping and appearance of individual phases of the development. Condition 3 also prescribes the extent of the information required.

4. Planning Policies and Standards

UDP / LDF Designation and London Plan

The following UDP Policies are considered relevant to the application:-

Part 1 Policies:

- PT1.BE1 (2012) Built Environment
- PT1.EM1 (2012) Climate Change Adaptation and Mitigation
- PT1.EM6 (2012) Flood Risk Management
- PT1.EM8 (2012) Land, Water, Air and Noise
- PT1.EM11 (2012) Sustainable Waste Management
- PT1.H1 (2012) Housing Growth
- PT1.H2 (2012) Affordable Housing
- PT1.T1 (2012) Accessible Local Destinations

Part 2 Policies:

- AM2 Development proposals - assessment of traffic generation, impact on congestion and public transport availability and capacity
- AM7 Consideration of traffic generated by proposed developments.
- AM8 Priority consideration to pedestrians in the design and implementation of road construction and traffic management schemes
- AM9 Provision of cycle routes, consideration of cyclists' needs in design of highway improvement schemes, provision of cycle parking facilities
- AM13 AM13 Increasing the ease of movement for frail and elderly people and people with disabilities in development schemes through (where appropriate): -
 - (i) Dial-a-ride and mobility bus services
 - (ii) Shopmobility schemes
 - (iii) Convenient parking spaces
 - (iv) Design of road, footway, parking and pedestrian and street furniture schemes
- AM14 New development and car parking standards.
- BE13 New development must harmonise with the existing street scene.

BE20	Daylight and sunlight considerations.
BE21	Siting, bulk and proximity of new buildings/extensions.
BE22	Residential extensions/buildings of two or more storeys.
BE23	Requires the provision of adequate amenity space.
BE24	Requires new development to ensure adequate levels of privacy to neighbours.
BE38	Retention of topographical and landscape features and provision of new planting and landscaping in development proposals.
H4	Mix of housing units
H5	Dwellings suitable for large families
OE1	Protection of the character and amenities of surrounding properties and the local area
OE5	Siting of noise-sensitive developments
OE7	Development in areas likely to flooding - requirement for flood protection measures
OE8	Development likely to result in increased flood risk due to additional surface water run-off - requirement for attenuation measures
OE11	Development involving hazardous substances and contaminated land - requirement for ameliorative measures
LPP 2.6	(2011) Outer London: vision and strategy
LPP 2.8	(2011) Outer London: Transport
LPP 3.1	(2011) Ensuring equal life chances for all
LPP 3.3	(2011) Increasing housing supply
LPP 3.4	(2011) Optimising housing potential
LPP 3.5	(2011) Quality and design of housing developments
LPP 3.6	(2011) Children and young people's play and informal recreation (strategies) facilities
LPP 3.7	(2011) Large residential developments
LPP 3.8	(2011) Housing Choice
LPP 3.9	(2011) Mixed and Balanced Communities
LPP 3.11	(2011) Affordable housing targets
LPP 3.12	(2011) Negotiating affordable housing (in) on individual private residential and mixed-use schemes
LPP 5.1	(2011) Climate Change Mitigation
LPP 5.2	(2011) Minimising Carbon Dioxide Emissions
LPP 5.3	(2011) Sustainable design and construction
LPP 5.7	(2011) Renewable energy
LPP 5.10	(2011) Urban Greening
LPP 5.12	(2011) Flood risk management
LPP 5.13	(2011) Sustainable drainage
LPP 6.1	(2011) Strategic Approach

- LPP 6.7 (2011) Better Streets and Surface Transport
- LPP 6.10 (2011) Walking
- LPP 6.13 (2011) Parking
- LPP 7.1 (2011) Building London's neighbourhoods and communities
- LPP 7.2 (2011) An inclusive environment
- LPP 7.3 (2011) Designing out crime
- LPP 7.4 (2011) Local character
- LPP 7.5 (2011) Public realm
- LPP 7.6 (2011) Architecture
- LPP 7.14 (2011) Improving air quality
- LPP 7.15 (2011) Reducing noise and enhancing soundscapes

5. Advertisement and Site Notice

5.1 Advertisement Expiry Date:- **29th January 2015**

5.2 Site Notice Expiry Date:- Not applicable

6. Consultations

External Consultees

Consultation letters were sent to 250 local owners/occupiers. No responses were received.

Heathrow Aerodrome Safeguarding:

We have now assessed conditions 2 & 3 (Appearance & Landscaping) for phase 5 of Block A against safeguarding criteria and can confirm that we have no safeguarding objections and that they can both be discharged from a Heathrow Airport Ltd point of view.

NATS:

The proposed development has been examined from a technical safeguarding aspect and does not conflict with our safeguarding criteria. Accordingly, NATS (En Route) Public Limited Company ("NERL") has no safeguarding objection to the proposal.

Internal Consultees

Access Officer:

I have considered the details of this planning application and deem the details to be acceptable from an accessibility standpoint.

Environmental Protection Unit:

I have no adverse comments as these matters are not relevant to EPU.

Trees/Landscape Officer:

This application relates to Phase 5, Block A, the final phase of the Drayton Garden Village development. Situated in the south-west corner of West Drayton Garden Village this 1-4 storey development phase fronts on to Porters Way to the south. To the west lies the Parkwest residential scheme, by St George, and the east boundary fronts on to the main spine road into the estate.

The Design & Access Statement re-iterates the guiding vision and design concept for the estate which informed the approved master plan and earlier phases. In addition to the communal areas, soft landscape buffers have been provided at ground level to all boundaries. While linking the building to the landscape, the boundary planting is also designed to create a degree of privacy and security, particularly for those residents in ground floor flats. The landscape principles are described in pages 51-55.

ACD's drawing No. INL19476-11C, Landscape Proposals, details the ground level planting plan around the building edges. This includes 39No. trees, instant hedges, climbing plants, ornamental shrubs, bulbs and grasses. Hard landscape proposals (surfacing, boundaries and street furniture) are specified.

ACD's drawing No. INL19476-12D, Landscape Proposals, specifies the hard and soft landscape details for the communal space on the podium deck. Amendments to the podium deck details take into account some issues discussed at a pre-application meeting.

RECOMMENDATION

No objection subject to the provision of supporting documentation in the form of a Landscape Specification and a Landscape Management/Maintenance Plan.

Officer comments:

A Landscape Specification Rev A and a Landscape Management and Maintenance Plan have been provided. The Council's Trees/Landscape Officer considers these documents to be acceptable.

7. MAIN PLANNING ISSUES

7.01 The principle of the development

The development principle was considered as part of the outline application (LBH Ref. 5107/APP/2009/2348) which the Central and South Planning Committee resolved to grant on 25th May 2010 and the decision was issued on the 1st October 2010 following the Mayoral Stage II approval and completion of the S106 legal agreement.

As stated in the outline application committee report, it is noted in the Council's adopted Porters Way planning brief SPD 2005 that the previous use of the site was predominantly a single employment use. However, redevelopment provides a good opportunity to create a mixed, balanced and sustainable community as embodied in the Sustainable Community Strategy 2008-2011, the Unitary Development Plan and the emerging Local Development Framework, along with regional and national government policies.

Given the range of planning policy considerations in the outline application assessment, including the Hillingdon Employment Land Study (2009), Council's UDP Saved Policies, H8, the Former NATS site, Porters Way, West Drayton SPD (NATS SPD), London Plan Policies as well as the Mayors Stage 1 response, the loss of industrial and business land is considered justified and a mixed use, residential-led redevelopment appropriate and acceptable in accordance with the NATS SPD and UDP Saved Policies LE2 and LE4 which enable consideration of a change of use of industrial/employment uses.

The current reserved matters application, for Landscaping and Appearance as it relates to 82 residential units is sufficiently consistent with the principles and illustrative information of the Outline Planning Permission. Therefore the scheme for Phase 5 (Block A) (82 residential units) is considered acceptable in principle.

7.02 Density of the proposed development

Residential density can be used as an indicator of the overall character and acceptability of a proposal. The density of the whole site was considered as part of the outline application

(LBH Ref. No. 5107/APP/2009/2348) which the Central and South Planning Committee resolved to grant 25th May 2010 and the decision was issued on the 1st October 2010 following the Mayoral Stage II and completion of the s106.

As part of the outline application for the whole site, the provision of 773 residential units (2585 habitable rooms) on the 12.59 ha site equates to a density of 61 units per hectare or 205 habitable rooms per hectare.

For the reasons set out in the 25th May 2010 committee report the density remains acceptable and unchanged, and it is considered to comply with the London Plan.

7.03 Impact on archaeology/CAs/LBs or Areas of Special Character

The site is not located within a Conservation Area, Area of Special Local Character or Archaeological Priority Area. There are no Listed Buildings on the site.

7.04 Airport safeguarding

BAA and NATS have reviewed the application and raise no objection to the Reserved Matters application from an airport safeguarding perspective.

7.05 Impact on the green belt

The site is not located within or near to the Green Belt.

7.07 Impact on the character & appearance of the area

The requirement to consider the proposal's potential future impact on the character and appearance of the surrounding area is contained in London Plan Policies, Policy BE1 of the Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012) and Policies BE13 and BE19 of the Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012). In addition, Policy BE35 of the Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012) requires a high standard of design and providing for an attractive view for sites adjacent to major rail connections into Central London.

In respect of the NATS SPD, the objectives of the site include, among other things, the promotion of a high quality scheme reflective of the area's general character as well as reinforcing local distinctiveness.

Phase 5 (Block A) would provide 82 residential units in the form of flats, with 85 parking spaces located within a rear parking area, part of which would be undercroft. The Layout and Means of Access to and around the dwellings and parking areas were considered acceptable at the outline state. Therefore, consideration is limited to the Appearance and Landscaping.

As part of the outline planning permission the supporting information included a Design and Access Statement, Design Coding and 3D perspective drawings, and an Environmental Statement which included a Townscape and Visual Analysis. These provided an analysis of the scheme from vantage points around and within the development. Illustrative building elevation drawings were provided as a suggestion of the future detailed design of buildings to show how these areas would be framed by buildings in the future. The Council's Principle Urban Design Officer and the GLA were supportive of the scheme at Outline stage in respect of its appearance and character and no impacts were identified to neighbours or the area in general in this regard. Along with details of buildings, comprehensive information pertaining to hard and soft landscaping for the entire site was considered at the outline stage by a range of consultees including the Council's Trees/Landscape Officer.

The proposed materials for the residential blocks would be as follows:

- i) Facing Brickwork - Hammersmith London Stock (Yellow)
- ii) Render - White and Grey
- iii) Roof - Flat roof System
- iv) Rainwater goods - UPVC
- v) Windows & French Doors - white colour UPVC
- vii) Galvanised steel balconies - black
- viii) Galvanised metal railings - black

These materials, along with the proposed hard surfaces for the parking area, access roads and footpaths are considered to be acceptable.

Overall, the scheme is considered to result in an acceptable character and appearance to this part of West Drayton, thereby complying with Policy BE1 of the Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012) and Policies BE13 and BE19 of the Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012).

This application provides supporting information pertaining to 'Appearance' and 'Landscaping', and in particular, building elevations and sections, as well as plans of the hard and soft landscaping arrangements.

7.08 Impact on neighbours

It should be noted that the consideration of potential impacts upon neighbours formed part of the assessment of the outline application (LBH Ref: 5107/APP/2009/2348) which the Central and South Planning Committee resolved to grant 25th May 2010 and was subsequently approved on 1st October 2010 following the Mayoral Stage II approval and completion of the S106. Matters considered include the following: construction impacts; traffic and car parking; noise and general disturbance; overlooking, outlook and overshadowing.

The reserved matters are consistent with the details and principles considered at the outline stage, and as such, it is not considered that there is a significant change in impacts on neighbours as to warrant refusal of this reserved matters application.

7.09 Living conditions for future occupiers

The outline application considered living conditions for future occupiers. The London Plan requires all new developments to provide adequate levels of internal floor areas. It is considered that the proposed scheme provides adequate floor space for the units.

Policy BE23 of the Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012) requires residential developments to provide or maintain sufficient external amenity space to protect the amenity of residents. The Council's guidelines state that studios and one-bed flats should be provided with 20sq.m, 25sq.m for two-bed flats and 30sq.m for three-bed flats. As such, the proposal would need to provide 1970sq.m of external amenity space.

396sq.m of communal amenity space would be provided on a podium on the first floor, whilst each individual flat would be provided with private amenity space in the form of terraces or balconies. Additional communal amenity space would be provided at the rear of the car parking area and alongside Porters Way. The external amenity space provided would exceed the required amount and is therefore considered to be acceptable.

New residential developments are required to provide adequate levels of light and privacy, and should not result in overdominance through their siting and bulk. The scale, location and height of the proposed dwellings are similar to the scale, location and height approved at the outline stage. The dwellings have been carefully laid out to ensure that privacy is

maintained, in compliance with the 21m overlooking distance, and adequate levels of light are provided to habitable rooms and kitchens.

7.10 Traffic impact, Car/cycle parking, pedestrian safety

The Council's Highways Engineer and TFL considered traffic and parking impacts along with means of access for the entire site as part of the original application (ref: 5107/APP/2009/2348). In addition to a transport assessment and travel plan, the outline application provided drawings detailing access, turning (refuse vehicle swept paths) and parking (cars, bicycles, car club, motorcycle allowance, 10% accessible parking provision allowance). Transport was also considered as part of the Environmental Impact Assessment, particularly in chapters 5, Construction and Phasing, (which contains the framework Construction Management Plan) and 9, Transport Accessibility and Movement, of the Environmental Statement. The outline consent imposed appropriately worded conditions of approval in respect of traffic management, parking numbers and allocation.

Matters relating to access and layout were approved as part of the outline consent. The car parking locations, road layout and widths as well as access are consistent with the outline proposal. The proposal would provide 85 parking spaces in total, ten of which would be disabled parking spaces. It is considered that the amount of car parking spaces provided would be acceptable for the proposed development.

7.11 Urban design, access and security

Issues of access and security were considered as part of the outline planning application. Secure by Design principles, such as defensible space around the dwellings, visual surveillance and secure parking areas, were a key component of the original masterplan.

It should be noted that appropriately worded conditions relating to CCTV and security measures were imposed on the outline permission and are being dealt with through a concurrent details application.

7.12 Disabled access

The proposed dwellings would comply with the Lifetime Homes standards and ten wheelchair accessible units would be provided, along with ten disabled parking spaces. The Council's Access Officer considers the proposal to be acceptable in terms of accessibility.

7.13 Provision of affordable & special needs housing

The proposal seeks permission for reserved matters 'Landscaping' and 'Appearance'. Accordingly considerations relating to affordable or special needs housing are not relevant to the application. Planning obligations are secured by legal agreement.

7.14 Trees, landscaping and Ecology

This application seeks approval of the landscaping which was part of the reserved matters under condition 2 of the original planning permission. Approval of the details of the landscaping and appearance of individual phases of the development were required as part of condition 3, which also prescribed the extent of the information required.

The proposed landscaping proposals are considered to fully comply with the concepts and design objective indicated at outline stage. Additional tree planting and soft landscaping is shown in appropriate locations around the site, to benefit the public realm.

Communal amenity space would be provided on a podium located on the first floor of the apartment block. The landscaping on the podium would comprise of hard and soft landscaping, including grass, trees planted within wooden planters, hedges, shrub planting and 1.25m high metal railings around the podium edges.

Overall, the development would provide for an appropriate landscape setting. The Council's Trees/Landscape Officer considers the proposed landscaping to be acceptable.

7.15 Sustainable waste management

The sustainable waste features of the proposed development were considered as part of the outline application (LBH Ref: 5107/APP/2009/2348). In the course of considering the outline scheme, the Highways and Waste teams confirmed that waste arrangements could be suitably accommodated on the site.

Phase 5 (Block A) would provide 82 residential units. Waste would be stored in 20 Eurobins located within two bin stores on the ground floor of the apartment block. The number of Eurobins proposed for Block A would exceed the minimum requirement and is considered to be acceptable.

7.16 Renewable energy / Sustainability

The renewable/sustainable features of the development were considered as part of the outline application (LBH Ref. No. 5107/APP/2009/2348) which the Central and South Planning Committee resolved to grant 25th May 2010 and the decision finally issued on 1st October 2010 following the Mayoral Stage II approval and completion of the s106 legal agreement.

The renewable and sustainable measures imposed were considered acceptable by both the Council and the GLA and appropriately worded conditions and s106 undertaking agreed accordingly. 116 PV panels would be provided on the roofs of Block A.

The subject application for reserved matters of Phase 5 is consistent with the principles established at the outline stage, and the permanent energy centre is now approved and is nearing completion on a separate part of the site.

7.17 Flooding or Drainage Issues

Whilst the application site is in an area of low risk (Flood Zone 1), a Flood Risk Assessment (FRA) was submitted as part of the Environmental Statement and flooding issues were considered as part of the outline application.

The assessment demonstrated that the proposal would not have an adverse impact to neighbouring properties as it will not increase flood risk through increased surface runoff.

The Environment Agency reviewed the application and raised no objection subject to a condition to ensure the development be carried out in accordance with the FRA.

Consideration of Flood Risk was satisfactorily addressed as part of the outline permission and is not relevant to this landscape and appearance Reserved Matters application.

7.18 Noise or Air Quality Issues

Phase 5 (Block A) is located in the south-west corner of West Drayton Garden Village and fronts onto Porters Way to the south. As such, at outline stage, consent was granted subject to conditions to ensure the development was not adversely impacted upon by road traffic noise or by air quality issues.

It is considered that these detailed design aspects would be controlled through the discharge of conditions and, as such, there are no issues to consider in the subject application for reserved matters.

7.19 Comments on Public Consultations

No responses were received during the public consultation.

7.20 Planning obligations

Not applicable to this reserved matters application.

7.21 Expediency of enforcement action

Not applicable to this reserved matters application.

7.22 Other Issues

None

8. Observations of the Borough Solicitor

General

Members must determine planning applications having due regard to the provisions of the development plan so far as material to the application, any local finance considerations so far as material to the application, and to any other material considerations (including regional and national policy and guidance). Members must also determine applications in accordance with all relevant primary and secondary legislation.

Material considerations are those which are relevant to regulating the development and use of land in the public interest. The considerations must fairly and reasonably relate to the application concerned.

Members should also ensure that their involvement in the determination of planning applications adheres to the Members Code of Conduct as adopted by Full Council and also the guidance contained in Probity in Planning, 2009.

Planning Conditions

Members may decide to grant planning consent subject to conditions. Planning consent should not be refused where planning conditions can overcome a reason for refusal. Planning conditions should only be imposed where Members are satisfied that imposing the conditions are necessary, relevant to planning, relevant to the development to be permitted, enforceable, precise and reasonable in all other respects. Where conditions are imposed, the Council is required to provide full reasons for imposing those conditions.

Planning Obligations

Members must be satisfied that any planning obligations to be secured by way of an agreement or undertaking pursuant to Section 106 of the Town and Country Planning Act 1990 are necessary to make the development acceptable in planning terms. The obligations must be directly related to the development and fairly and reasonably related to the scale and kind to the development (Regulation 122 of Community Infrastructure Levy 2010).

Equalities and Human Rights

Section 149 of the Equalities Act 2010, requires the Council, in considering planning applications to have due regard to the need to eliminate discrimination, advance equality of opportunities and foster good relations between people who have different protected characteristics. The protected characteristics are age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

The requirement to have due regard to the above goals means that members should consider whether persons with particular protected characteristics would be affected by a proposal when compared to persons who do not share that protected characteristic. Where equalities issues arise, members should weigh up the equalities impact of the

proposals against the other material considerations relating to the planning application. Equalities impacts are not necessarily decisive, but the objective of advancing equalities must be taken into account in weighing up the merits of an application. The weight to be given to any equalities issues is a matter for the decision maker to determine in all of the circumstances.

Members should also consider whether a planning decision would affect human rights, in particular the right to a fair hearing, the right to respect for private and family life, the protection of property and the prohibition of discrimination. Any decision must be proportionate and achieve a fair balance between private interests and the public interest.

9. Observations of the Director of Finance

Not applicable to this reserved matters application.

10. CONCLUSION

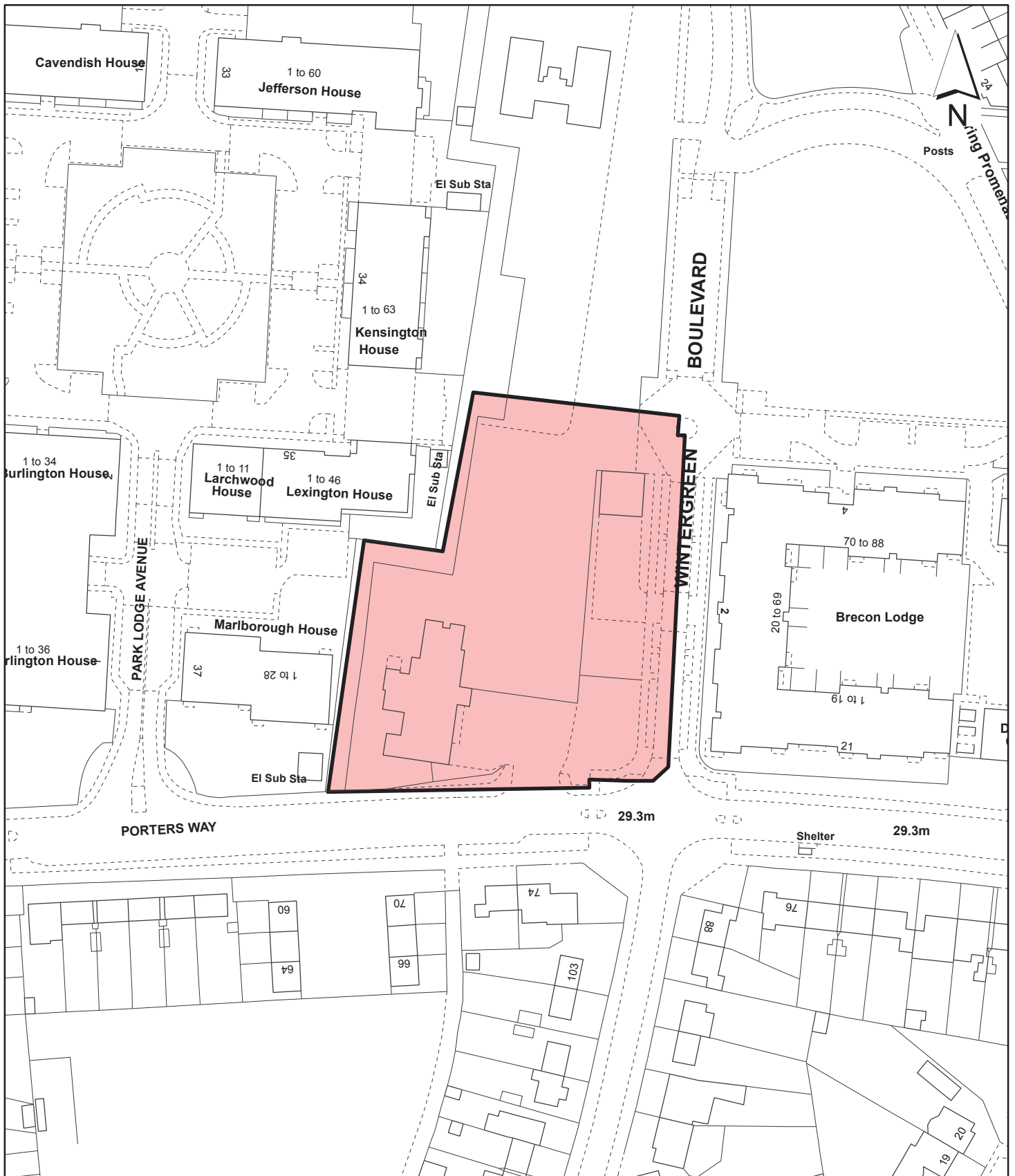
Conditions 2 and 3 of the outline planning consent (ref: 5107/APP/2009/2348) required the provision of details of the landscaping and appearance of individual phases of the development. This Reserved Matters application for Phase 5 (Block A) considers the Appearance and Landscaping to be acceptable, and in compliance with the relevant policies and planning guidance for the site. Therefore, the application is recommended for approval.

11. Reference Documents

Hillingdon Local Plan: Part 1 - Strategic Policies (November 2012)
Hillingdon Local Plan: Part 2 - Saved UDP Policies (November 2012)
The London Plan (July 2011)
National Planning Policy Framework 2012

Contact Officer: Katherine Mills

Telephone No: 01895 250230



Notes

 Site boundary

For identification purposes only.
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Site Address

**West Drayton Garden Village
 Porters Way
 West Drayton**

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111

Planning Application Ref:

5107/APP/2014/4304

Scale

1:1,250

Planning Committee

Major Page 166

Date

March 2015



HILLINGDON
 LONDON

Plans for Major Applications Planning Committee

5th March 2015



HILLINGDON
LONDON



INVESTOR IN PEOPLE

www.hillingdon.gov.uk

Page 167

Report of the Head of Planning, Sport and Green Spaces

Address 88-94 LONG LANE ICKENHAM

Development: Demolition of 5 existing dwellinghouses and redevelopment of the site for a 85 unit Class C2 care home for the elderly of 1.5 to 2.5 storeys in height with associated landscaping and car parking (40 spaces in total), stopping up of existing vehicular accesses on Long Lane and construction of new vehicular access onto Long Lane.

LBH Ref Nos: 52129/APP/2014/2996

Date Plans Received: 20/08/2014

Date(s) of Amendment(s): 20/01/2015

Date Application Valid: 15/09/2014

20/08/2014

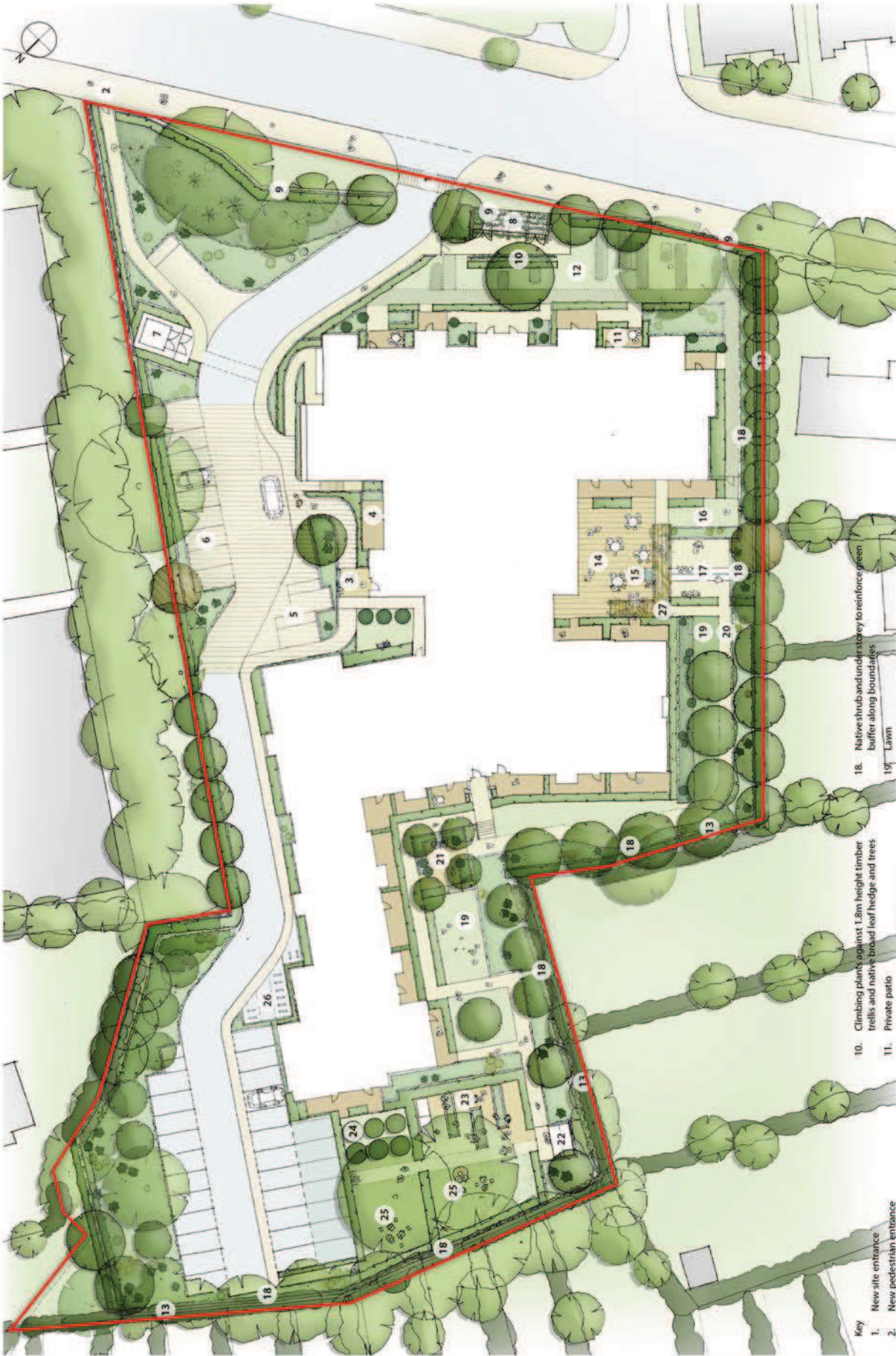
21/10/2014



date	rev	revision/author/checker
	drawn	Author
	checked	Checker
	scale	1 : 1250
	date	May 2014

title	Site Plan Existing
drawing no	AA4378/2001



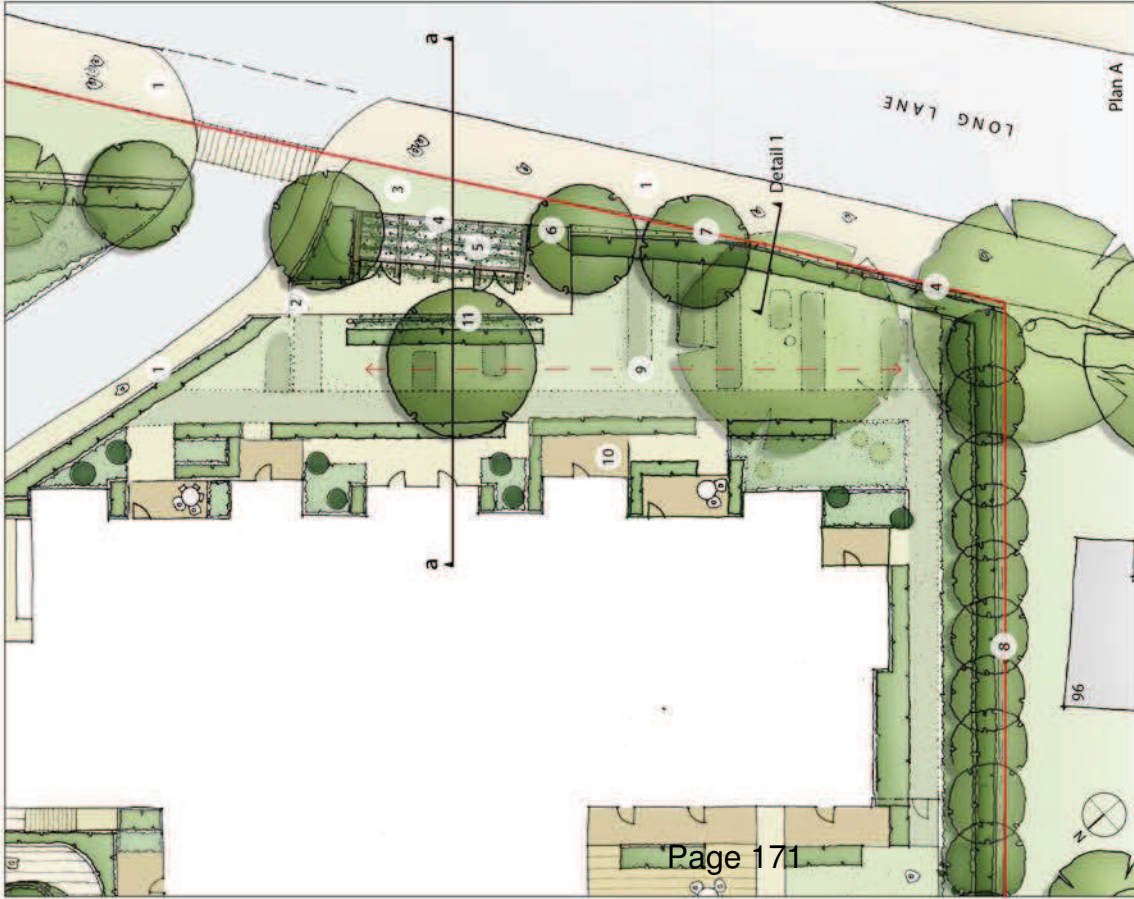


- Key**
1. New site entrance
 2. New pedestrian entrance
 3. Main entrance/lobby
 4. Outdoor cafe area
 5. Disabled parking
 6. Parking area
 7. Substation enclosure
 8. Bin store with pergola top/climber
 9. 1200 railing over brick wall with clipped native hedge

10. Climbing plants against 1.8m height timber trellis and native broad leaf hedge and trees
11. Private patio
12. Front garden: Ornamental grass and lavender parterre
13. Existing timber close board fencing and native evergreen hedge and trees
14. Dining terrace
15. Water feature: mirror pool
16. Courtyard garden
17. Water feature/Bench

18. Native shrub and understorey to reinforce green buffer along boundaries
19. Lawn
20. Resin bound gravel path
21. Sensory garden
22. Green house/Storage
23. Activity area/Kitchen vegetable garden with compost
24. Orchard
25. Seating/Picnic area
26. Cycle parking
27. Pergola with climbing plants



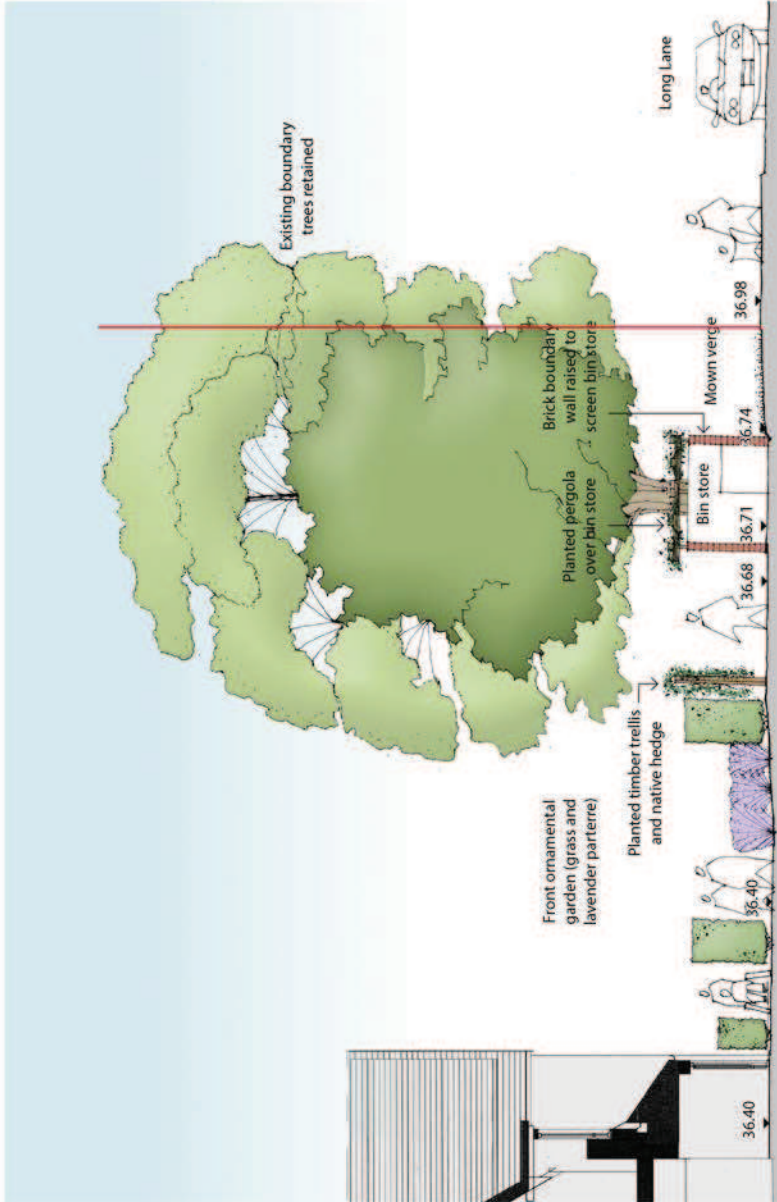


- Key**
- 1. Footpath
 - 2. New gate/maintenance access
 - 3. Mown verge
 - 4. Brick boundary wall
 - 5. Bin store
 - 6. New gate
 - 7. 1.2m height railing over wall and native broad leaf hedge clipped to top of railings
 - 8. Existing timber close board fencing and native evergreen hedge and trees
 - 9. Ornamental grass and lavender parterre
 - 10. Private patio
 - 11. Climbing plants against 1.8m height timber trellis and native broad leaf hedge and trees
- Site boundary

- 1. Footpath
- 2. New gate/maintenance access
- 3. Mown verge
- 4. Brick boundary wall
- 5. Bin store
- 6. New gate
- 7. 1.2m height railing over wall and native broad leaf hedge clipped to top of railings
- 8. Existing timber close board fencing and native evergreen hedge and trees
- 9. Ornamental grass and lavender parterre
- 10. Private patio
- 11. Climbing plants against 1.8m height timber trellis and native broad leaf hedge and trees



Key Plan



Section aa

(for illustrative purposes only)



Detail 1: Typical wall section (for illustrative purposes only)





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drawn:	author:	checked:	checker:	scale:	1:500
date:	06/27/14	rev:			

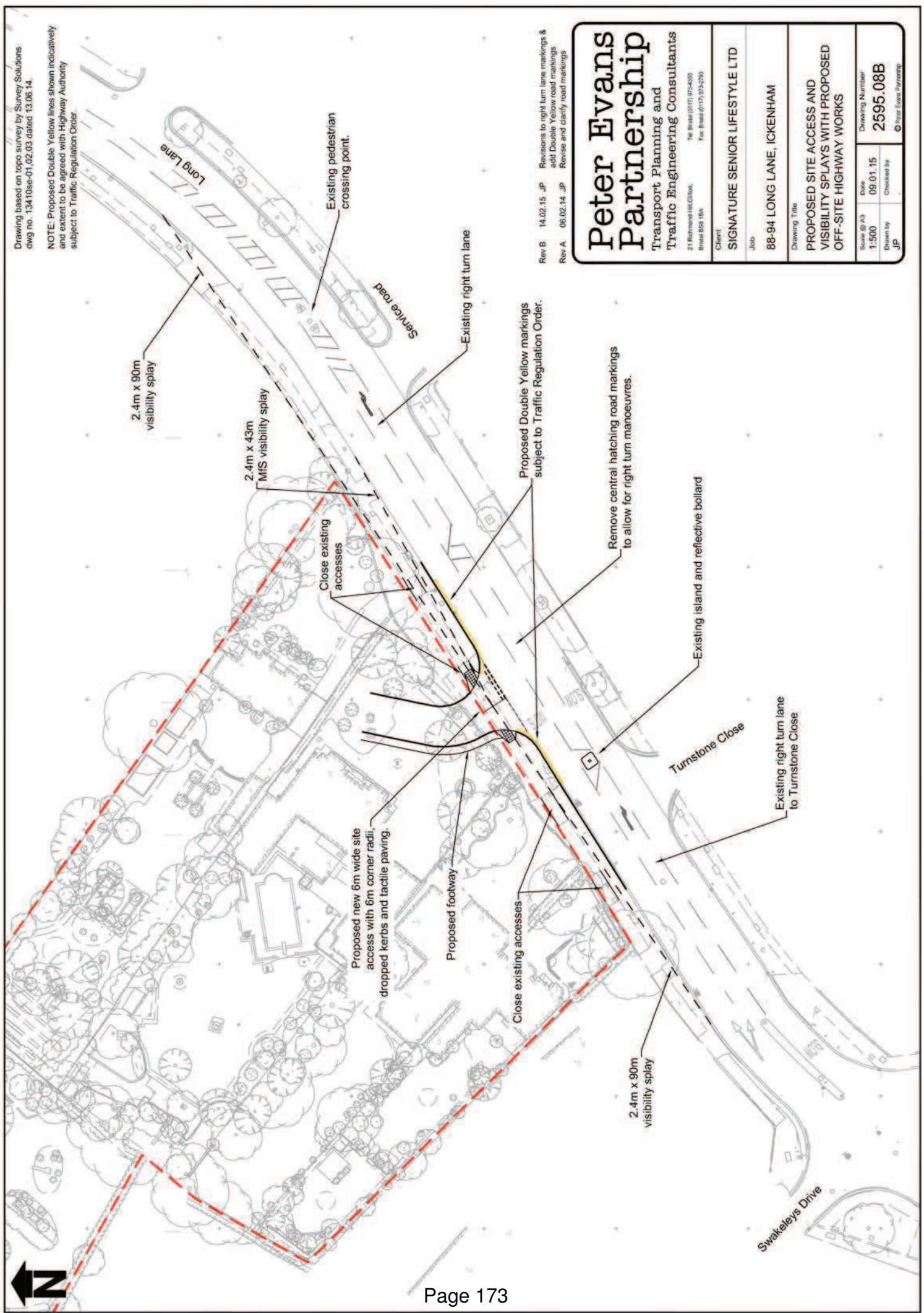
Project: Ickenham
 Drawing no: A44276/1005
 rev: rev

Site Plan Proposed 1-500
 1:500

Ickenham



Drawing based on topo survey by Survey Solutions
 dwg no. 13410sc-01,02,03 dated 13.06.14.
 NOTE: Proposed Double Yellow lines shown indicatively
 and extent to be agreed with Highway Authority
 subject to Traffic Regulation Order.



Rev B 14.02.15 JP Revisions to right turn lane markings & add Double Yellow road markings
 Rev A 06.02.14 JP Revise and clarify road markings

Peter Evans Partnership Transport Planning and Traffic Engineering Consultants 21 Richmond Hill Clon, Tel: 0117 973-4888 Bristol BS8 1BA Fax: 0117 973-2799	
Client	SIGNATURE SENIOR LIFESTYLE LTD
Job	88-94 LONG LANE, ICKENHAM
Drawing Title	PROPOSED SITE ACCESS AND VISIBILITY SPLAYS WITH PROPOSED OFF-SITE HIGHWAY WORKS
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Date	09.01.15
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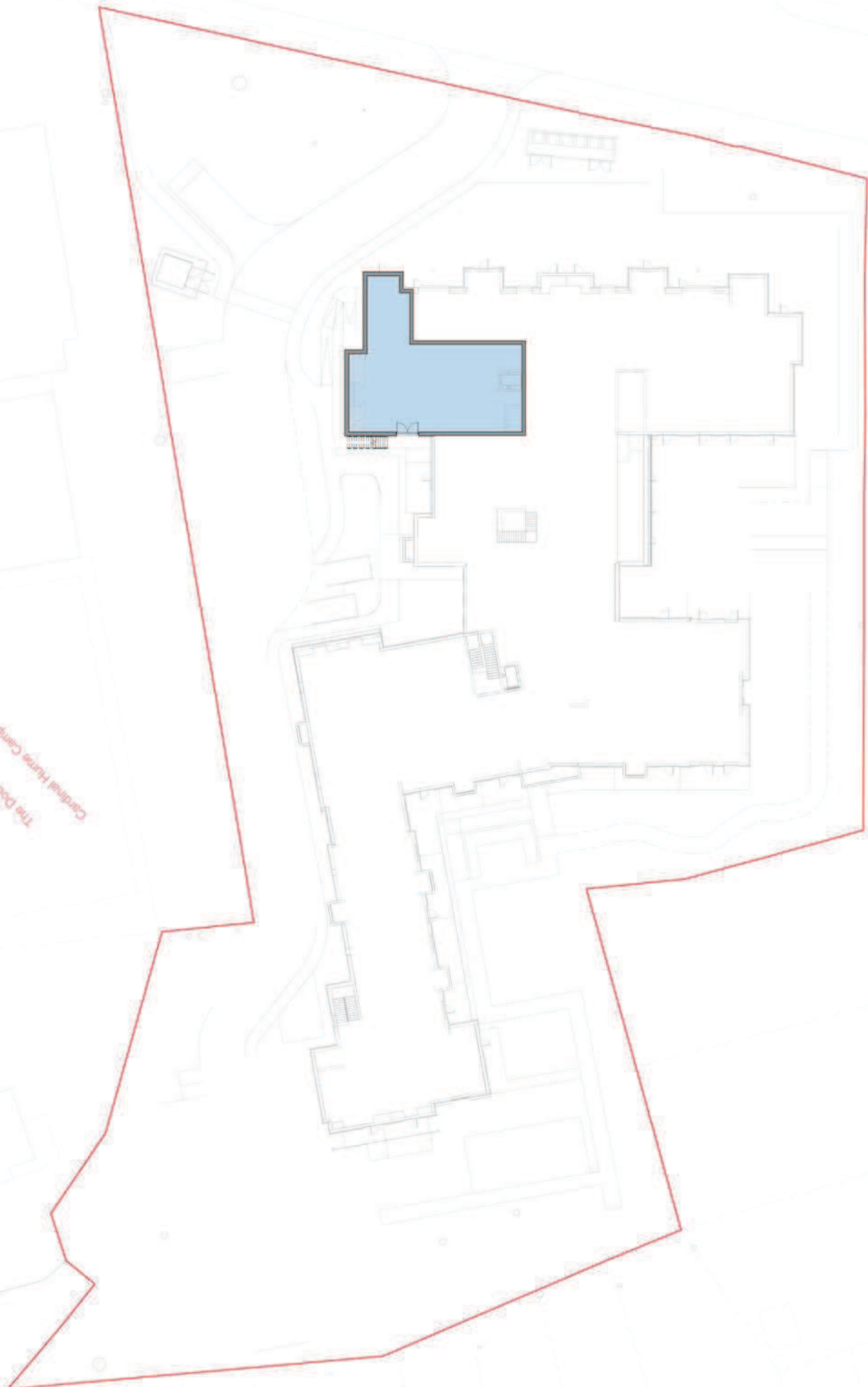


LONG LANE

121

The Power House
Cardinal Hume Campus

70



1500414 - A - General Update - CJP - IP
DATE: 05/05/2004 BY: IP/050504/05/05/04/05/05/04

NO	DESCRIPTION	DATE	BY	CHKD	APPD
1	Basement	05/05/04	IP	IP	

drawing no: AA4376/2004 rev: A date: 05/05/13

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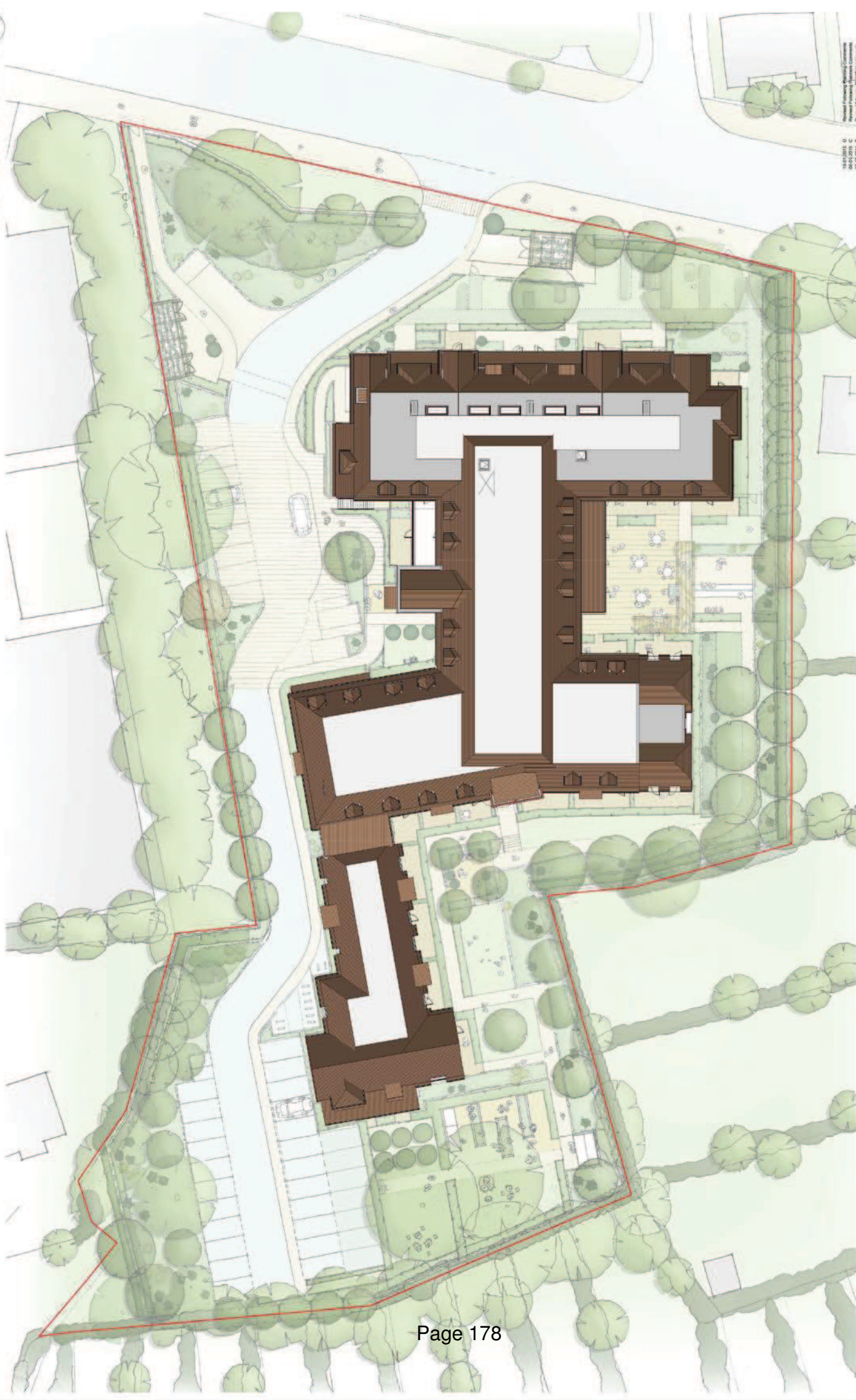
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Basement
1:200

Signature
The Power House at Home

Ickenham



18.01.2015 G
 08.01.2015 C
 10.01.2015 R
 15.02.2014 B
 General Layout
 Free Issue



DATE: 15/02/2014
 DRAWN: JUP
 CHECKED: JP
 SCALE: 1:200
 DATE: 06/08/13

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 REV: 90



KEY

- 60° DEGREE ROOF
- 52° DEGREE ROOF
- 47° DEGREE ROOF

PLAN DIAGRAM OF ROOF PITCHES

KEY:



Elevation 1
Dorry House

Elevation 2

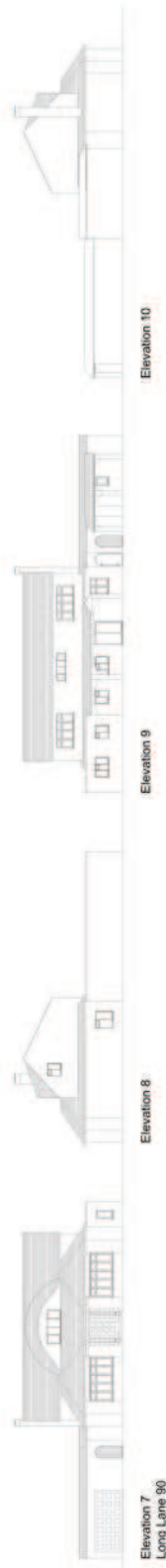
Elevation 3

Elevation 4



Elevation 5
Long Lane 94 & 92

Elevation 6



Elevation 7
Long Lane 90

Elevation 8

Elevation 9

Elevation 10



Elevation 12
Long Lane 88

Elevation 13

Elevation 14

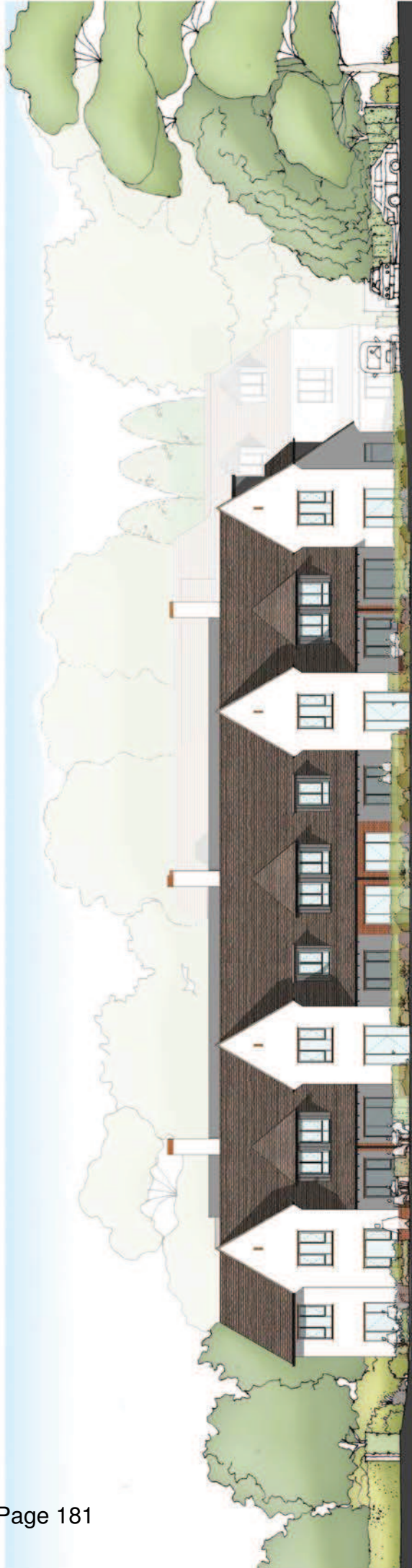
Elevation 15

Garage

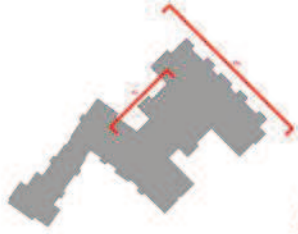




Elevation 1 - East Entrance Courtyard



Elevation 2 - South Elevation Long Lane



Key Plan
1:100

KEY

ROOF

Dark brown clay tile roof. Universally roof system with brown aluminium cladding. All roof elements to be finished with a dark brown paint. All roof elements to be finished with a dark brown paint. All roof elements to be finished with a dark brown paint.

DOORS

Dark brown wood finish. All doors to be finished with a dark brown paint. All doors to be finished with a dark brown paint. All doors to be finished with a dark brown paint.

WALLS

White painted masonry walls with smaller elements of red facing treatment at ground level. All walls to be finished with a dark brown paint. All walls to be finished with a dark brown paint. All walls to be finished with a dark brown paint.

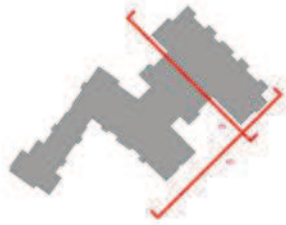
WINDOWS

Dark brown wood finish. All windows to be finished with a dark brown paint. All windows to be finished with a dark brown paint. All windows to be finished with a dark brown paint.

RAILINGS & BALUSTRADES

Black to be finished in the building with other walls and doors. In each instance, balustrades to be composed of powder coated aluminium sections and a glass infill section to 1000mm height.





Key Plan
1:1000

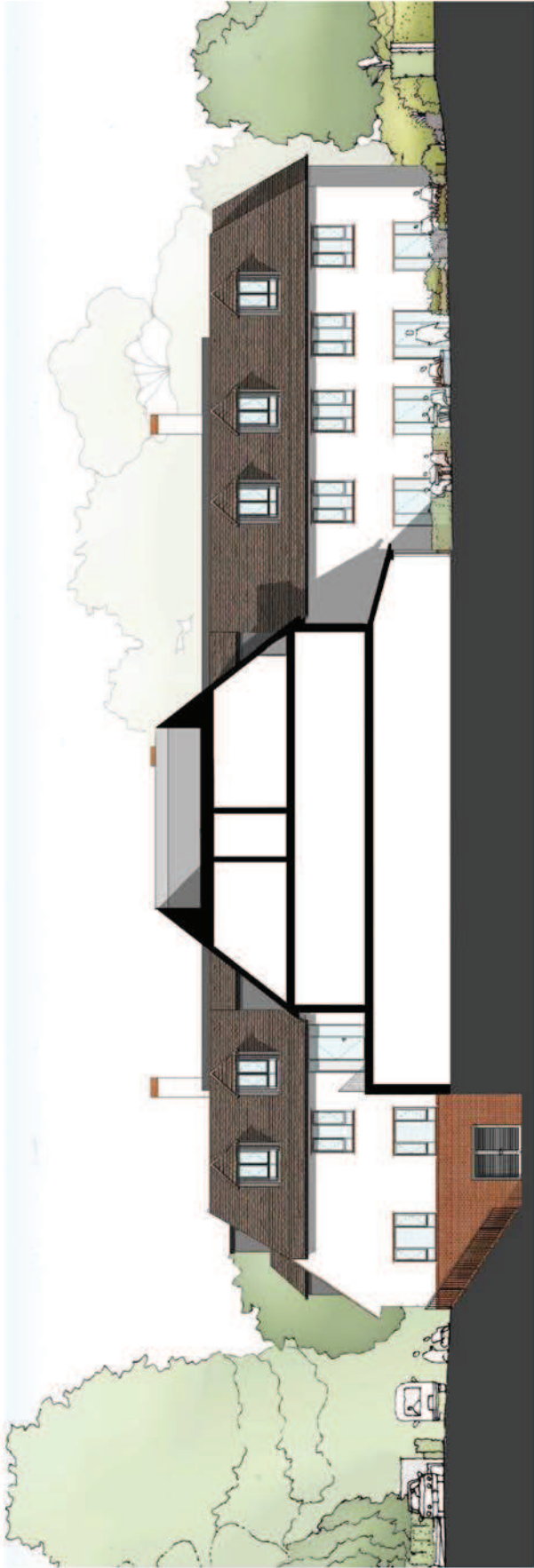
KEY

ROOF
 This section shows the roof structure and supports with the internal structure. The roof structure is shown in grey and the supports in black. The roof structure will be shown in the working plans. The roof structure is shown in the working plans. There will be a separate section for the roof structure. There will be a separate section for the roof structure.

WALLS
 This section shows the walls with the internal structure. The walls are shown in grey and the internal structure in black. The walls are shown in the working plans. The walls are shown in the working plans.

WINDOWS
 This section shows the windows and glazing with the internal structure. The windows are shown in black and the glazing in white. The windows are shown in the working plans. The windows are shown in the working plans.

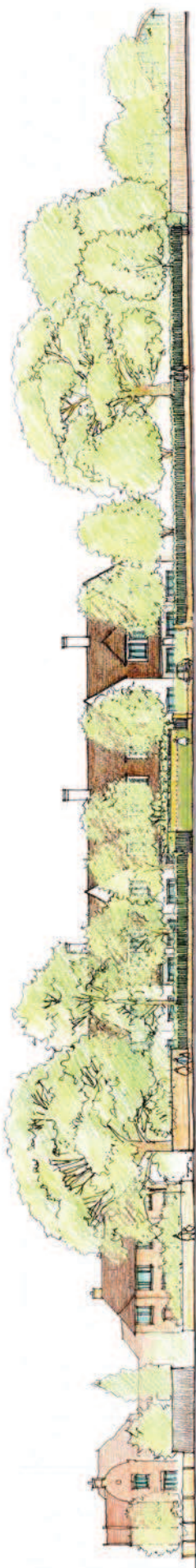
DOORS & GLAZING
 This section shows the doors and glazing with the internal structure. The doors are shown in black and the glazing in white. The doors are shown in the working plans. The doors are shown in the working plans.



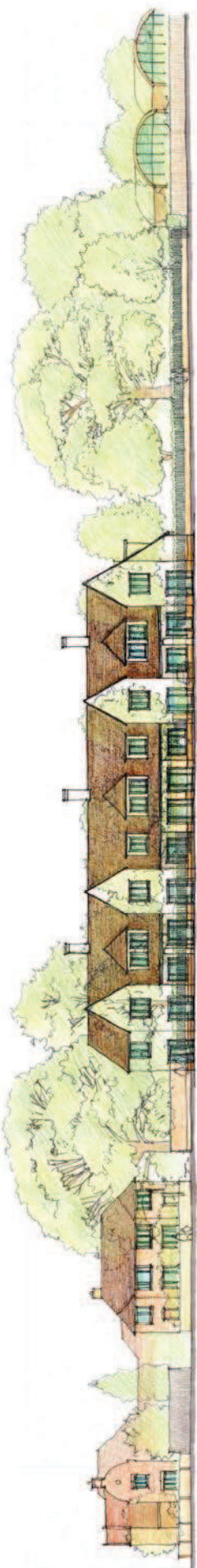
Elevation 3 - North Courtyard



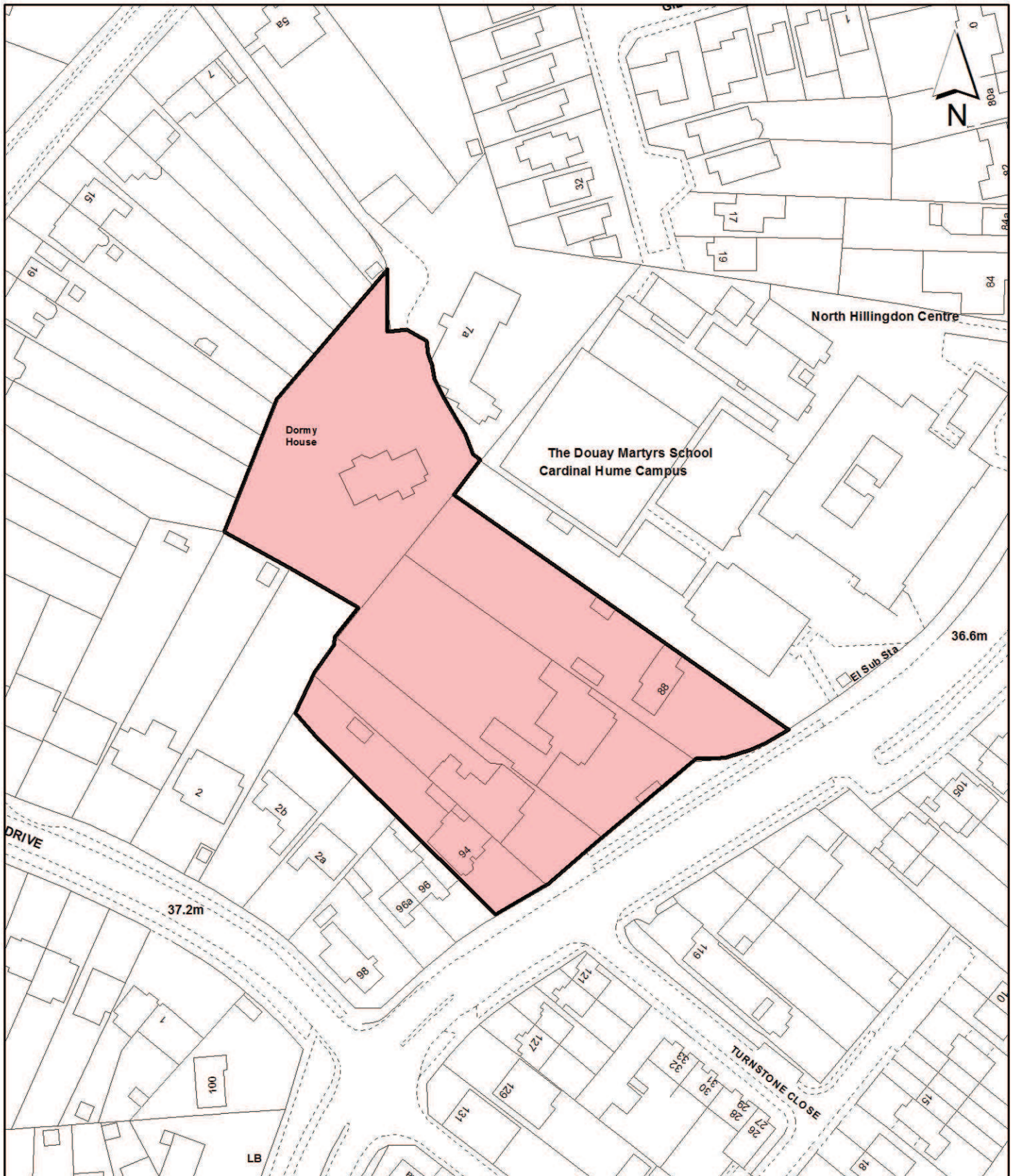
Elevation 5 - West Elevation Garden Courtyard



Long Lane Street Elevation - With trees as proposed



Long Lane Street Elevation - Without trees to reveal elevation



Notes

 Site boundary

For identification purposes only.

This copy has been made by or with the authority of the Head of Committee Services pursuant to section 47 of the Copyright, Designs and Patents Act 1988 (the Act).

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Site Address

**88-94 Long Lane
Ickenham**

Planning Application Ref:

52129/APP/2014/2996

Planning Committee

Major Page 187

Scale

1:1,250

Date

March 2015

**LONDON BOROUGH
OF HILLINGDON**

**Residents Services
Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 250111



HILLINGDON
LONDON

Report of the Head of Planning, Sport and Green Spaces

Address HAYES AND HARLINGTON STATION STATION ROAD HAYES

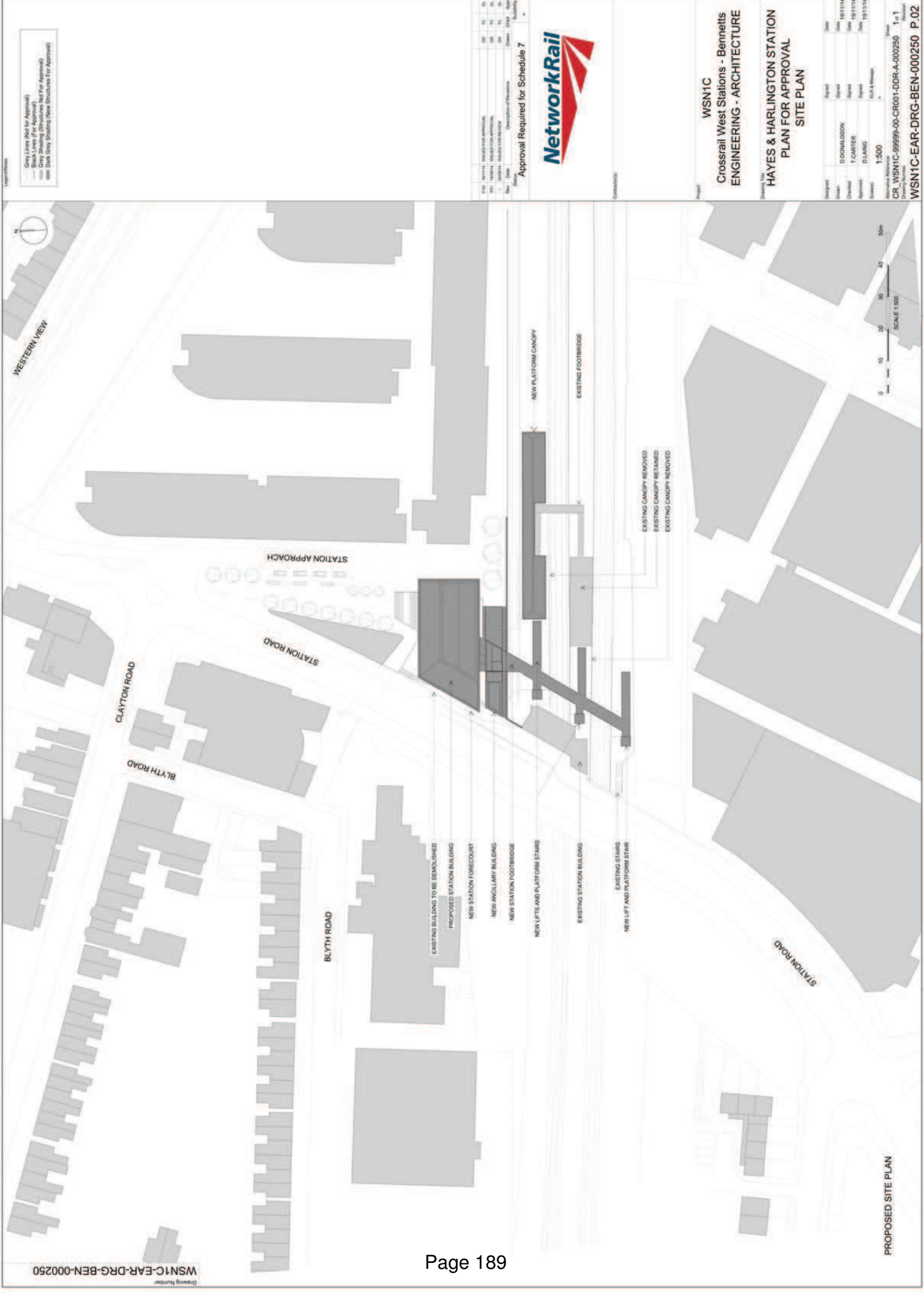
Development: Application under Schedule 7 of the Crossrail Act 2008 for the approval of Plans and Specifications associated with the erection of a new station building external lift shaft, footbridge with lift shafts, extension of platforms 2/3 and 4/5 with canopy modifications, new waiting room to Platform 4/5 and associated lighting.

LBH Ref Nos: 10057/APP/2014/4338

Date Plans Received: 09/12/2014

Date(s) of Amendment(s):

Date Application Valid: 09/12/2014



Legend

Approval Required for Schedule 7

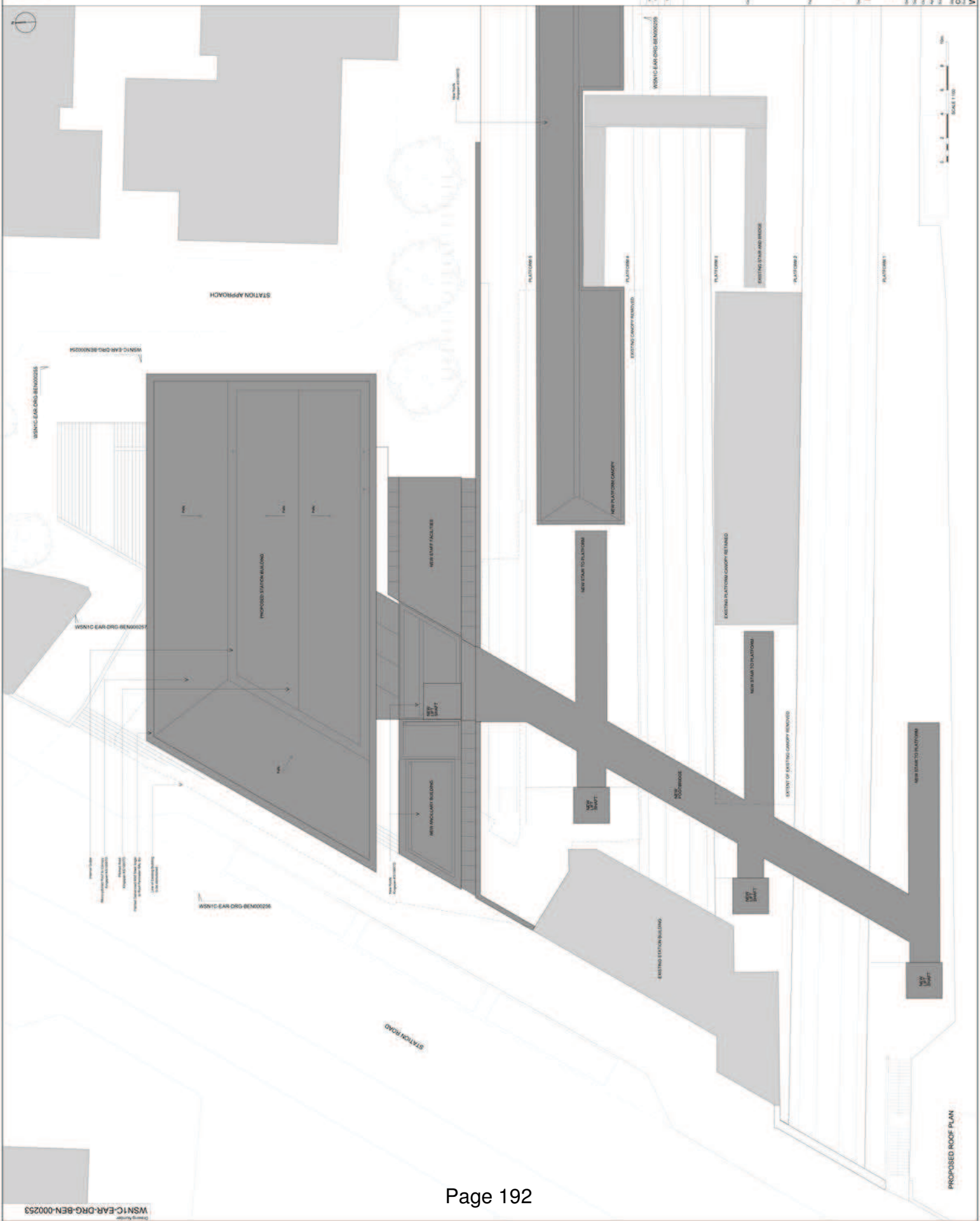


Project: WSN1C
Crossrail West Stations - Bennetts
ENGINEERING - ARCHITECTURE

Drawing Title: HAYES & HARLINGTON STATION
PLAN FOR APPROVAL
SITE PLAN

Designated	Checked	Checked	Checked	Checked	Date	Date	Date	Date	
Drawn	D DONALDSON	Checked	T CARTER	Approved	D LIANG	Scale	1:500	Altitude Reference	100m
Drawn		Checked		Approved		Scale		Altitude Reference	
Drawn		Checked		Approved		Scale		Altitude Reference	
Drawn		Checked		Approved		Scale		Altitude Reference	

Sheet: 1 of 1
CR_WSN1C-98699-00-CR001-DDR-A-000250
Drawing Number: WSN1C-EAR-DRG-BEN-000250 P.02



DATE: 11/15/2011
 DRAWN BY: J. B. BENTLEY
 CHECKED BY: J. B. BENTLEY
 APPROVED BY: J. B. BENTLEY

NO.	DATE	DESCRIPTION	BY	CHK
1	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
2	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
3	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
4	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
5	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
6	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
7	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
8	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
9	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
10	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY

Approval Required for Schedule 7



WSN1C
 Crossrail West Stations - Bennetts
 ENGINEERING - ARCHITECTURE

HAYES AND HARLINGTON STATION
 PLAN FOR APPROVAL
 PROPOSED BUILDING ROOF PLAN

NO.	DATE	DESCRIPTION	BY	CHK
1	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
2	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
3	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
4	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
5	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
6	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
7	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
8	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
9	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY
10	11/15/2011	ISSUED FOR PERMIT	J. B. BENTLEY	J. B. BENTLEY

Scale: 1:100

WSN1C-EAR-DRG-BEN-000253 P.02

11/15/2011

11/15/2011

11/15/2011

11/15/2011

11/15/2011

11/15/2011

11/15/2011

11/15/2011

1. Information shown in this Drawing has been compiled from the information provided to the Designer and is intended to be used for the purposes of the project only. It is not to be used for any other purpose without the written consent of the Designer.

2. Contractors to the project are to refer to the relevant specifications and standards for the materials and workmanship to be used in the construction of the project.

3. All dimensions are to be given unless otherwise specified.

SAFETY, Health and Environmental Information
 Notes below are additional to hazards/risks normally associated with this type of work.
 All references below are to Hazard Log WSN1B-MPM-AEA-000271

Construction:
 C. H&M/020: Passenger circulation conflicts with later construction
 C1. H&M/13c: Temporary instability of proposed bridge

Operations:
 O1. H&M/10: Signal visibility obscured by construction works

Maintenance:
 M. Dismantling/DEMOLITION (F.4/01)

These notes are based on the use of experienced staff and are not intended to be used as an approved safe method of working.



Rev	Date	Description of Revision	Drawn	Checked	Job No	Scale
1		ISSUED FOR REVIEW				
2		ISSUED FOR REVIEW				



FOR FORM A REVIEW

Project: **WSN1B WEST STATIONS - OUTER/INNER ESTATES (LAND & PROPERTY)**

Drawing Title: **HAYES & HARLINGTON STATION PLAN FOR APPROVAL PROPOSED PLATFORM 2 / 3 EXTENSION**

Author: J. RUGENT
 Date: 17/09/12

Checked: J. STRATTON
 Date: 17/09/12

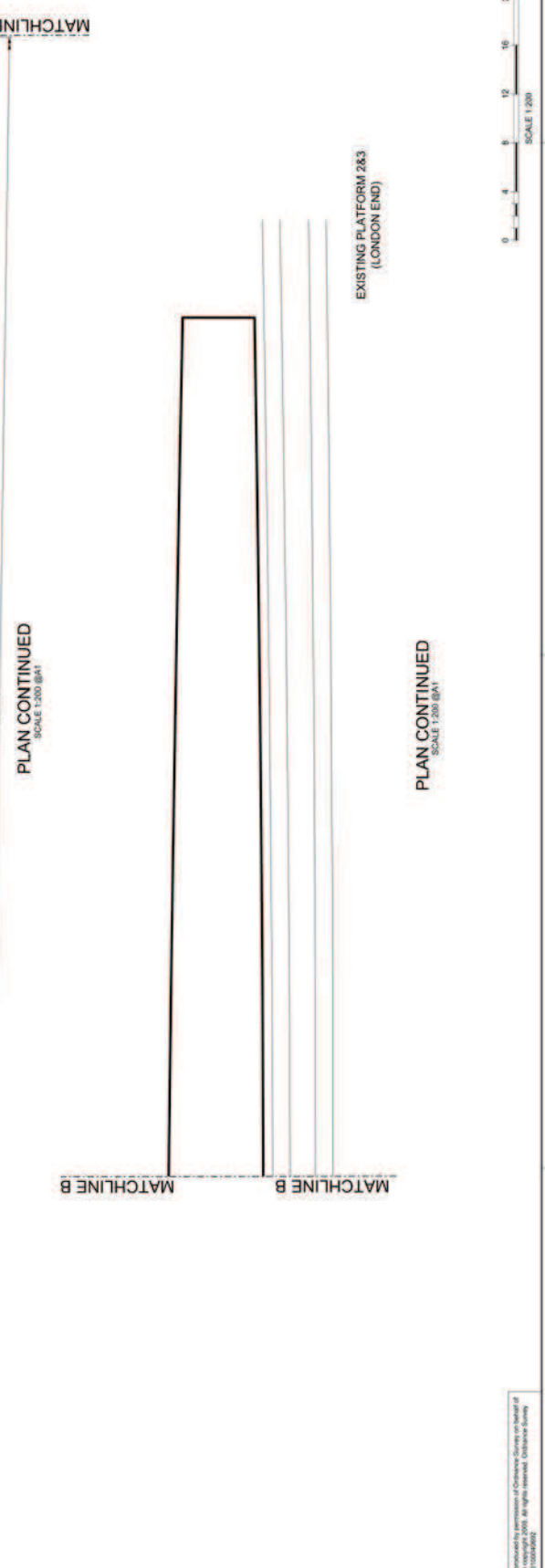
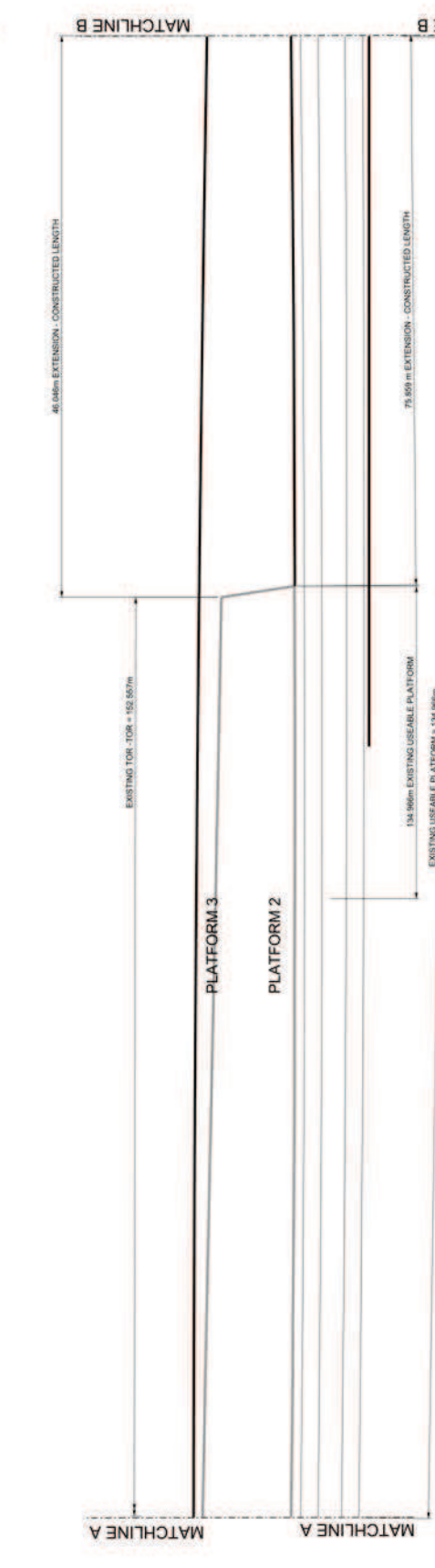
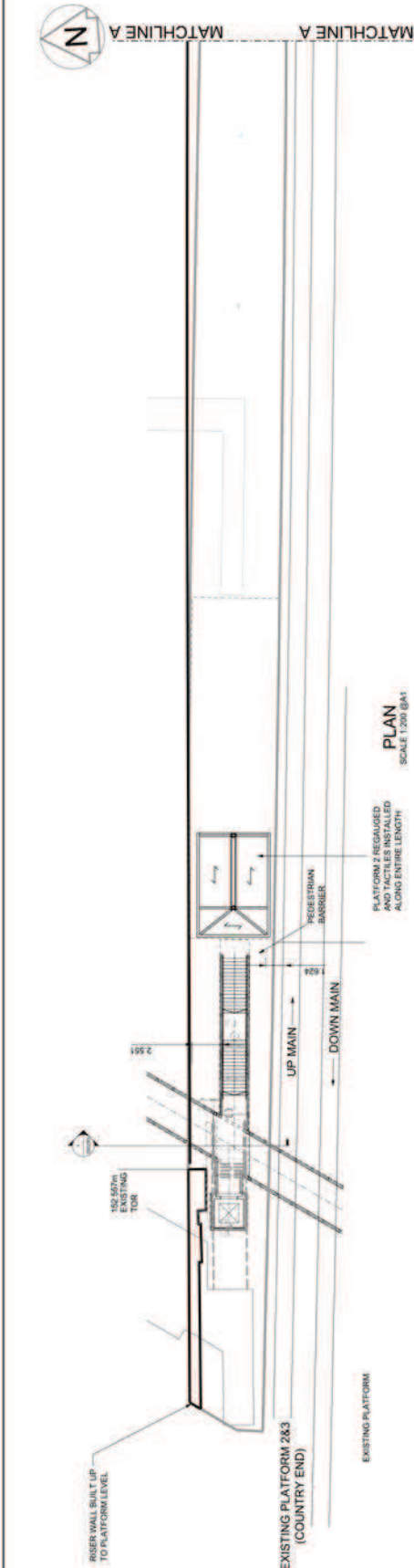
Approved: J. STRATTON
 Date: 17/09/12

Scale: 1:200
 MLN1

Drawing Number: **WSN1B-ALL-DRG-AEA-000271**

Revision: **A02**

Sheet: **1** of **1**



Safety, Health and Environmental information
 normally associated with this type of work
 All references below are to Hazard Log
 WSN1B-EAR-DRG-AEA-000011

Construction
 C, Not applicable
 Ch, Ch
 Operations
 O, Not applicable
 OI, OI
 Maintenance
 M, Not applicable
 M1, M1
 Demolition/Alteration (Future)
 D, Not applicable
 DI, DI

These notes are based on the site of experienced
 and competent contractors carrying out the work
 using an approved safe method of working.

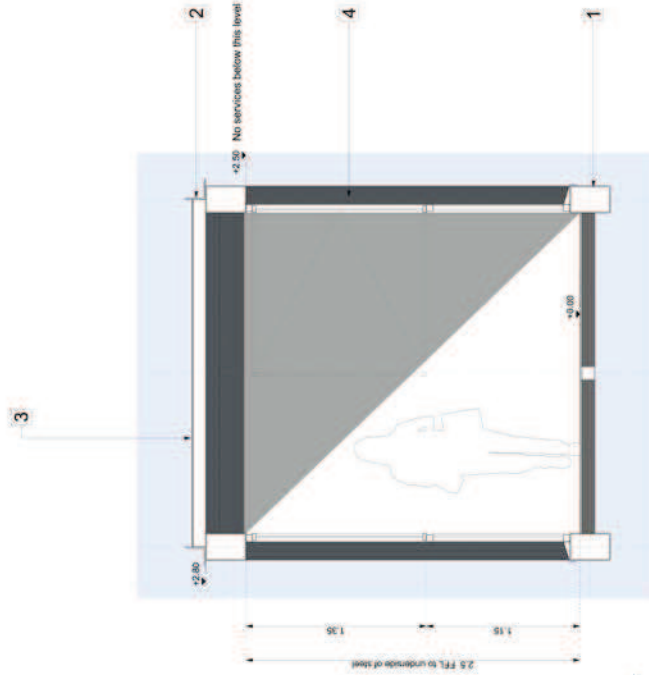
Other Discipline References:
 For Bridge Structures layouts, please refer to
 drawing

ACTION
 WSN1B-EAR-DRG-AEA-00000102
 Hayes & Macgregor
 WSN1B-EAR-DRG-AEA-00000272

Southall
 WSN1B-EAR-DRG-AEA-00002022

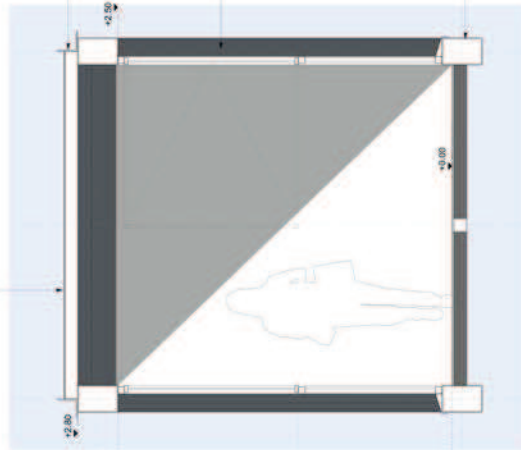
West Drayton
 WSN1B-EAR-DRG-AEA-00002023

West Ealing
 WSN1B-EAR-DRG-AEA-00002012



ELEVATION

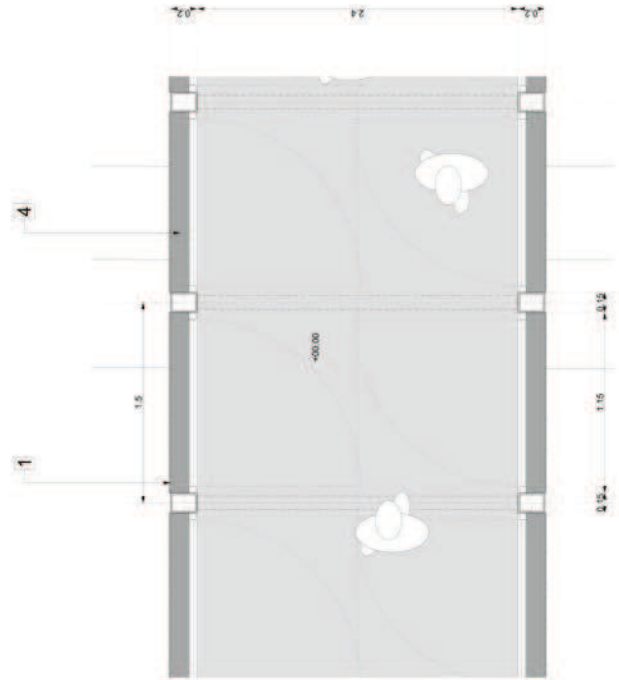
4.50 Min. clearance above rails



CROSS SECTION

Please refer to services
 engineer's drawings for
 site specific structural layout.

- 1 Exposed Steel Frame
 SHS Columns and T-beams with dark grey paint finish RAL 7015
- 2 Steel Angle Ties to Roof/Chimneys (100 x 50mm)
 with dark grey paint finish RAL 7015
- 3 Trapezoidal metal profile roof decking
 e.g. Kingspan K2000R09, design life 50+ years, or similar approved.
- 4 WINDOW
 Galvanneal steel frame with solid galvanneal steel infill panel.
 e.g. Cristal Corporate V20, design life 50+ years, or similar approved



PLAN



FOR FORM A REVIEW

No.	Date	Description of Revisions	Drawn	Chk'd	App'd	Submittal
001	15/04/12	Issued for Review - Station PFA	MM	MM	MM	S3
002	15/04/12	Issued for Review - Station PFA	MM	MM	MM	S3
003	20/05/12	Issued for Review - Station PFA	MM	MM	MM	S3
004	20/05/12	Issued for Review - Station PFA	MM	MM	MM	S3
005	13/12/11	Issued for Review - Station PFA	MM	MM	MM	S3
006	13/12/11	Issued for Review - Station PFA	MM	MM	MM	S3
007	13/12/11	Issued for Review - Station PFA	MM	MM	MM	S3
008	13/12/11	Issued for Review - Station PFA	MM	MM	MM	S3
009	13/12/11	Issued for Review - Station PFA	MM	MM	MM	S3
010	13/12/11	Issued for Review - Station PFA	MM	MM	MM	S3



Project
WSN1B
WEST STATIONS - OUTER/INNER
ENGINEERING - ARCHITECTURE

Drawing Title
STANDARD FOOTBRIDGE
PLAN FOR APPROVAL
PROPOSED BAY STUDY
TYPICAL SPAN

Designated	Approved	Date	Revised
PROWMAN	Approved	06/06/12	1 of 1
PROWMAN	Approved	06/06/12	1 of 1
EMRCHAND	Approved	05/06/12	1 of 1
JLPOCOBEE	Approved	06/06/12	1 of 1
Author	1:20	MILN1	Sheet
Alteration Reference			

Safety, Health and Environmental Information
 Safety, Health and Environmental information normally associated with this type of work is normally associated with the type of work. All references below are to Hazard Log WSN1B-EAR-DRG-AEA-000260.

Construction
 C, Not applicable
 CH, CH
 Operations
 O, Not applicable
 OH, OH
 Maintenance
 M, Not applicable
 MI, MI
 Demolition/Overhaul (Future)
 D, Not applicable
 DH, DH

These notes are based on the site of experienced and competent contractors carrying out the work using an approved safe method of working.

Other Discipline References:
 For Bridge Structures layouts, please refer to drawing:

Action
 WSN1B-EST-DRG-AEA-00020322
 Hayes & Harlington
 WSN1B-EST-DRG-AEA-00020372
 Southall
 WSN1B-EST-DRG-AEA-00020222
 West Drayton
 WSN1B-EST-DRG-AEA-00020303
 Westalling
 WSN1B-EST-DRG-AEA-00020112

Legend/Notes

Platform Edge
 Back of Coping



PLAN



ELEVATIONS

- 1 Exposed Steel Frame
SIS Columns and Trusses with dark grey paint finish (RAL 7015)
- 2 Steel Angle Trim to Roof Overhang (500 x 500mm)
with dark grey paint finish (RAL 7015)
- 3 Tripartite metal panels and decking
e.g. Kingspan K2200RM, design life 60+ years or similar approved
- 4 WINDOW
Metal roof frame with solid polycarbonate sheet (6mm)
e.g. Corral Corporate 1020, design life 60+ years, or similar approved
- 5 Precast Concrete Panel
Lift motor room and shaft support at low level



Rev	Date	Description of Revision	Drawn	Chk'd	Appr'd	Submittal
001						

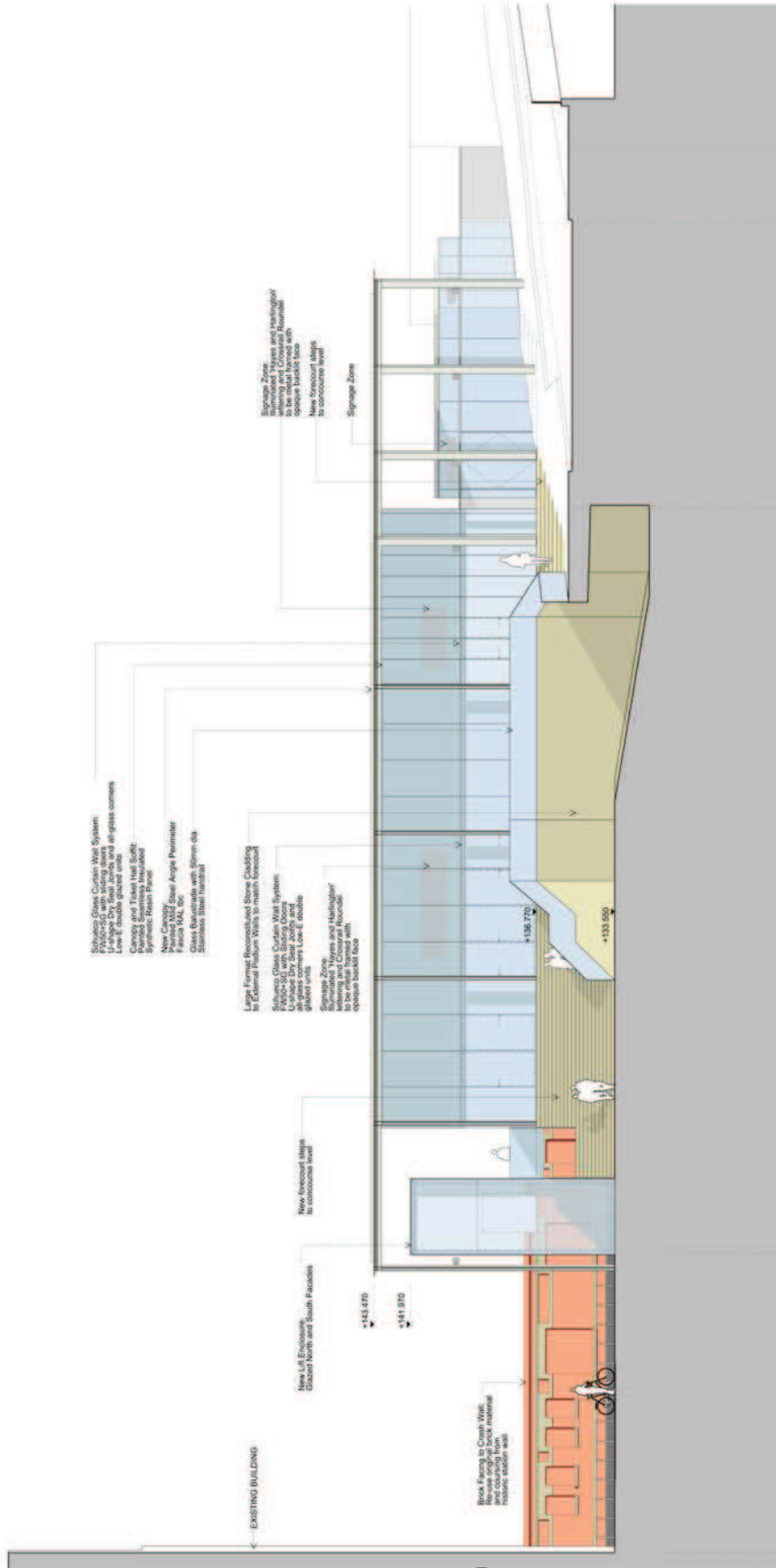
SHARED - (G4) FORM D AUTH S4



Project
WSN1B
WEST STATIONS - OUTER/INNER
ENGINEERING - ARCHITECTURE

Drawing Title
HAYES & HARLINGTON STATION
PLAN FOR APPROVAL
PROPOSED BAY STUDY
TYPICAL STAIRCASE

Design/Drawn	Checked	Approved	Scale	Altitude Reference	Sheet
PROVWANN	ABCOGLE	EMERICHAND	1:20	MILN1	1 of 1



- Gray Lines (Not for Approval)
- Gray Shading (Structures Not for Approval)
- Dark Gray Shading (New Structures For Approval)

Rev	Date	Description of Revisions	Drawn	Check	Submittal
001	10/11/14	ISSUED FOR APPROVAL			
002	10/11/14	ISSUED FOR APPROVAL			
003	10/11/14	ISSUED FOR APPROVAL			
004	10/11/14	ISSUED FOR APPROVAL			

Approval Required for Schedule 7



Customer(s)

Project

WSN1C
Crossrail West Stations - Bennetts
ENGINEERING - ARCHITECTURE

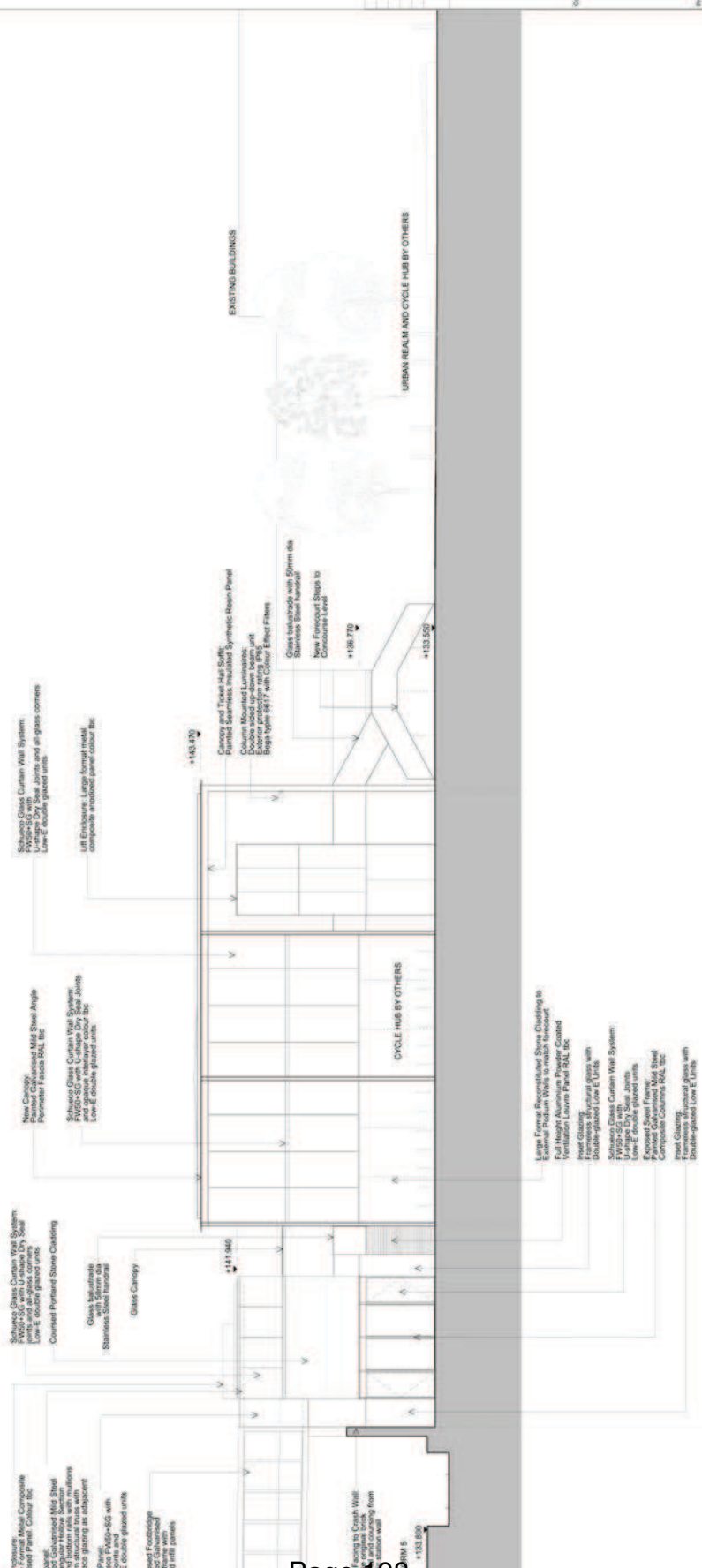
Drawing Title

HAYES & HARLINGTON STATION
PLAN FOR APPROVAL
PROPOSED BUILDING ELEVATION
NORTH

Discipline	Design	Check	Date
Structural	DONALDSON		05/11/14
Architect	CARTER		05/11/14
MEP	CLARKE		05/11/14

Scale: 1:100
Drawing Number: WSN1C-99999-00-CR001-DRG-A-000255
Revision: 1 of 1
Drawing Title: WSN1C-EAR-DRG-BEN-000255 P.02

- Grey Lines (Not for Approval)
- Light Grey Shading (Structures Not for Approval)
- Dark Grey Shading (New Structures For Approval)



Footbridge

New Staff Facilities and Footbridge Link Enclosure

Proposed New Station and Canopy

Station Square

10m

SCALE 1:100

0 2 4 6 8 10m

PROPOSED EAST ELEVATION



Approval Required for Schedule 7

Rev	Date	Description of Revisions	Drawn	Chk'd	Appr'd
1	18/11/14	ISSUED FOR APPROVAL	DR	TC	DL
2	18/11/14	ISSUED FOR APPROVAL	DR	TC	DL
3	18/11/14	ISSUED FOR APPROVAL	DR	TC	DL

Project: WSN1C
 Crossrail West Stations - Bennetts
 ENGINEERING - ARCHITECTURE

Client: HAYES & HARLINGTON STATION
 PLAN FOR APPROVAL
 PROPOSED BUILDING ELEVATION
 EAST

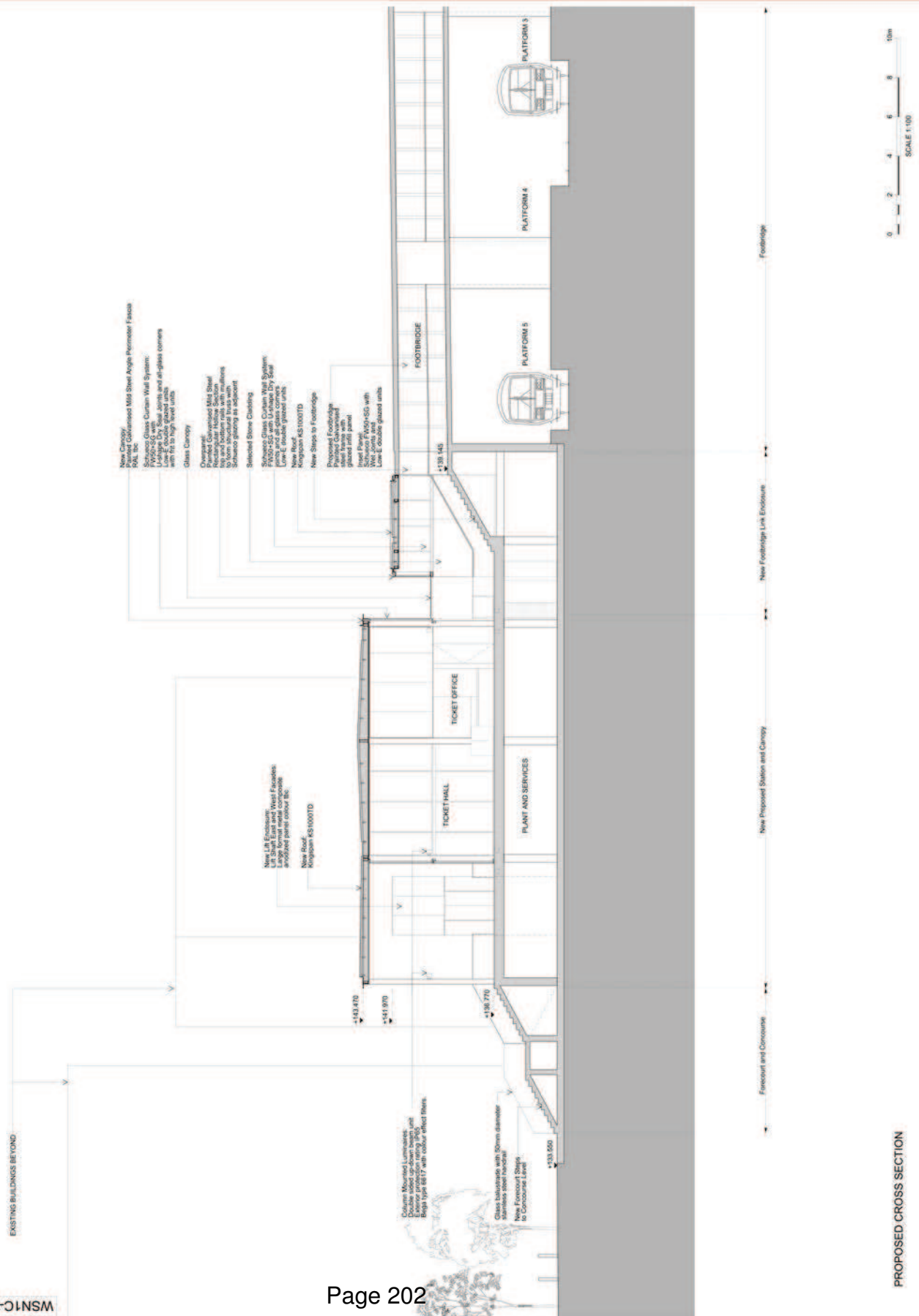
Designing: The
 HAYES & HARLINGTON STATION
 PLAN FOR APPROVAL
 PROPOSED BUILDING ELEVATION
 EAST

Approval: 1:100

Approval Reference: CR_WSN1C-99899-00-CR001-DDR-A-000254
 Drawing Number: WSN1C-EAR-DRG-BEN-000254 P.02

Sheet 1 of 1

- Grey Lines (Not for Approval)
- Grey Shading (Structures Not For Approval)
- Dark Grey Shading (New Structures For Approval)



New Canopy
Painted Galvanneal Mild Steel Angle Perimeter Fascia
P.A.L. 100
Fixed Glass Curtain Wall System:
Fixed Glass with
Low-E double glazed units and air-glass corners
with fit to high-level units
Glass Canopy

Overhead
Painted Galvanneal Mild Steel
High and bottom rails with multiple
straps and stainless steel
straps painted in adjacent
Scheme paint as adjacent

Standard Stone Cladding
Painted Galvanneal Mild Steel
P.A.L. 100
New Roof:
Kingdom AS1000TD

Proposed Footbridge
Painted Galvanneal
plated mild steel
Steel Panels
Weld joints and
Low-E double glazed units.

New L.A. Enclosure
New Footbridge
Large format metal composite
architectural panel colour: BC
New Roof:
Kingdom AS1000TD

Double Glazed Aluminium
Double Glazed Aluminium
External protection using 200g
edge tape and 17mm rubber gasket blocks

Glass balustrade with 20mm diameter
New Footbridge Slings
to Concourse Level

Rev	Date	Description of Revisions	Drawn	CHK'd	Appr'd
1	18/11/14	ISSUED FOR APPROVAL	DL	TC	DL
2	18/11/14	ISSUED FOR APPROVAL	DL	TC	DL
3	18/11/14	ISSUED FOR APPROVAL	DL	TC	DL

Approval Required for Schedule 7

WSN1C
Crossrail West Stations - Bennetts
ENGINEERING - ARCHITECTURE

HAYES & HARLINGTON STATION
PLAN FOR APPROVAL
PROPOSED BUILDING SECTIONS
CONCOURSE

Drawn: DL
Checked: T CARTER
Approved: DLJANG
Scale: 1:100

Client: Network Rail
Project: Hayes & Harlington Station
Drawing Number: CR_WSN1C-98699-00-CR001-DRG-A-000257
Revision: 1 of 1
Drawing Title: WSN1C-EAR-DRG-BEN-000257 P.02







Report of the Head of Planning, Sport and Green Spaces

Address PRONTO INDUSTRIAL ESTATE AND 585 - 591 UXBRIDGE ROAD HAYES

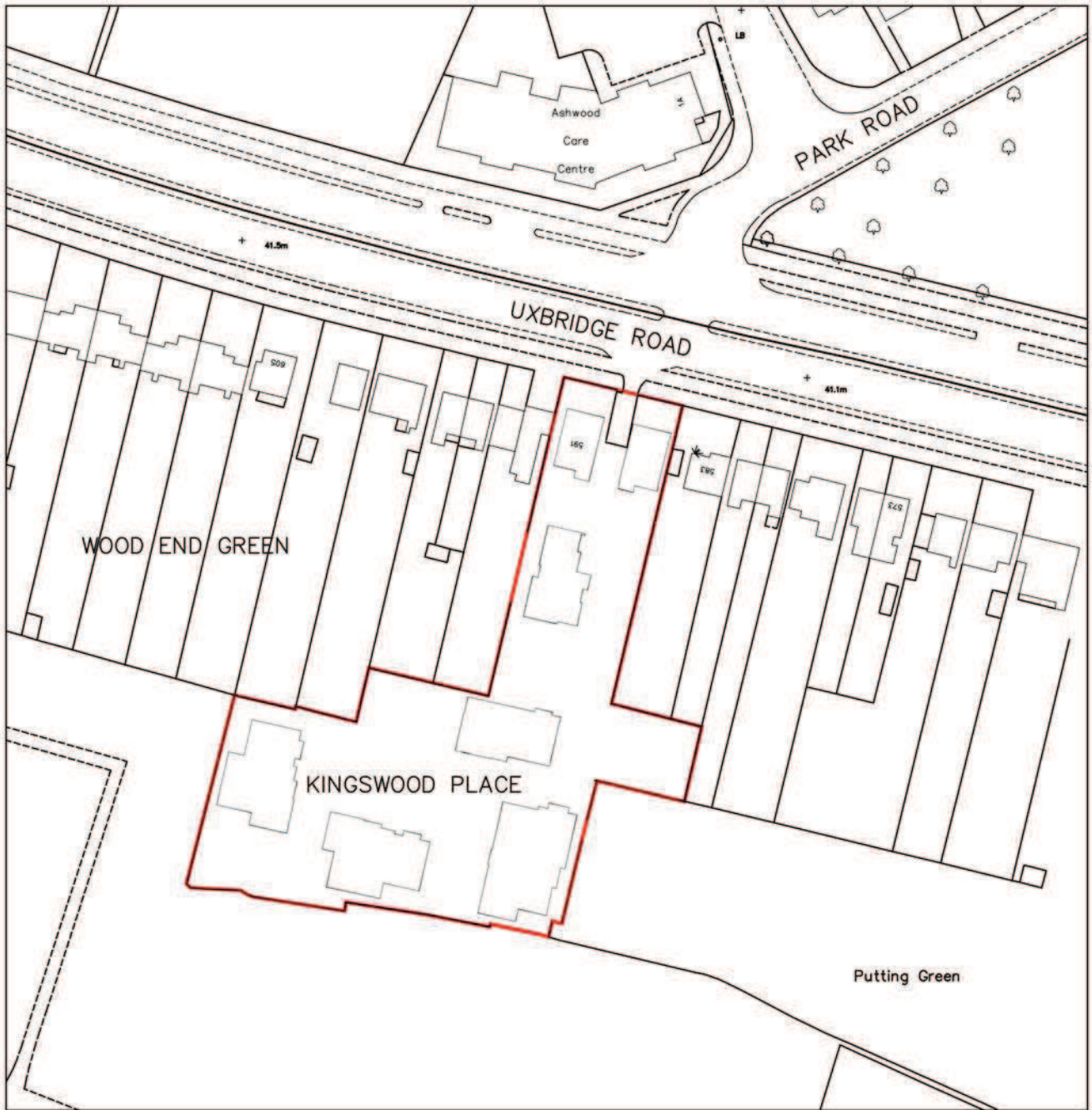
Development: Change of use of B1(c) floorspace to provide 12 additional residential units and associated ancillary works. (Amendment to planning permission ref: 4404/APP/2011/2079, dated 30-03-2012 (Application to replace extant planning permission ref: 4404/APP/2008/3558, dated 23-03-2009); Redevelopment of site to provide replacement Class B1(c) light industrial space and 34 two-bedroom and 9 one-bedroom flats with associated car parking, landscaping and amenity space).

LBH Ref Nos: 4404/APP/2014/2506

Date Plans Received: 16/07/2014

Date(s) of Amendment(s):

Date Application Valid: 17/07/2014



Check all dimensions and report all errors and omissions.
 This plan was prepared in conjunction with construction specifications TP4-MD-001.
 All materials to be installed strictly in accordance with manufacturer's recommendation.



ACCOMMODATION SCHEDULE:

BLOCK A: 1 Units	12 3000 sqm
BLOCK B: 5 Units	12 3000 sqm
BLOCK C: 2 Units	12 3000 sqm
BLOCK D: 4 Units	12 3000 sqm
TOTAL 4x BED TOTAL 12 UNITS	

12 x 3000 sqm, Block B (12)

Uxbridge Road

One-way only vision splay of 120m to Highways Approval

Pronto Trading Estate, Uxbridge Rd, Hayes

SITE PLAN

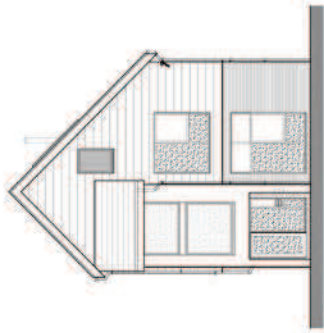
Thorney Farm Developments

DATE	JUNE 2014
PROJECT	1:200@A1
DESIGNER	TPH4
CLIENT	PL-011S41

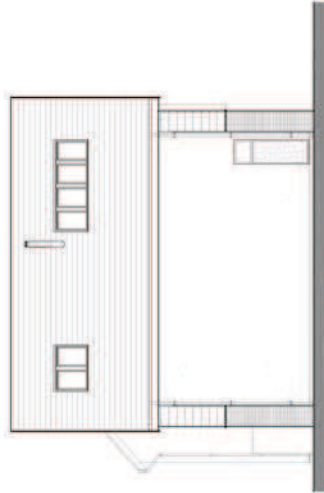
jasa
 JSA ARCHITECTURE
 ARCHITECTS AND INTERIORS
 11, THE GARDENS
 WINDYBUSH, DURHAM
 TEL: 0191 283 2222
 WWW.JSA-ARCHITECTS.CO.UK



Proposed Ground Floor Plan



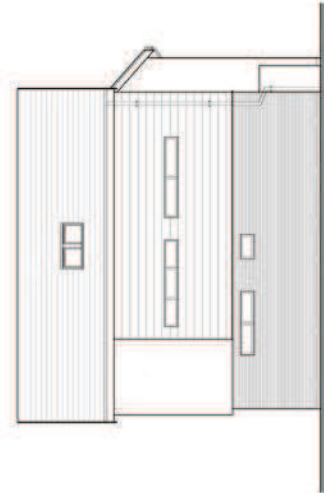
Proposed Front Elevation



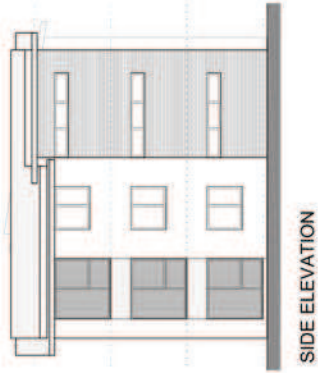
Proposed Side Elevation



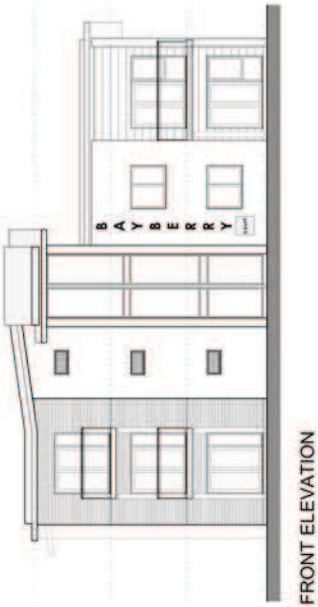
Proposed Rear Elevation



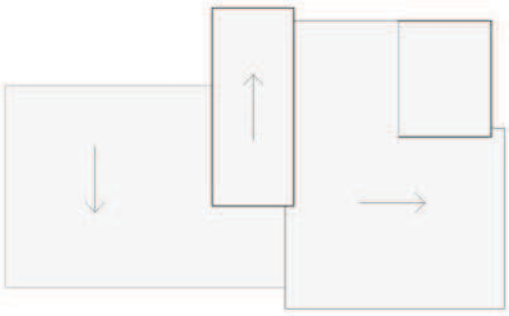
Proposed Side Elevation



FRONT ELEVATION



REAR ELEVATION



ROOF PLAN



SECOND FLOOR



FIRST FLOOR



GROUND FLOOR



Project: TP-14
 Design: TP-14-008-A1
 Date: APR 2014
 Checked by: [Signature]

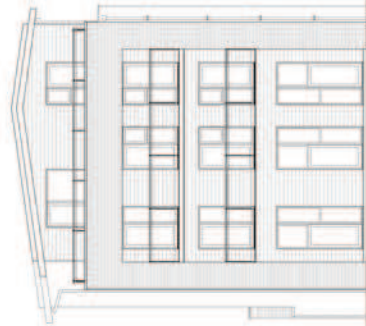
Thornley Farm Developments
 Toronto Tracing Estate, Uxbridge Rd, Hayes
 Proposed Floor Plans and Elevations
 Block B



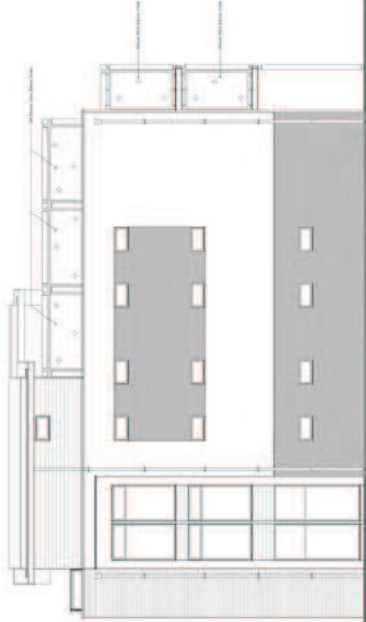
Proposed Ground Floor Plan



Proposed Front Elevation



Proposed Side Elevation



Proposed Rear Elevation



Proposed Side Elevation



Page 216

Proposed Ground Floor Plan



Proposed Front Elevation



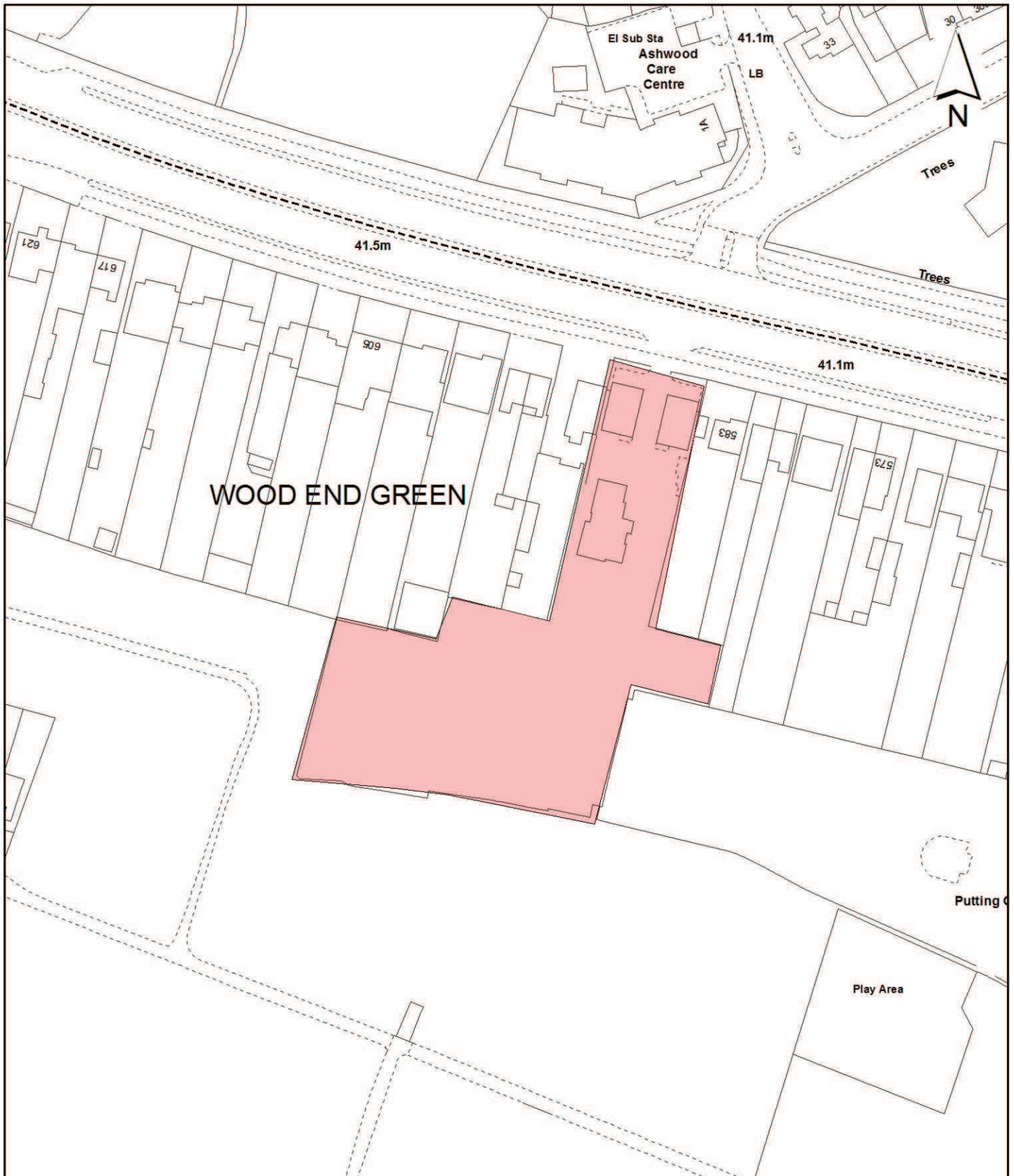
Proposed Side Elevation



Proposed Rear Elevation



Proposed Side Elevation



Notes

 Site boundary

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Site Address

**Pronto Industrial Estate
 & 585 - 591 Uxbridge Road
 Hayes**

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111

Planning Application Ref:

4404/APP/2014/2506

Scale

1:1,250

Planning Committee

Major Page 217

Date

March 2015



HILLINGDON
 LONDON

Report of the Head of Planning, Sport and Green Spaces

Address 1 NOBEL DRIVE HARLINGTON

Development: Conversion and extension of existing office building to form a 200 bedroom hotel with banqueting suite, conference facilities, and rooftop restaurant, including a seven-storey extension to rear, a three storey addition at roof level and single-storey side extension, together with the creation of a new vehicle access, and alterations to car parking and landscaping.

LBH Ref Nos: 46214/APP/2014/2827

Date Plans Received: 08/08/2014

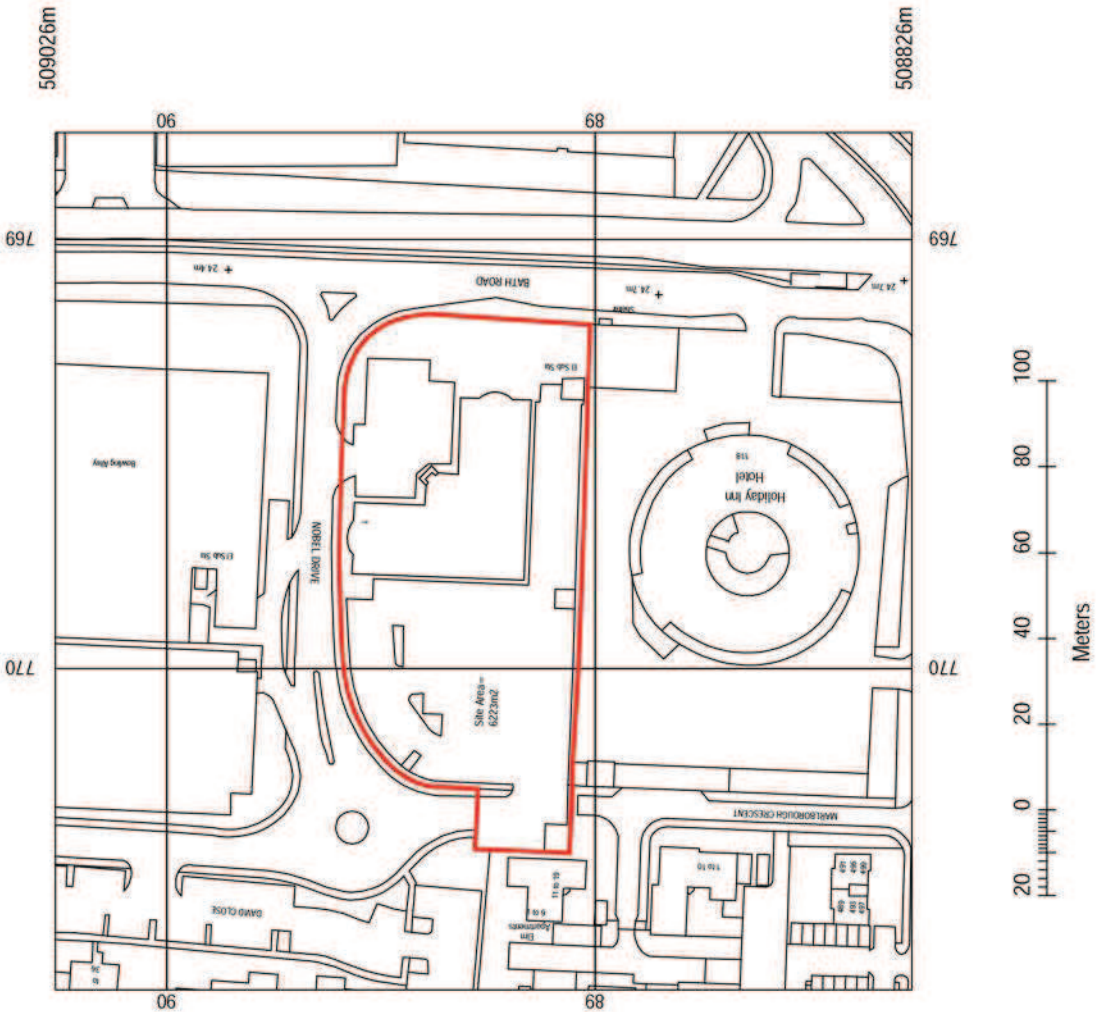
Date(s) of Amendment(s):

19/02/2015

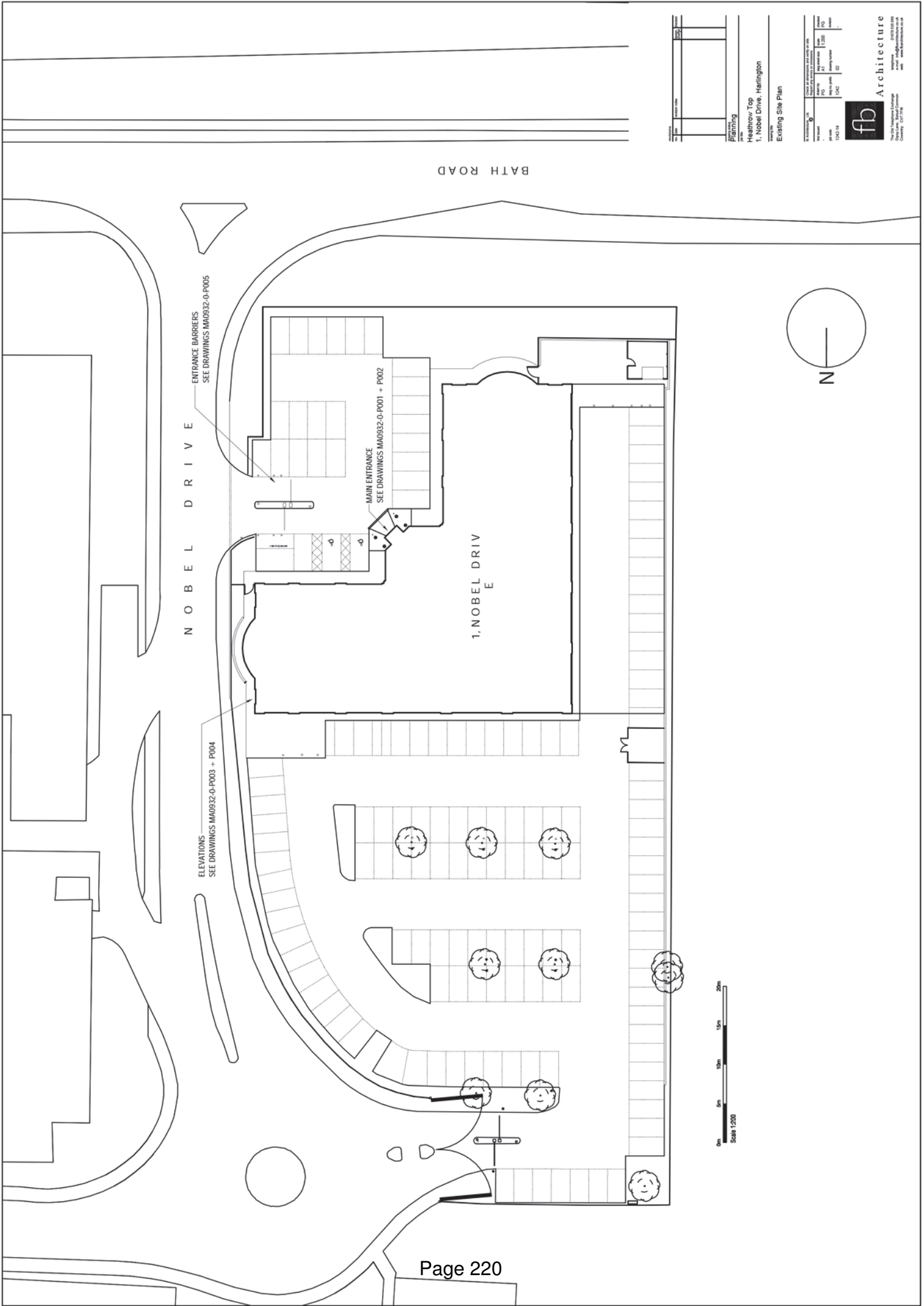
Date Application Valid: 03/09/2014

08/08/2014

23/02/2015



rev | date revisions	revision notes	checked	design change	drawing status	Architects @	Check all dimensions and verify on site. Report any errors or omissions.	checked
job title:	Heathrow Top 1 Nobel Drive, Harlington	drawing title	Location Plan	Planning	Architects @ first issued job code 1242-14	drawn by PG	checked
						drawn short size A3	revision
						drawing number 01	
						scale 1:1250	
						dag no prefix	

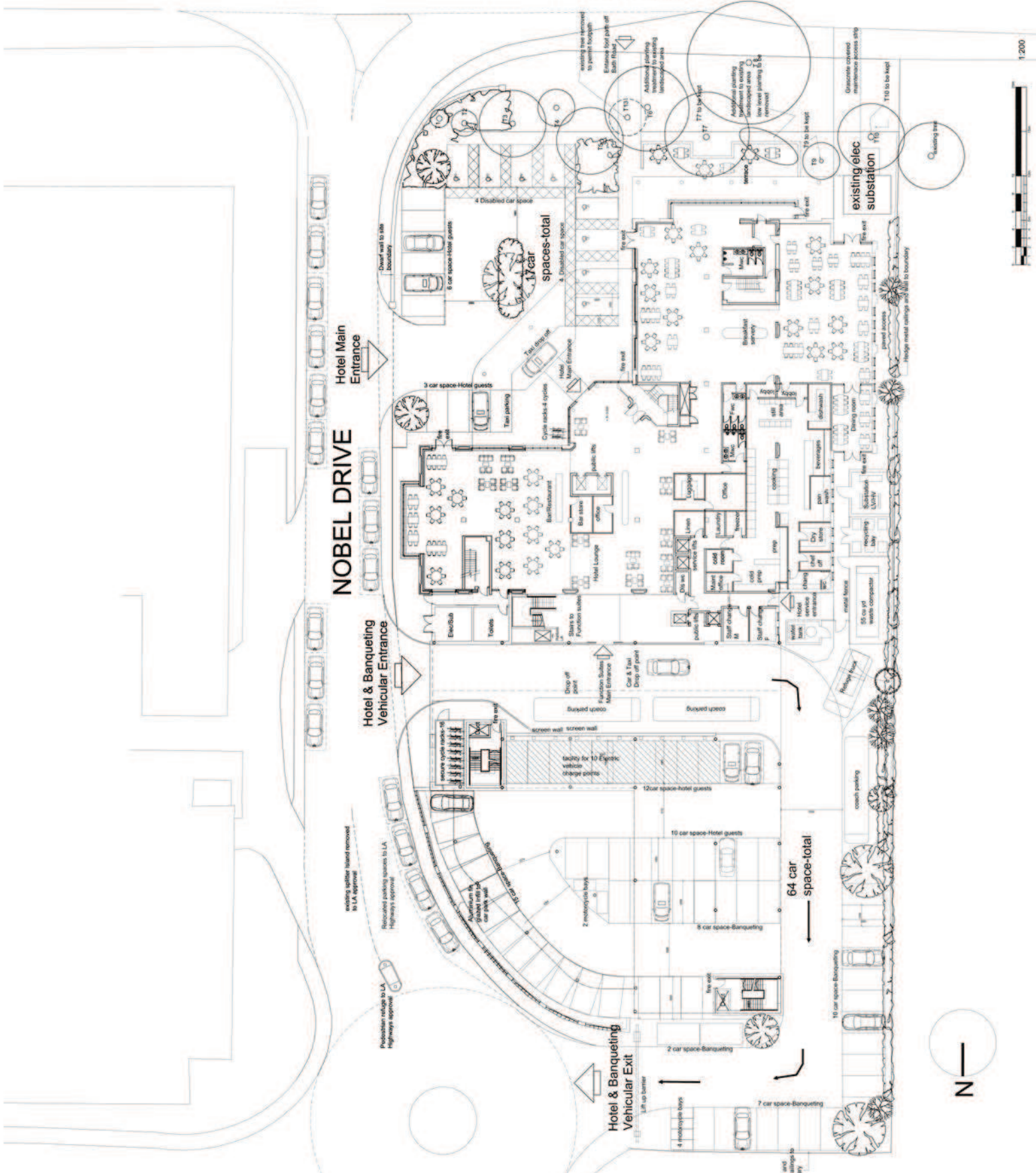


NO.	REVISIONS	DATE

fb Architecture
 Heathrow Top
 1. Nobel Drive, Harlington
 Existing Site Plan

fb Architecture
 The Old Haystack Exchange
 10000 Old Harlington Road
 Harlington, Leicestershire
 LE15 8JG
 Tel: 01533 531304
 Fax: 01533 531305
 Email: info@fbarchitecture.co.uk
 Website: www.fbarchitecture.co.uk

BATH ROAD



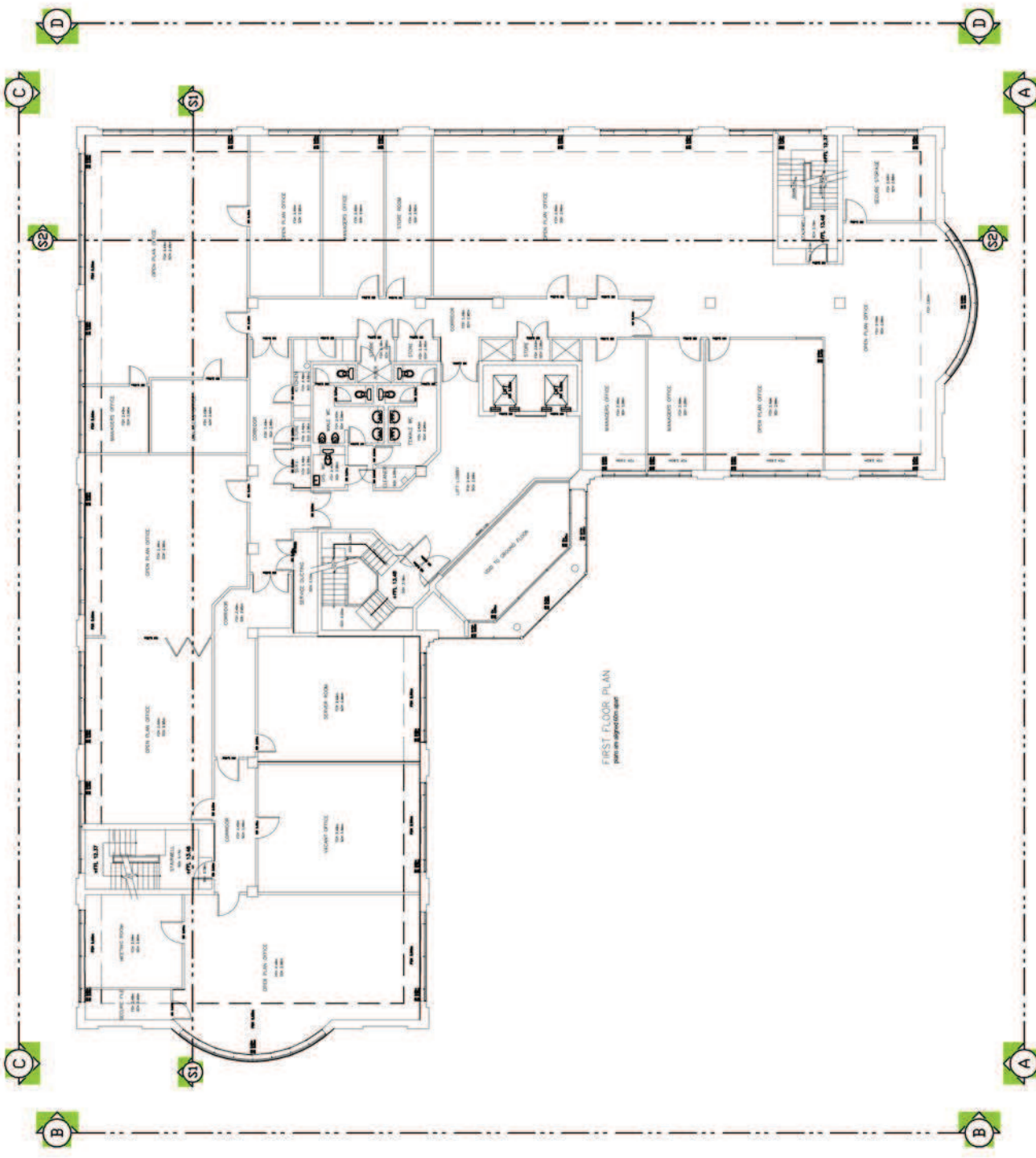


rev / date	revision notes	design change	checked	drawing status	Scale Ltd Architects	Check all dimensions and verify on site. Report any errors or omissions.	drawn by	dwg sheet size	scale	checked
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					job code		dwg no. prefix	drawing number		
					1242-14		-	03		-
					Existing Ground Floor Plan					
					drawing title					

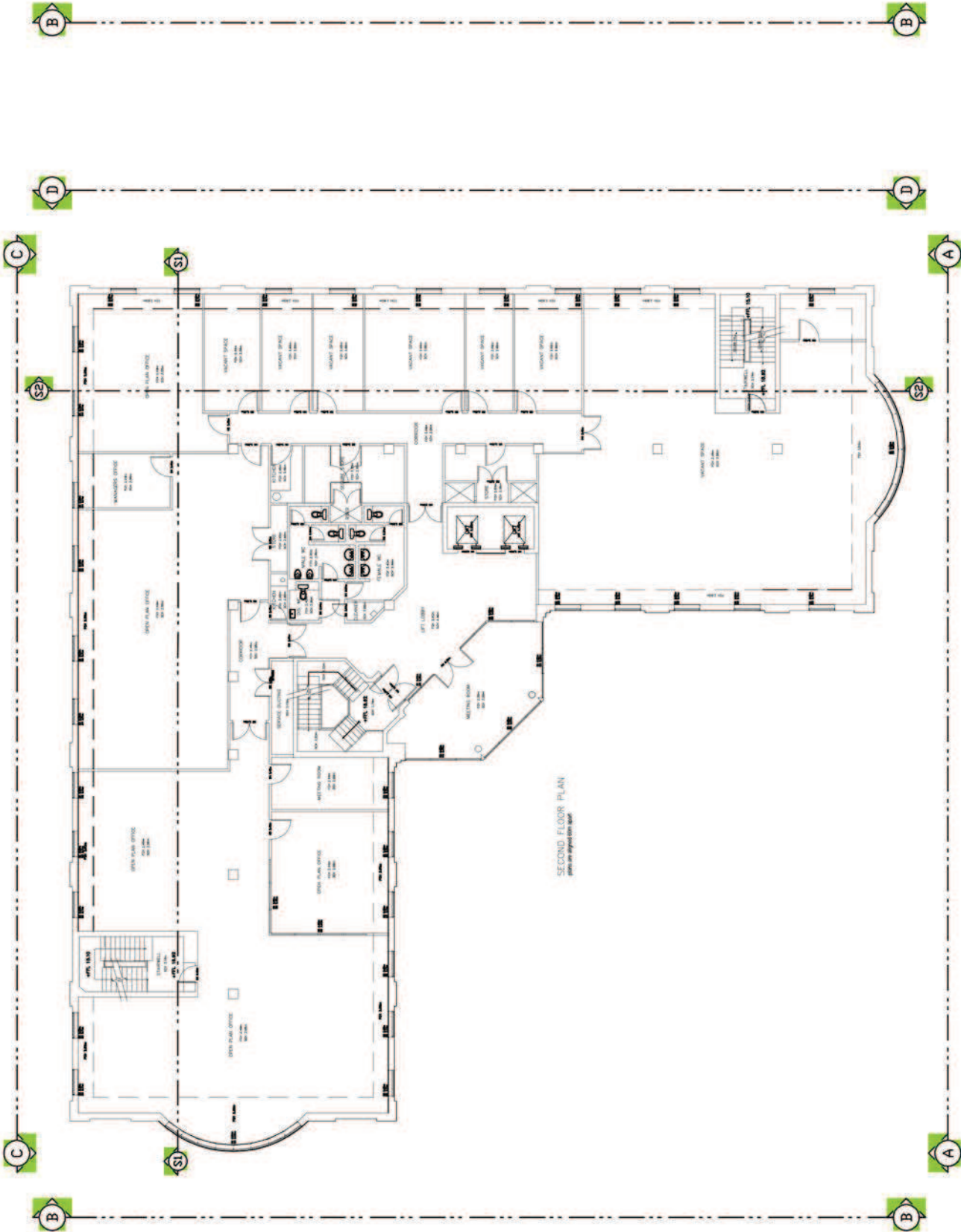
Heathrow Top
1 Nobel Drive, Harlington



Architecture
Scale Ltd Architects
The Old Telephone Exchange
Clipsy Lane, Basingstoke
Hampshire, RG24 0AP
Tel: 01256 335530
E-mail: info@scale.co.uk
Web: www.scale.co.uk



rev / date	revision notes	design change	checked	drawing status	Scale	checked
Heathrow Top 1 Nobel Drive, Harlington				Survey Existing First Floor Plan	A3 1:200	04
job title:				drawing number:	drawing no. prefix:	revision:
job code: 1242-14				first issued:	drawn by: PG	checked:
job issued:				job code:	drawing number:	revision:
job title:				job code: 1242-14	drawing number: 04	revision: -



SECOND FLOOR PLAN
 items as agreed with client

rev. / date revisions	revision notes	design change	checked	drawing status	Survey	Pella Ltd Architects	Check all dimensions and verify on site. Report any errors or omissions.		
							drawn by PG	dwg sheet size A3	scale 1:200
job title Heathrow Top 1 Nobel Drive, Harlington		drawing title Existing Second Floor Plan		first issued 1242-14	checked 05	drawing number 05		revised	



Second Floor Plan



1.200

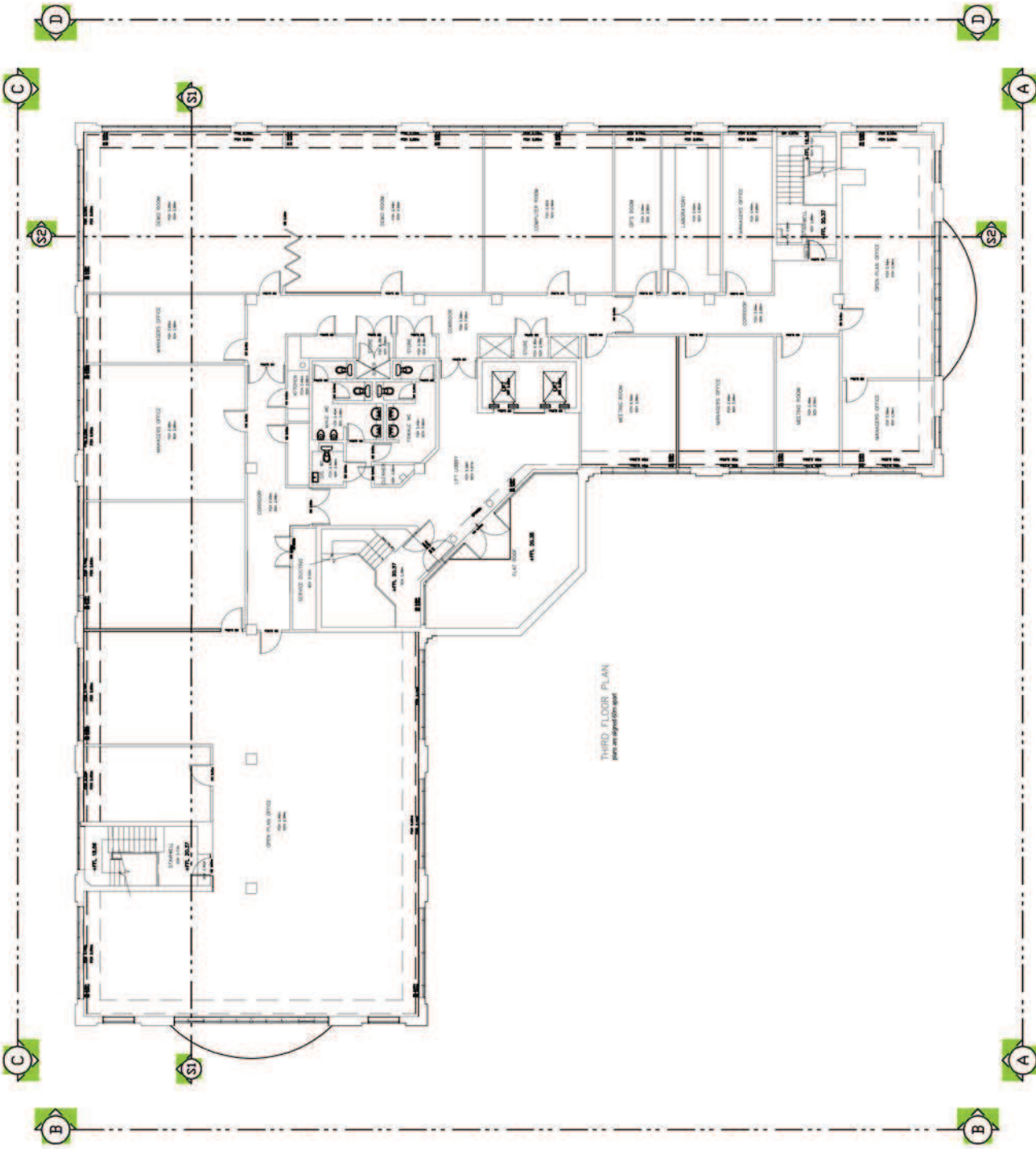
Project Name	132318 Heathrow Top Hotel
Client	132318 Heathrow Top Hotel
Location	1 Nobel Drive, Harlington
Scale	1:100
Date	13/05/14

fb Architecture
 Heathrow Top Hotel
 1 Nobel Drive, Harlington
 Middlesex TW17 1AA

Project Name	132318 Heathrow Top Hotel
Client	132318 Heathrow Top Hotel
Location	1 Nobel Drive, Harlington
Scale	1:100
Date	13/05/14

Project Name	132318 Heathrow Top Hotel
Client	132318 Heathrow Top Hotel
Location	1 Nobel Drive, Harlington
Scale	1:100
Date	13/05/14

fb Architecture
 132318 Heathrow Top Hotel
 1 Nobel Drive, Harlington
 Middlesex TW17 1AA
 www.fb-architecture.co.uk



rev	date	revision notes	design change	checked	drawing status	Scale	1:200	checked
					Survey	drawn by	PG	revision
Heathrow Top 1 Nobel Drive, Harlington			Existing Third Floor Plan			drawn by	PG	revision
job title:			job code			drawing number	06	
			1242-14			drawn by	PG	revision
			1242-14			drawing number	06	
			1242-14			drawing number	06	



Third Floor Plan

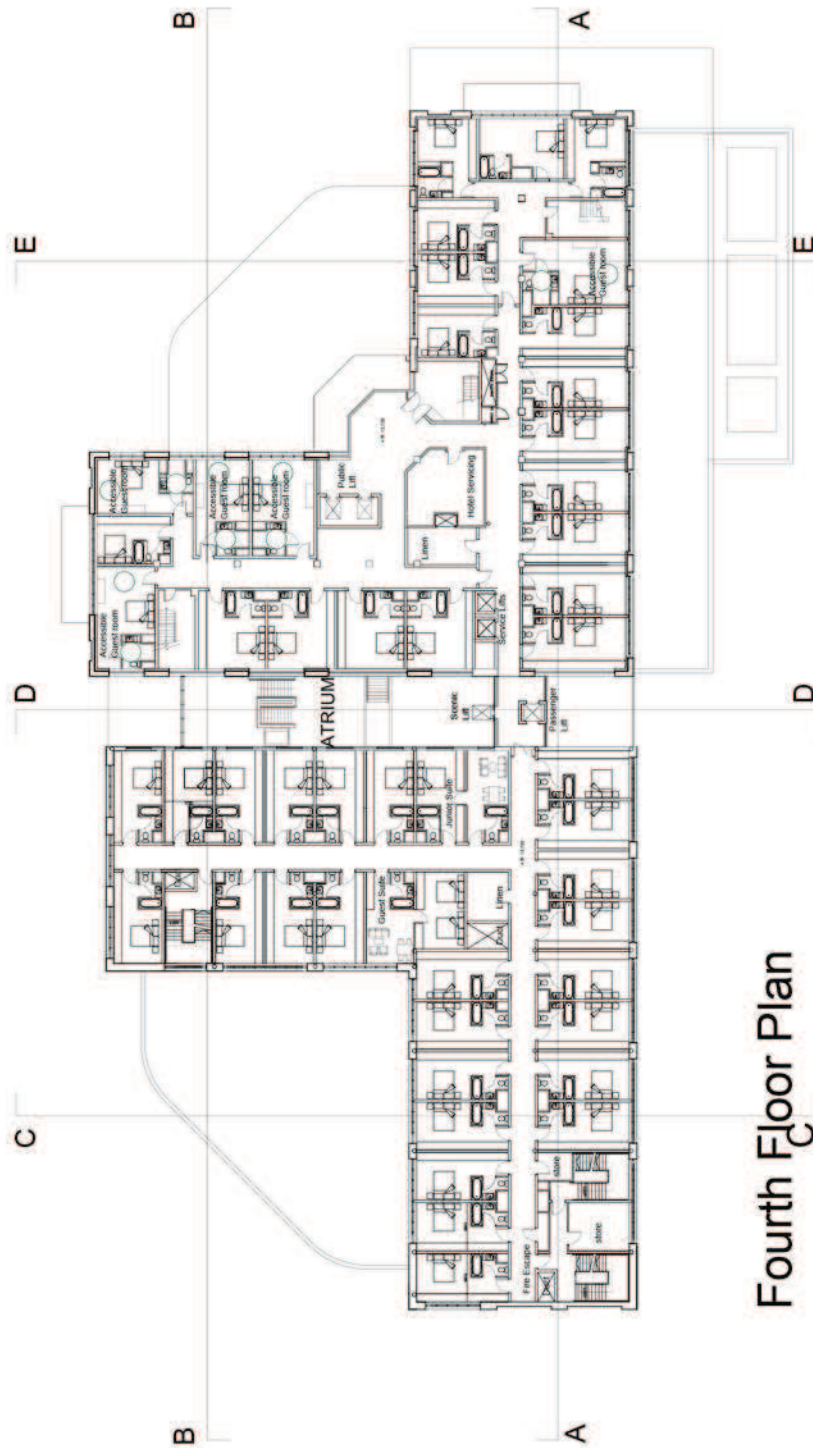


NO.	REVISION	DATE
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2	11.1.14	Final Construction Documents
3	12.1.14	Final Construction Documents
4	13.1.14	Final Construction Documents

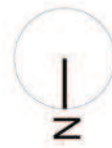
fb
 Heathrow Top Hotel
 1 Nobel Drive, Harlington
 Proposed Third Floor Plan

NO.	REVISION	DATE
1	10.1.13	Final Approval
2	11.1.14	Final Construction Documents
3	12.1.14	Final Construction Documents
4	13.1.14	Final Construction Documents

fb Architecture
 1 Nobel Drive, Harlington
 Harlington, Middlesex HA8 7NS
 Tel: 0208 811 1111
 Email: info@fbarchitecture.co.uk
 www.fbarchitecture.co.uk



Fourth Floor Plan

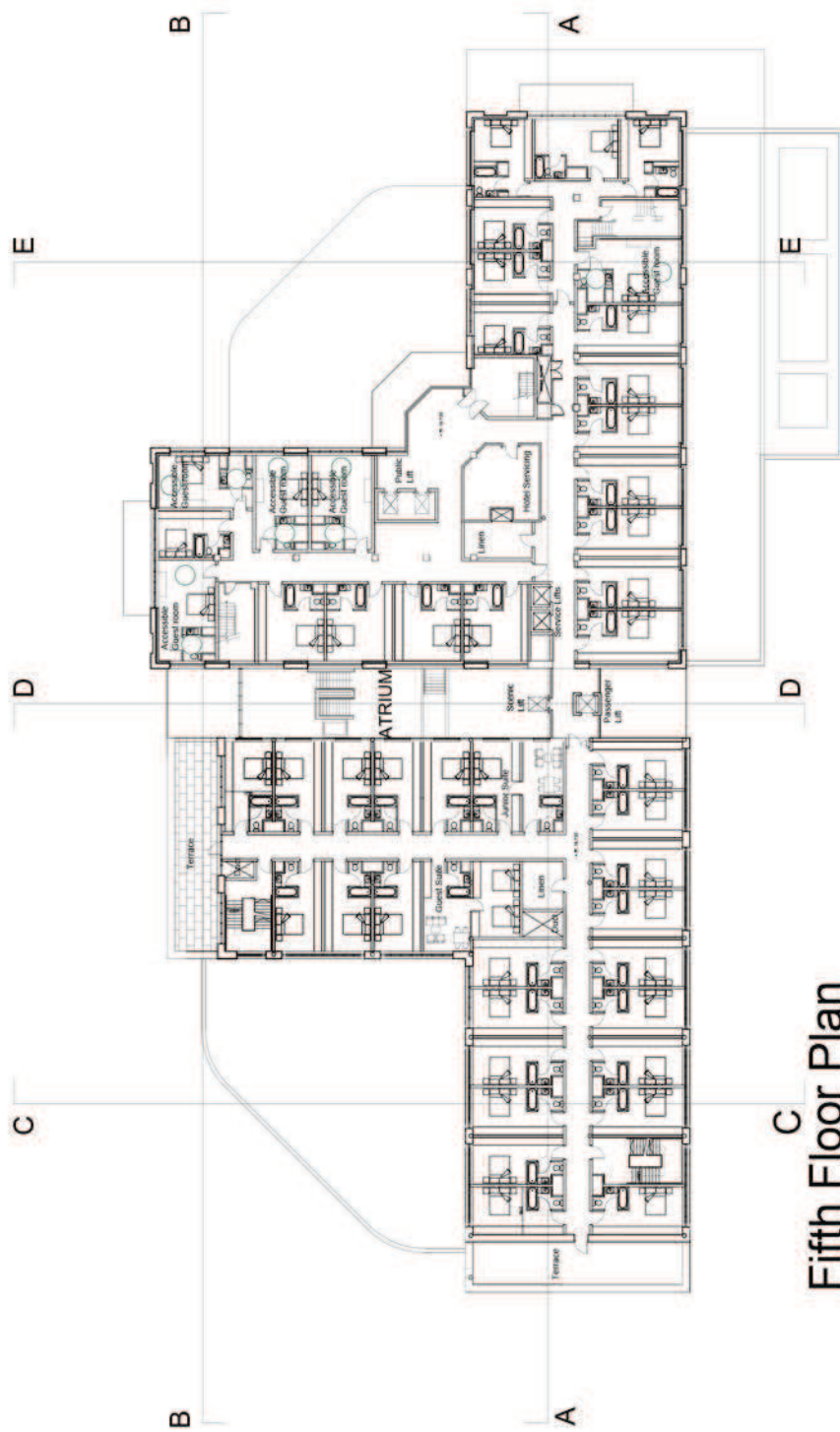


NO.	DATE	DESCRIPTION	BY	CHECKED
A	10.12.10	Final Planning	...	...
B	10.12.10	Final Architecture	...	...
C	10.12.10	Final Services	...	...
D	10.12.10	Final Structure	...	...

Planning
 Heathrow Top Hotel
 1 Nobel Drive, Hatfield
 Herts AL10 9BB
 Proposed Fourth Floor Plan

NO.	DATE	DESCRIPTION	BY	CHECKED
A	10.12.10	Final Planning	...	...
B	10.12.10	Final Architecture	...	...
C	10.12.10	Final Services	...	...
D	10.12.10	Final Structure	...	...

fb Architecture
 1 The Courtyard
 Hatfield, Herts AL10 9BB
 Tel: 0457 843333
 Fax: 0457 843334
 Email: info@fbarchitecture.co.uk
 Website: www.fbarchitecture.co.uk



Fifth Floor Plan

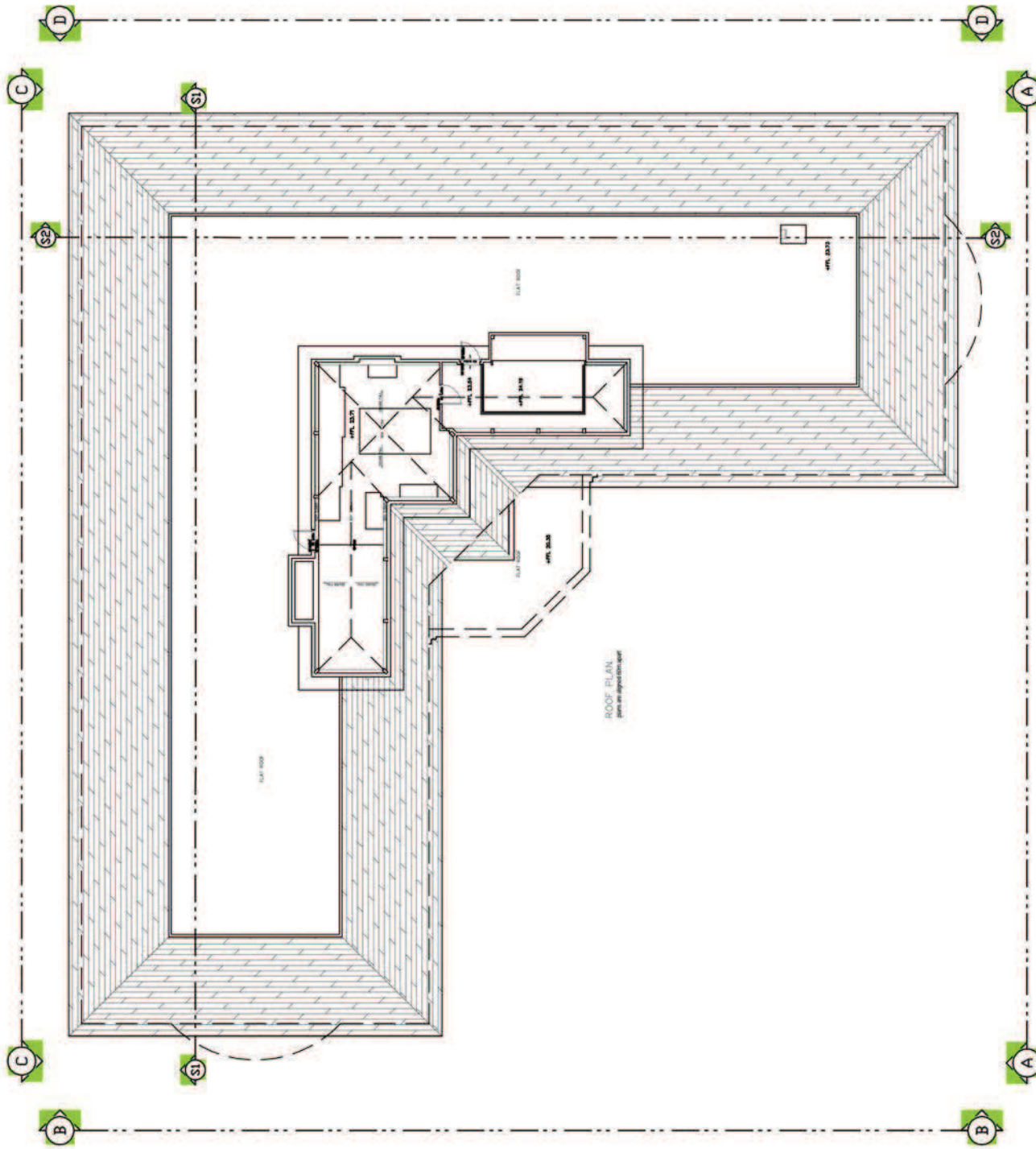


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3	Final Planning	10/15/14	MM	MM
4	Final Planning	10/15/14	MM	MM

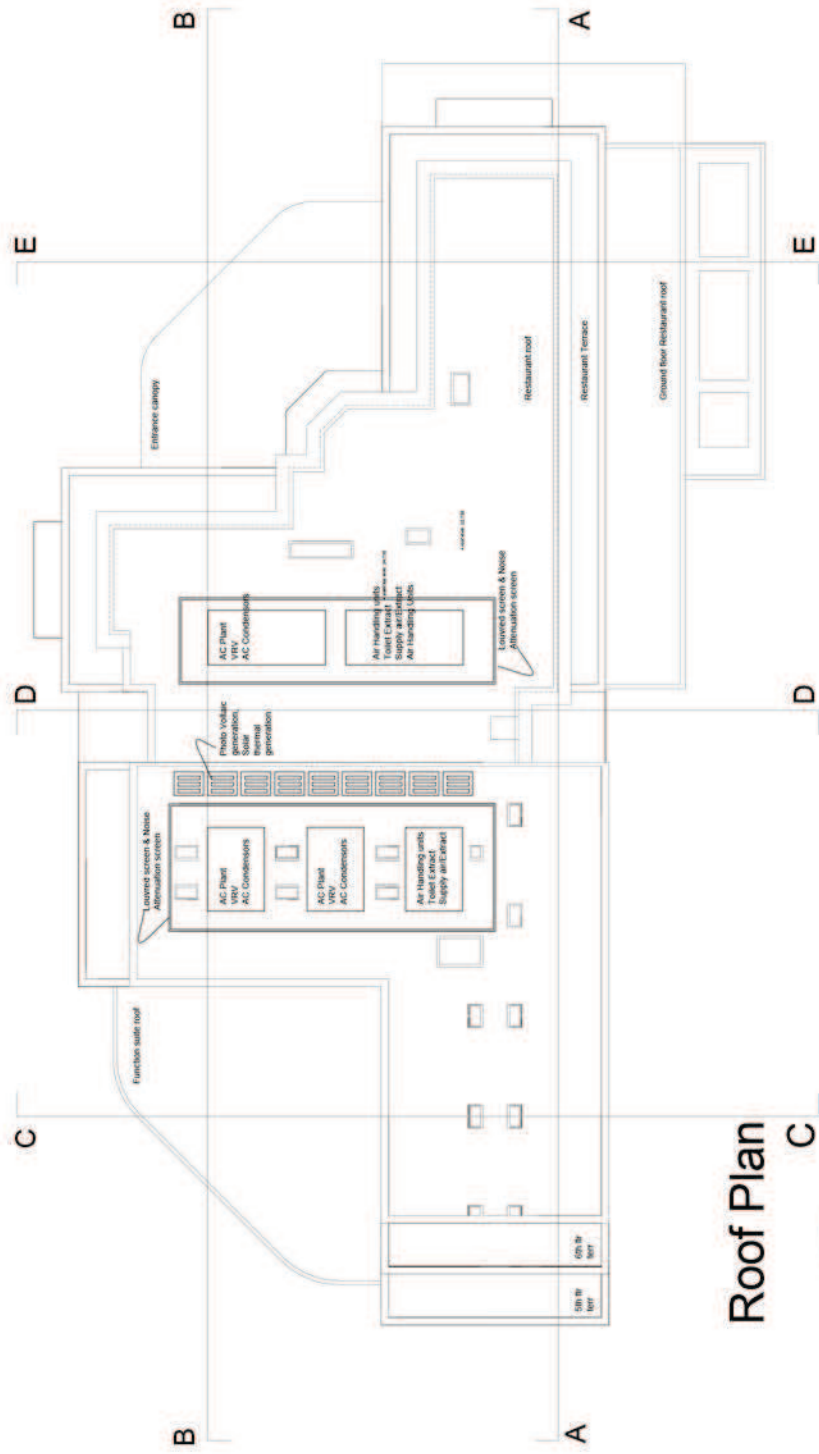
Planning
 Heathrow Top Hotel
 1 Nobel Drive, Hatfield, VA
 22061
 Proposed Fifth Floor Plan

NO.	REVISION	DATE	BY	CHKD.
1	Final Planning	10/15/14	MM	MM
2	Final Planning	10/15/14	MM	MM
3	Final Planning	10/15/14	MM	MM
4	Final Planning	10/15/14	MM	MM

fb Architecture
 11000 Lee Highway, Suite 100
 Fairfax, VA 22030
 Phone: 703.261.1100
 Fax: 703.261.1101
 www.fbarchitecture.com



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					Survey	first issued	
Heathrow Top 1 Nobel Drive, Harlington						job code	1242-14
Existing Roof Plan						drawn by	PG
						dwg sheet size	A3
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						scale	1:200
						drawing number	07
						checked	-
						revision	-

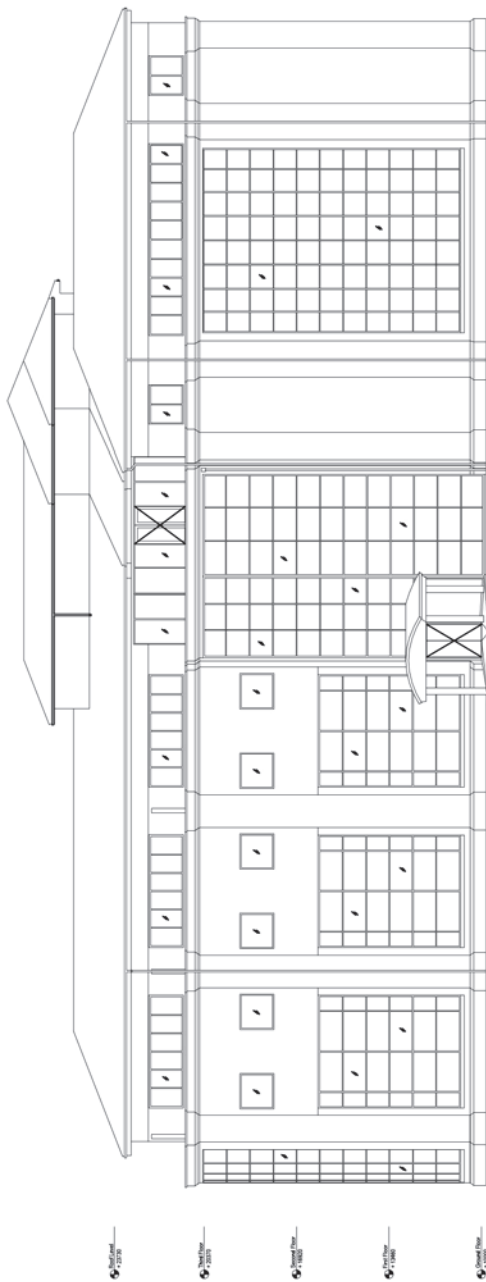


NO.	DATE	DESCRIPTION	BY	CHECKED
1	10-1-13	Approved Final Issue	PO	
2	28-10-14	Issued with subject to new details	PO	
Planning Date back to the work file				

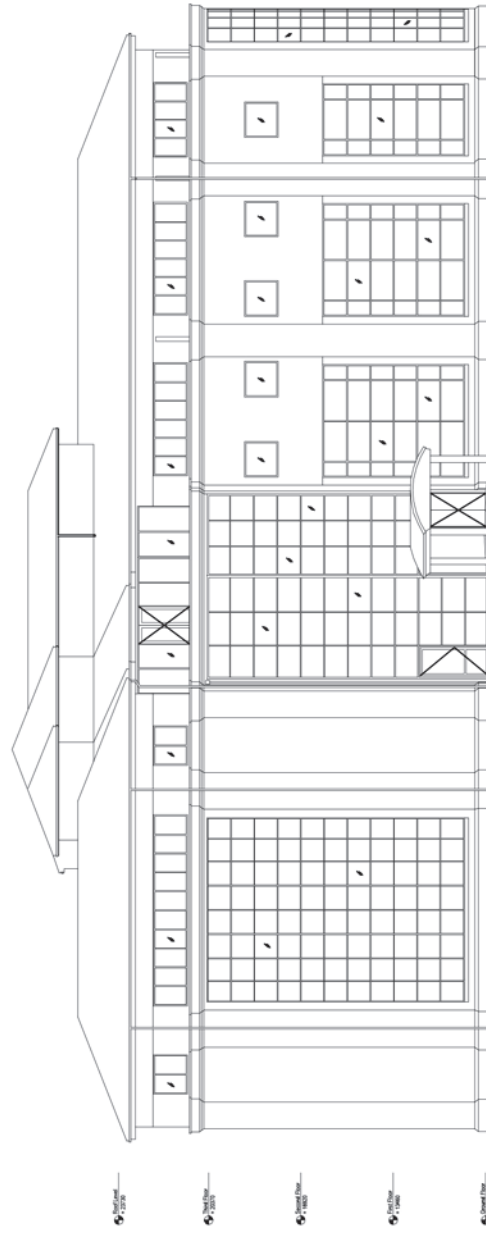
Highbury Top Hotel
 1 Nobel Drive Harington
 Proposed Roof Plan

NO.	DATE	DESCRIPTION	BY	CHECKED
1	10-1-13	Approved Final Issue	PO	
2	28-10-14	Issued with subject to new details	PO	

fb Architecture
 1 The Courtyard
 Highbury, London, N5 2RU
 Tel: 020 7434 2000
 Email: info@fb-arch.co.uk
 www.fb-arch.co.uk



EAST ELEVATION



SOUTH ELEVATION

- 12/20
- 12/20
- 12/20
- 12/20
- 12/20

- 12/20
- 12/20
- 12/20
- 12/20
- 12/20

rev.	date	revision notes	design change	checked	drawing status	Reds Ltd. Architects	Check all dimensions and verify on site. Report any errors or omissions			
					Survey	first issued	drawn by	dwg sheet size	scale	checked
					Existing South and East Elevations	job code	PG	A3	1:200	revision
						1242-14	dwg no. prefix	drawing number		-
							-	08		

Heathrow Top
1 Nobel Drive, Harlington



fb Architecture
The Old Telephone Exchange
Cripps Lane, Basal Common
Coveley CV7 7FW
Telephone 01676 535 530
e-mail info@fbarch.co.uk
web www.fbarch.co.uk

East Elevation
South Elevation
Key Plan

Notes:

- 1 Roof: Flat roof set behind parapet upstand. Single ply membrane on insulated deck on structural deck
- 2 Fascias-projecting tapering fascia sections in perforated powdercoat aluminium-colour white
- 3 Walls- white- through coloured, insulated smooth-render panel system to walls generally
- 4 Top 2 floors of rear block- Grey coloured, metal faced rainscreen cladding panels,vertical joints
- 5 Fenestration- Bronze anodised Aluminium curtain glazing system,projecting vertical fins to selected glazing bars. Clear solar treated glass. Acoustic secondary glazing system
- 6 Feature Entrance canopies- grey colour powder coated aluminium fascias. flat white soffits containing structural frame.
- 7 Columns- circular section, perforated pc aluminium cladding into steel column sections
- 8 External Balcony balustrading- light grey lint, toughened glass infill, stainless steel handrails and support
- 9 Roof plant enclosures- Grey colour pc aluminium horizontal louvered screen with steel noise attenuating screen wall behind. To M+E recommendations



East Elevation

Revision to Nabal Drive Boundary Treatment:
 Revision to wall with aluminium fins should still be implemented in line with our park. Our new isolated format with access deck above car spaces to be implemented and including a landscape treatment.
 To be used for occasional seating - accessed from the Function suite. Chaired balustrade to edge of deck.



South Elevation

DATE	DESCRIPTION	BY	CHECKED
15/05/14	Final Elevation	...	...

1. To be used for occasional seating - accessed from the Function suite. Chaired balustrade to edge of deck.
 2. To be used for occasional seating - accessed from the Function suite. Chaired balustrade to edge of deck.

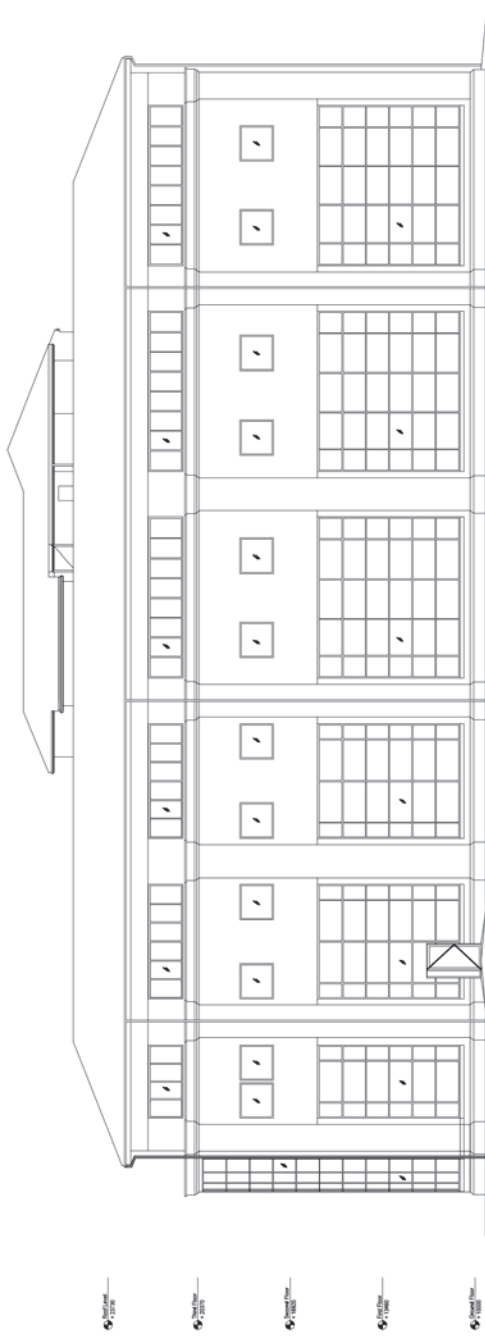
Planning
 Heathrow Top Hotel
 1 Nabal Drive, Hatfield
 Hertfordshire, AL10 1AA

South and East Elevations

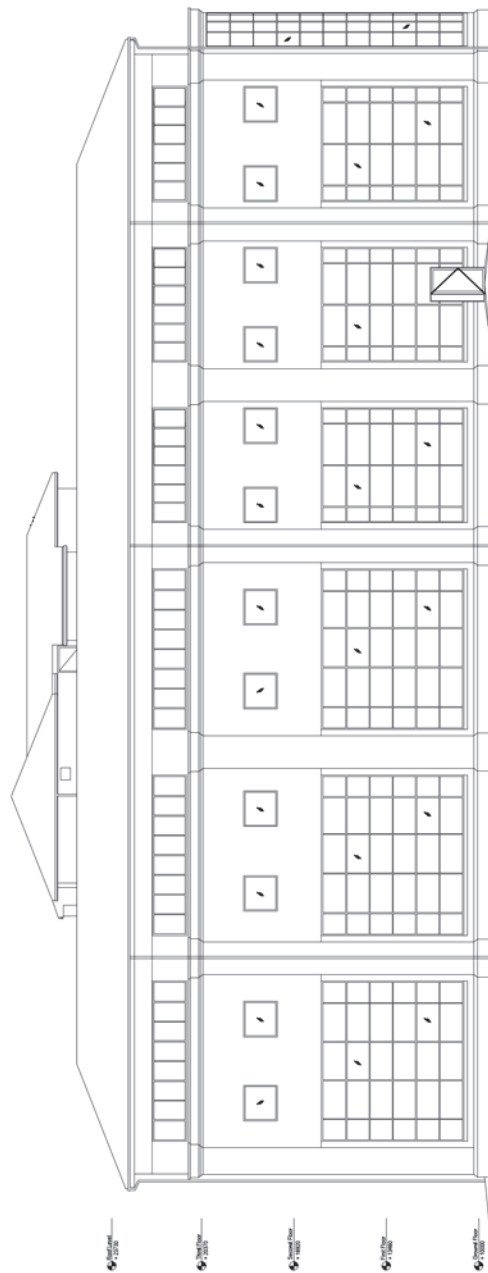
Client	Project Name	Project No.	Scale
...	1:200

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 11 The Courtyard
 Hatfield, AL10 1AA
 01454 610000
 www.fb-architects.com





⤴ NORTH ELEVATION

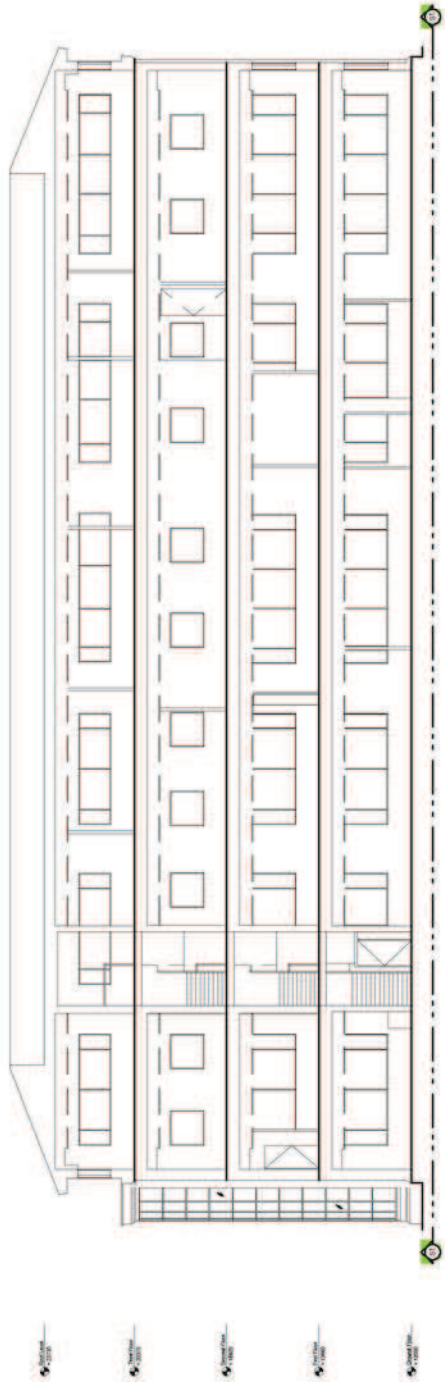


⤴ WEST ELEVATION

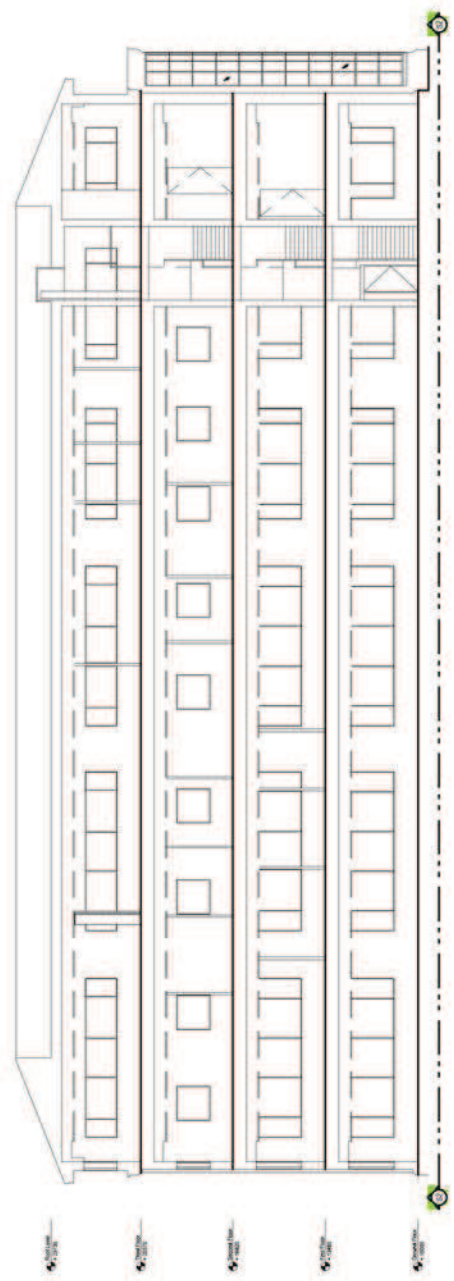
rev. / date revisions	revision notes	design change	checked	drawing status	Reds Ltd. Architects	Check all dimensions and verify on site. Report any errors or omissions	drawn by PG	dwg sheet size A3	scale 1:200	checked revision
				Survey	first issued job code 1242-14		dwg no. prefix -	drawing number 09		-
				Existing North & West Elevations						
				Heathrow Top 1 Nobel Drive, Harlington						



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 The Old Telephone Exchange
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 Coventry CV7 7FW
 Telephone 01676 555 530
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 web www.fbarch.co.uk



SECTION A-A



SECTION B-B

rev.	date	revision notes	design change	checked	drawing status	Scale	job code	job issued	job code	drawing number	scale	checked
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<p>Heathrow Top 1 Nobel Drive, Harlington</p> <p>Existing Sections A-A & B-B</p> <p>drawing title</p>												
<p>Check all dimensions and verify on site. Report any errors or omissions.</p>										<p>drawn by PG</p>		
										<p>drawing number 10</p>		
										<p>revision -</p>		



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 The Old Telephone Exchange
 Clipsy Lane, Basingstoke
 Hampshire RG24 0JY
 01635 535 530
 info@fbarch.co.uk
 www.fbarch.co.uk



Key Plan



Section A-A

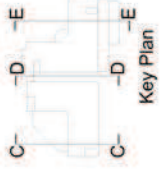


Section B-B

1:200

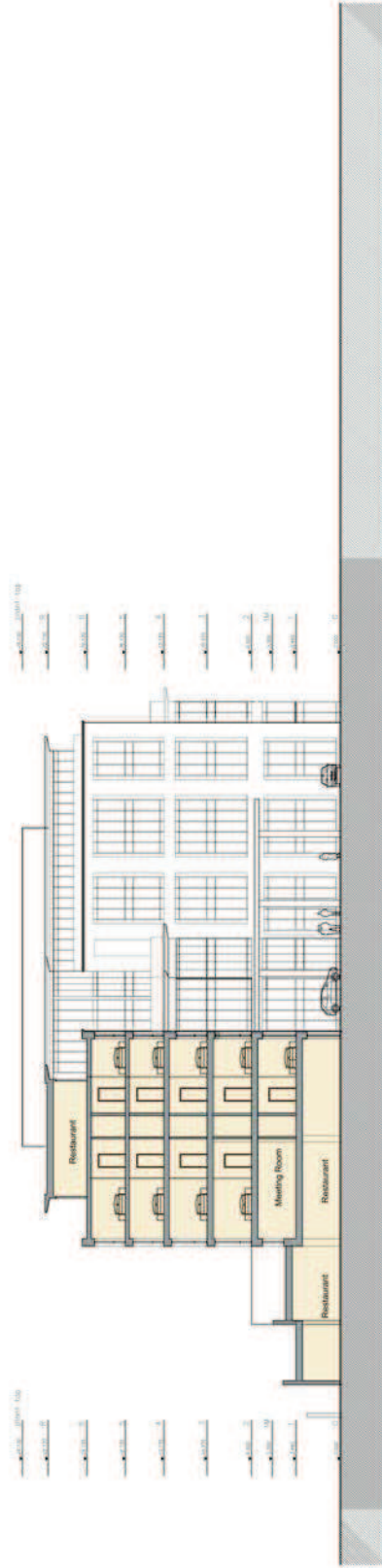
Project Name	Heathrow Top Hotel
Client	1 Nobel Drive, Harlington
Phase	Planning
Scale	1:200
Date	June 2014
Author	fb
Checker	fb
Project Manager	fb
Architect	fb

fb Architecture
 1 The Courtyard, Harlington, Middlesex, HA8 7JF
 0208 861 1111
 www.fb-architects.co.uk



Section D-D

Section C-C



Section E-E



Project Name	1 Nobol Drive Harington
Client	1 Nobol Drive Harington
Architect	fb Architecture
Scale	1:200
Date	June 2014
Sheet No.	1000-14
Revision	1

fb Architecture
11777-11778
11779-11780
11781-11782
11783-11784
11785-11786
11787-11788
11789-11790
11791-11792
11793-11794
11795-11796
11797-11798
11799-11800

Resin/fibreglass troughs filled with wind & drought resistant plants

parapet glazed balustrading



Composite decking, eg Enhanced golden grain by Millboard

Composite decking, eg Enhanced golden grain by Millboard

Female wc

Duct

Composite decking, eg Enhanced golden grain by Millboard

ANNE EVANS Director OBE
CHARTERED LANDSCAPE ARCHITECT
WATNY ELMS
THE GREEN
SHILSTONE
COLESHILL
BRIMMINGHAM B46 8AN
Tel: 01675 487118
Mobile: 07973 671802
anne@annesevans.co.uk
www.annesevans.co.uk

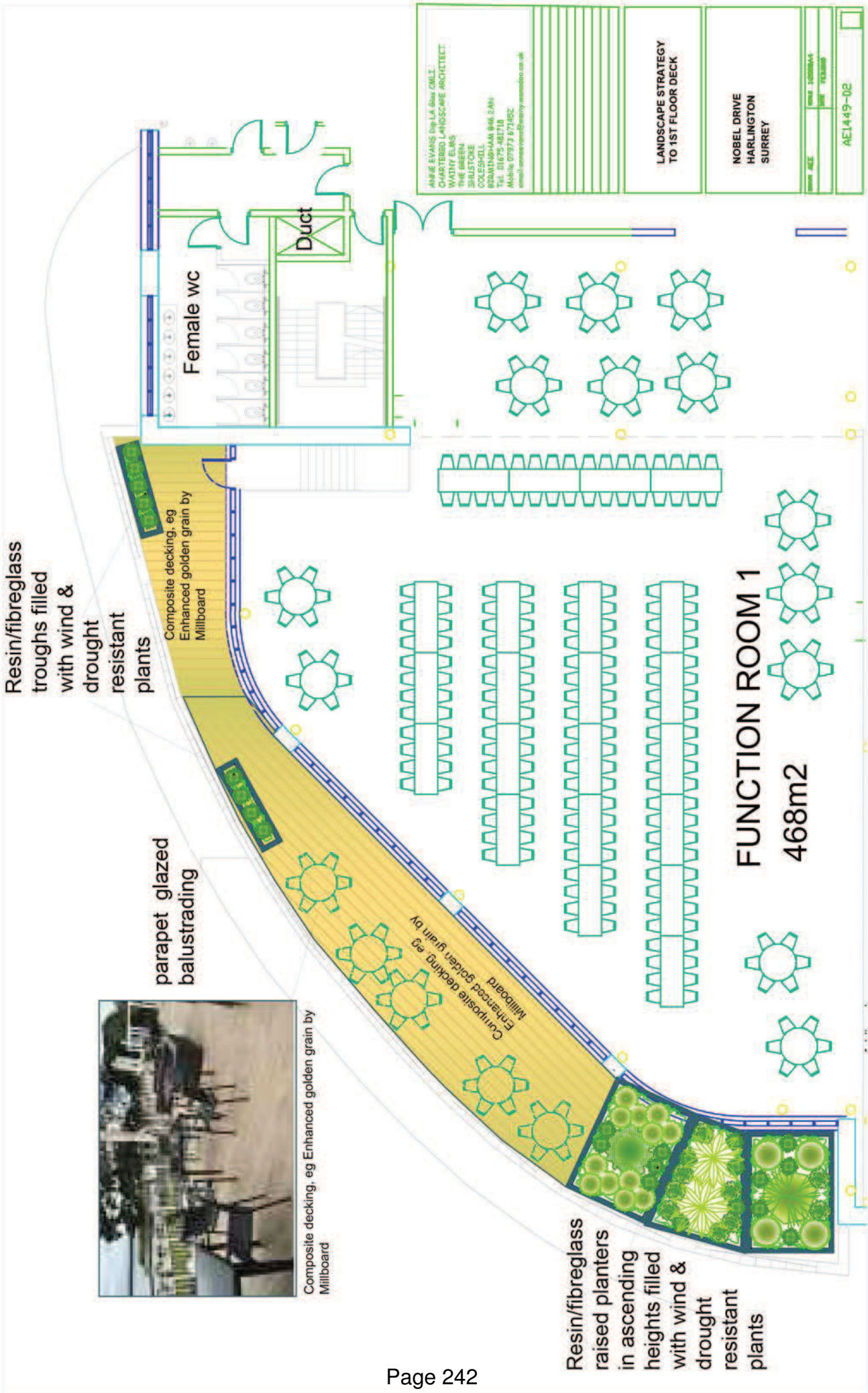
LANDSCAPE STRATEGY
TO 1ST FLOOR DECK

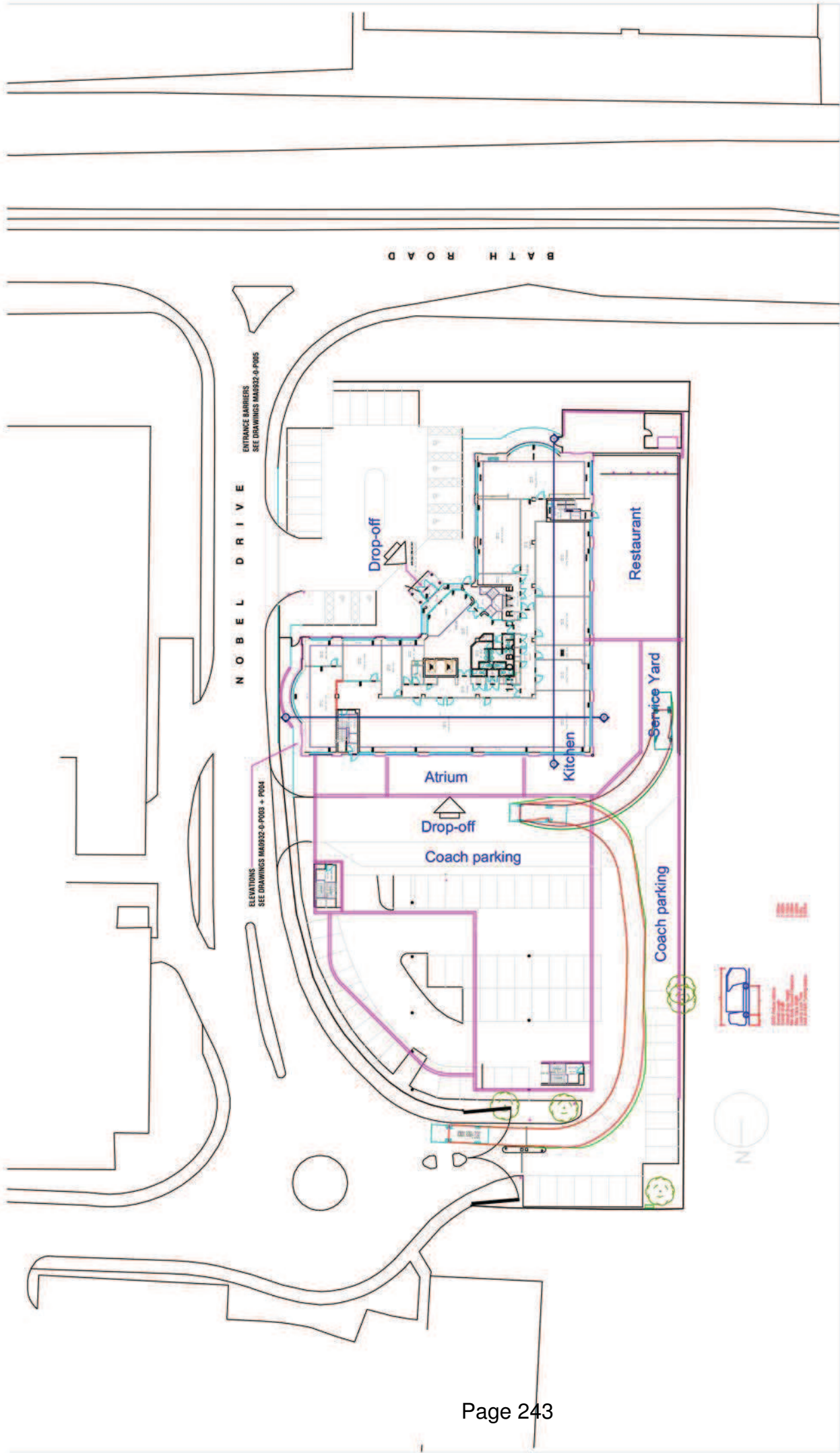
NOBEL DRIVE
HARLINGTON
SURREY

AE1449-02

FUNCTION ROOM 1
468m²

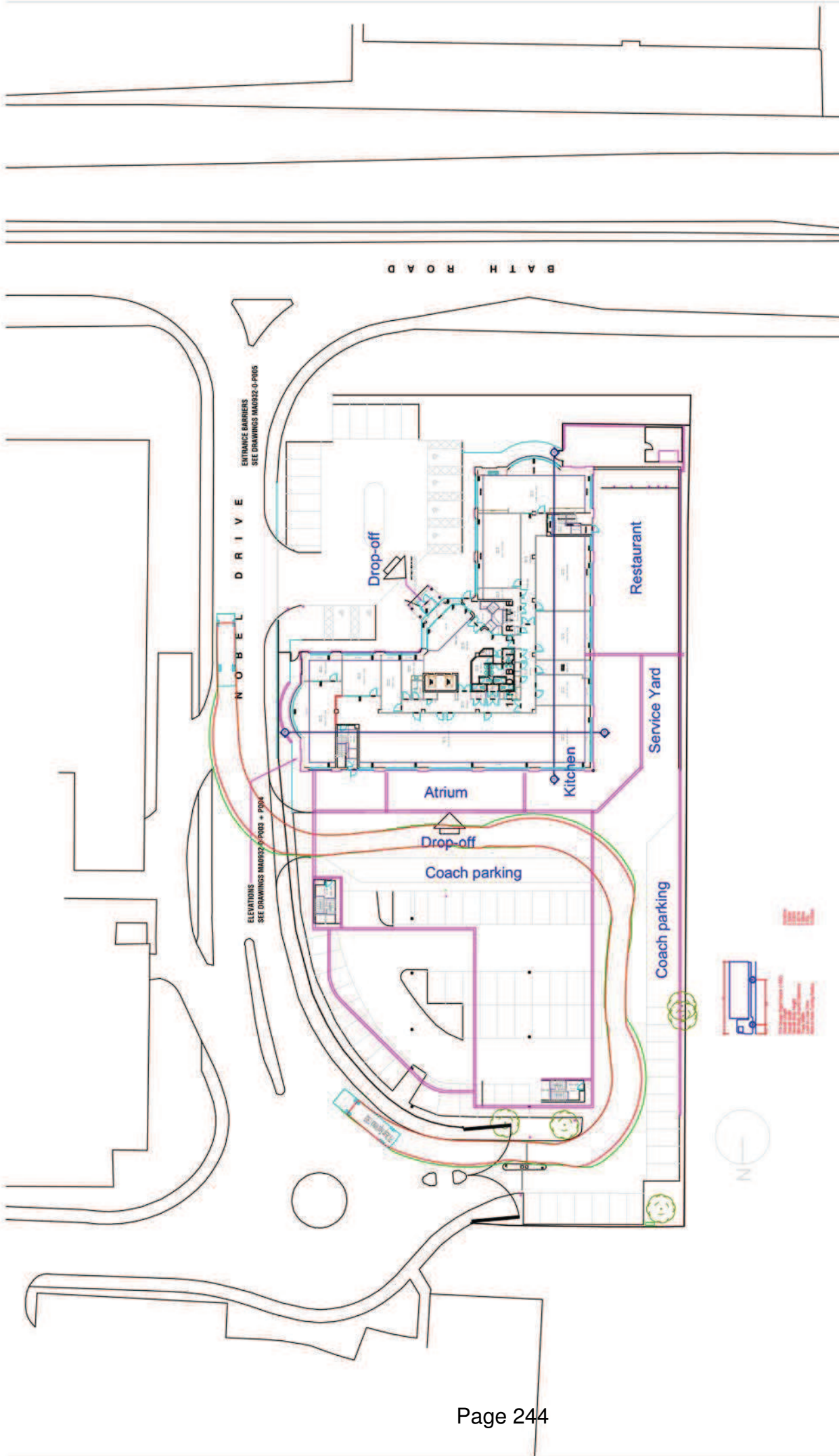
Resin/fibreglass raised planters in ascending heights filled with wind & drought resistant plants





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					Preliminary	first issued		dwg no. prefix -	drawing number 01		
					Site Layout Plan	job code 1242-14					

Heathrow Top
1 Nobel Drive, Harlington

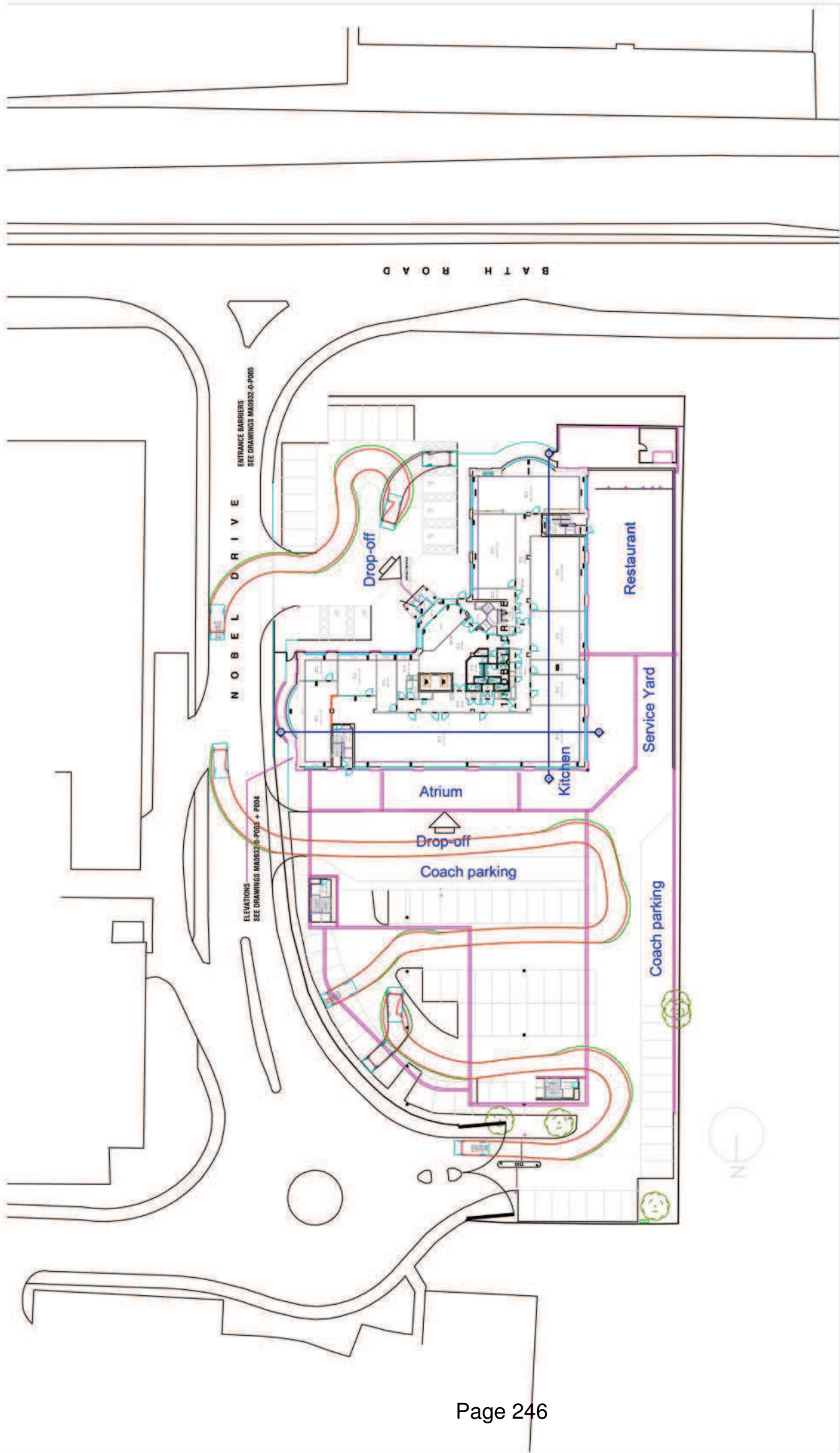


rev date revisions	revision notes	design change	checked	drawing status	Architects ©	Check all dimensions and verify on site. Report any errors or omissions	
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						dwg no. prefix -	drawing number 01
							revision -
				Preliminary			
				Site Layout Plan			

Heathrow Top
1 Nobel Drive, Harlington



fb Architecture
The Old Telephone Exchange
Gypsy Lane, Basildon Common
Basildon, Essex, SSO 9JH
Telephone: 01706 535 530
e-mail: info@fbarch.co.uk
www.fbarch.co.uk



ENTRANCE BARRIERS
SEE DRAWINGS MAD032-0-P005

ELEVATIONS
SEE DRAWINGS MAD032-0-P005 + P004

BATH ROAD

NOBEL DRIVE

Drop-off

Atrium

Drop-off

Coach parking

Kitchen

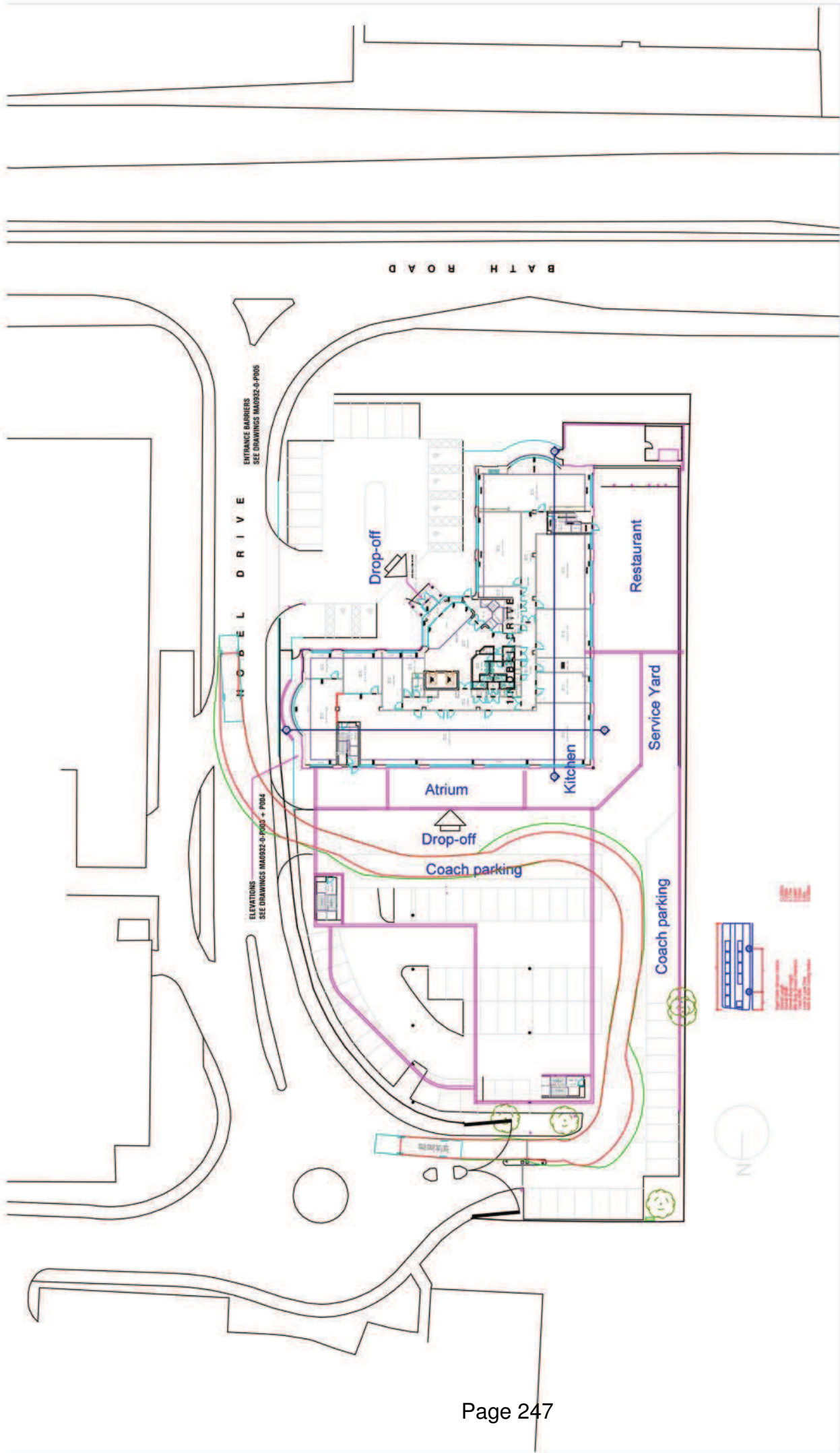
Restaurant

Service Yard

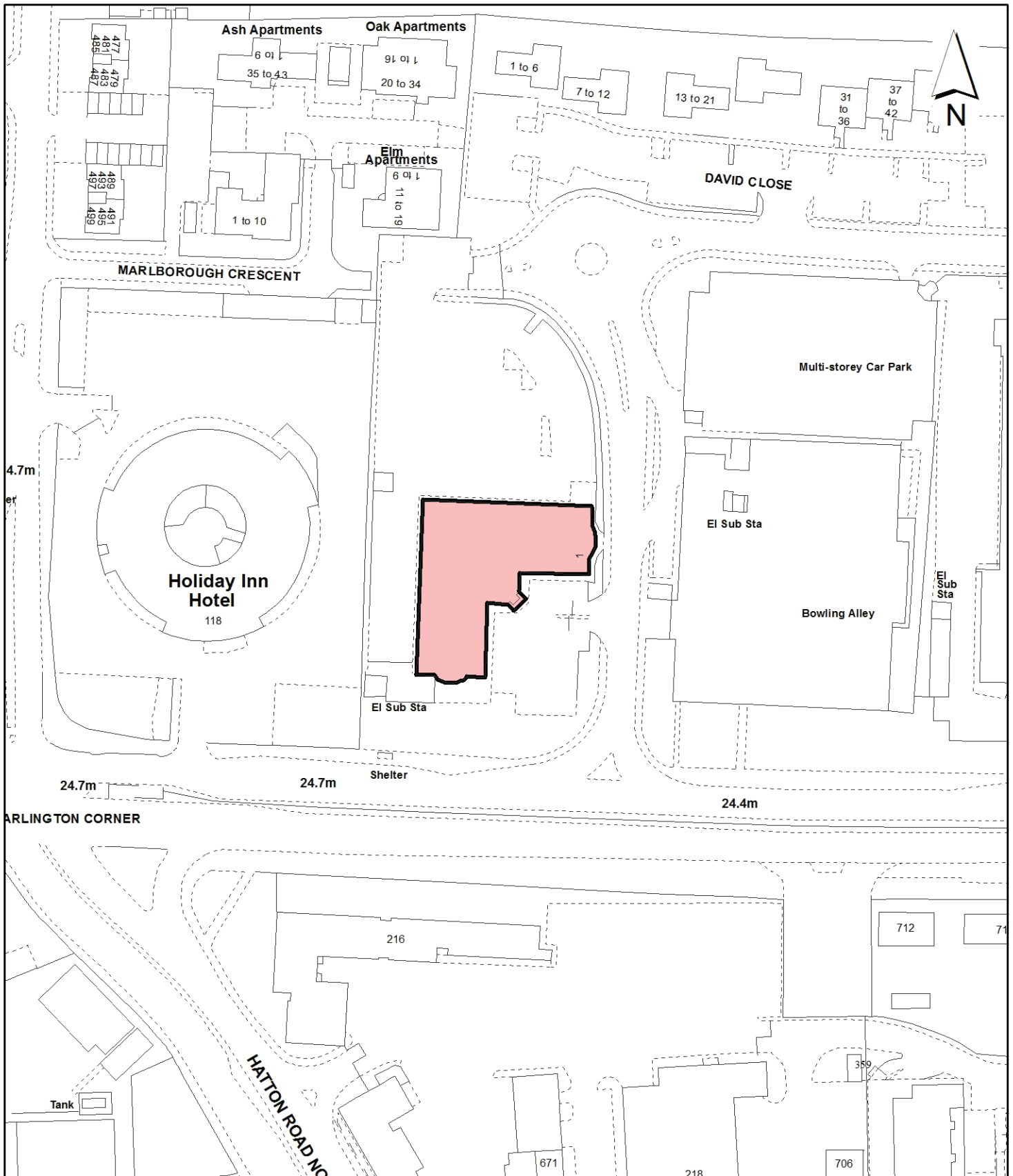
Coach parking



rev | date revisions	revision notes	design change	checked	drawing status	focta Ltd. Architects ©	Check all dimensions and verify on site. Report any errors or omissions	
Heathrow Top 1 Nobel Drive, Harlington		Preliminary		first issued	job code 1242-14	drawn by PG	Check all dimensions and verify on site. Report any errors or omissions
Site Layout Plan		drawing title		job code 1242-14	drawn by PG	drawn sheet size A3	checked
				job code 1242-14	drawn no. prefix -	drawing number 01	revision
				job code 1242-14	drawn no. prefix -	drawing number 01	revision
				job code 1242-14	drawn no. prefix -	drawing number 01	revision



rev | date revisions	revision notes	design change	checked	drawing status	Architects ©	Check all dimensions and verify on site. Report any errors or omissions	
Heathrow Top 1 Nobel Drive, Harlington		Preliminary		first issued	job code 1242-14	drawn by PG	drawing number 01
job title		drawing title Site Layout Plan		drawn by PG	draw sheet size A3	checked	revision
				scale 1:500	drawing number 01	revision	-



Notes

 Site boundary

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Site Address

**1 Nobel Drive
 Harlington**

**LONDON BOROUGH
 OF HILLINGDON**

**Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 250111

Planning Application Ref:

46214/APP/2014/2827

Scale

1:1,250

Planning Committee

Major Page 248

Date

March 2015



HILLINGDON
 LONDON

Report of the Head of Planning, Sport and Green Spaces

Address WEST DRAYTON GARDEN VILLAGE PORTERS WAY WEST DRAYTON

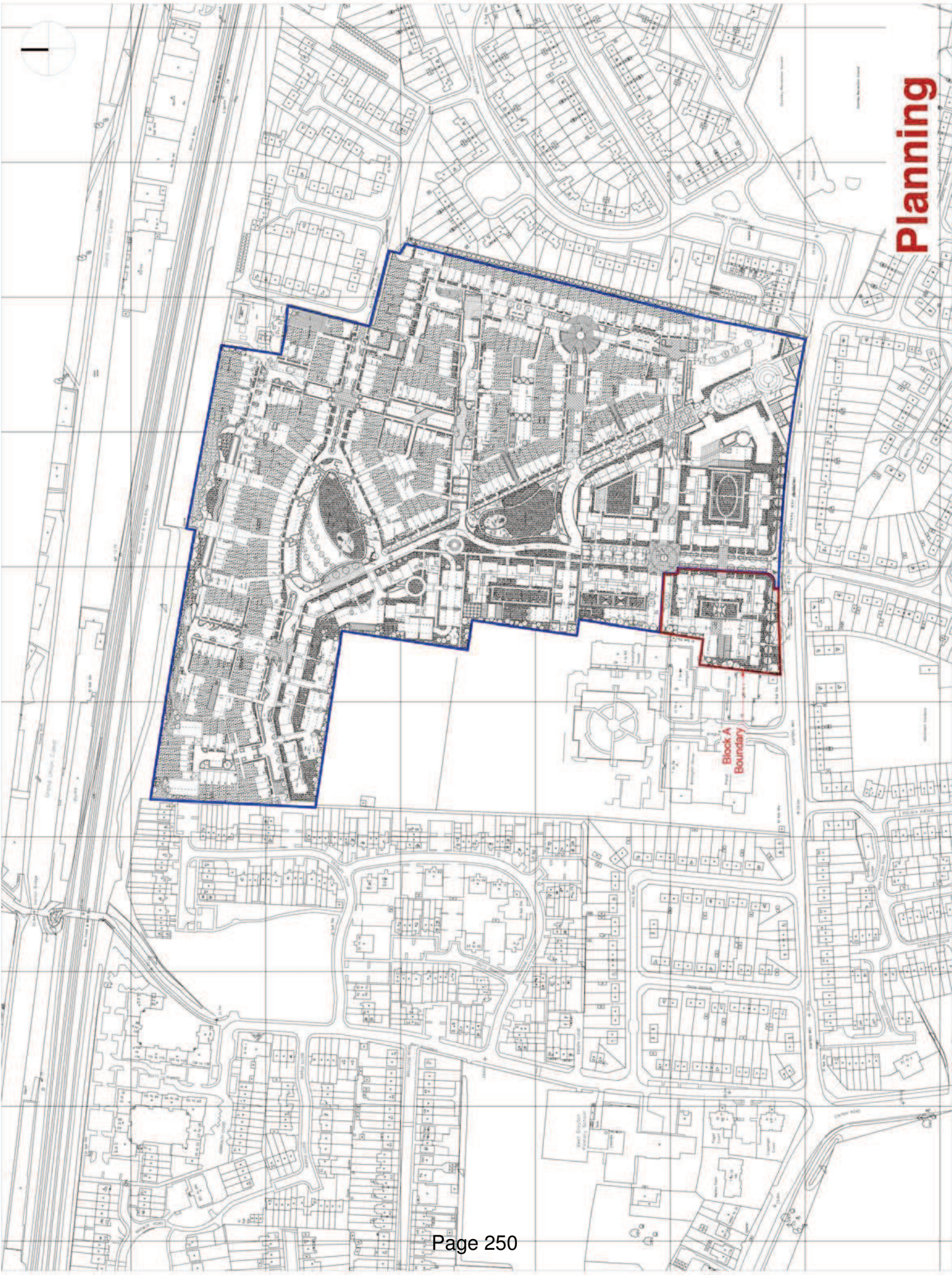
Development: Reserved matters (appearance and landscaping) in compliance with condition 2 and 3 for Phase 5 (Block A) (82 residential units) of planning permission ref: 5107/APP/2009/2348, dated 01/10/2010, for the proposed mixed used redevelopment of the Former NATS Site.

LBH Ref Nos: 5107/APP/2014/4304

Date Plans Received: 08/12/2014

Date(s) of Amendment(s):

Date Application Valid: 11/12/2014



Planning

Key:

Private Residential

Block paving (Subject to drainage strategy)

Tarmac to Car Park (Subject to drainage strategy)

El Sub Sta

BOULEVARD

BLOCK E

New raised table in accordance with Engineers details

Landscaping is indicative. Refer to Landscape Consultant drawings for details

Planning



- Revisions:
- 00 07 August 14
 - 01 19 August 14
 - 02 24 September 14
 - 03 01 October 14
 - 04 08 October 14
 - 05 23 October 14
 - 06 30 October 14
 - 07 17 November 14
 - 08 25 November 14
 - 09 12 December 14
 - 10 16 February 15

Drayton Garden
Village, Block A

for

Inland Homes



1363-D2101-rev10

Scale: 1:200 @ A1

Date: 05 August 2014

First floor plan:
Proposed

Key:

Private Residential



Landscaping is indicative. Refer to Landscape Consultant drawings for details
Planning

Key:

Private Residential



Landscaping is indicative. Refer to Landscape Consultant drawings for details
Planning

New raised table in accordance with Engineers details

- Revisions:
- 00 07 August 14
 - 01 19 August 14
 - 02 24 September 14
 - 03 01 October 14
 - 04 05 October 14
 - 05 29 October 14
 - 06 30 October 14
 - 07 17 November 14
 - 08 25 November 14
 - 09 12 December 14
 - 10 16 February 15

Drayton Garden
Village, Block A

for

Inland Homes



1363-D2103-rev10

Scale: 1:200 @ A1

Date: 05 August 2014

Third floor plan:
Proposed

Key:

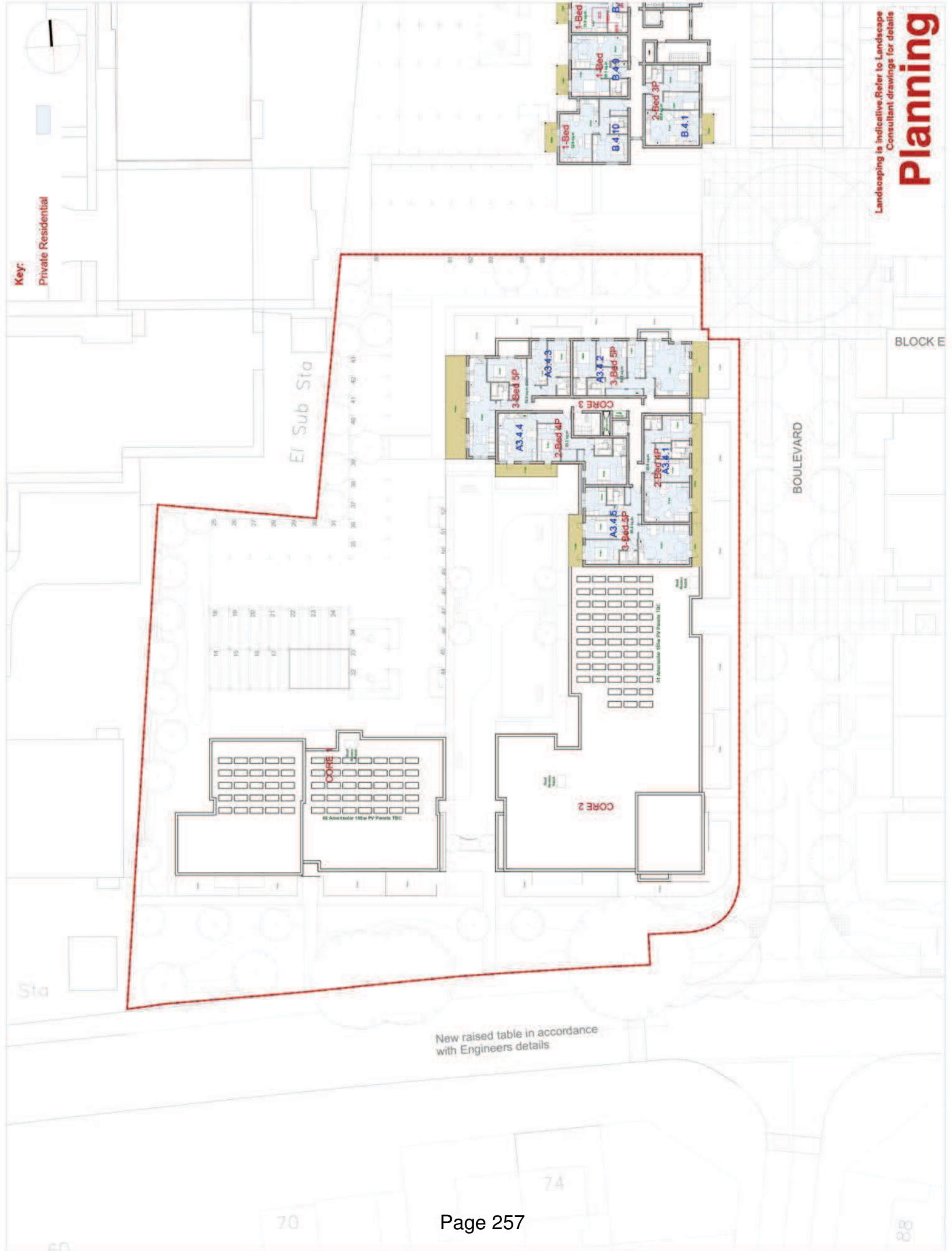
Private Residential



Landscaping is indicative. Refer to Landscaping Consultant drawings for details
Planning

Key:

Private Residential



Landscaping is indicative. Refer to Landscape
 Consultant drawings for details
Planning

New raised table in accordance
 with Engineers details

- Revisions:
- 00 05 September 14
 - 01 14 September 14
 - 02 03 October 14
 - 03 29 October 14
 - 04 30 October 14
 - 05 07 November 14
 - 06 18 November 14
 - 07 19 November 14
 - 08 05 December 14



Drayton Garden
Village, Block A

for

Inland Homes



1.363-D2700-rev.08

Scale: 1:100 @ A1
Date: 04 September 2014

Elevation AA:
Proposed



ENTRANCE

Landscaping is indicative. Refer to
Landscape Consultant drawings for details

Planning

- Revisions:
- 00 05 September 14
 - 01 03 September 14
 - 02 03 October 14
 - 03 29 October 14
 - 04 30 October 14
 - 05 07 November 14
 - 06 12 November 14
 - 07 25 November 14



Drayton Garden
Village, Block A

for

Inland Homes



1363-D2701-rev07

Scale: 1:100 @ A1
Date: 04 September 2014

Elevation BB:
Proposed



Landscaping is indicative. Refer to
Landscape Consultant drawings for details

Planning

- Revisions:
- 00 05 September 14
 - 01 05 September 14
 - 02 03 October 14
 - 03 29 October 14
 - 04 30 October 14
 - 05 07 November 14
 - 06 18 November 14
 - 06 05 December 14



Drayton Garden
Village, Block A

for

Inland Homes



1363-D2702-rev08

Scale: 1:100 @ A1
Date: 04 September 2014

Elevation CC:
Proposed



Landscaping is indicative. Refer to
Landscape Consultant drawings for details

Planning

- Revisions:
- 00 05 September 14
 - 01 14 September 14
 - 02 03 October 14
 - 03 29 October 14
 - 04 30 October 14
 - 05 07 November 14
 - 06 18 November 14
 - 07 25 November 14



Drayton Garden
Village, Block A

for

Inland Homes



1363-D2703-rev07

Scale: 1:100 @ A1
Date: 04 September 2014

Elevation DD:
Proposed

ELEVATION BREAK



Landscaping is indicative. Refer to
Landscape Consultant drawings for details

Planning



Landscape is indicative. Refer to
Landscape Consultant drawings for details
Planning

- Revisions:
- 00 05 September 14
 - 01 14 September 14
 - 02 03 October 14
 - 03 29 October 14
 - 04 30 October 14
 - 05 07 November 14
 - 06 18 November 14
 - 07 25 November 14



Drayton Garden
Village, Block A

for

Inland Homes



1363-D2705-rev07

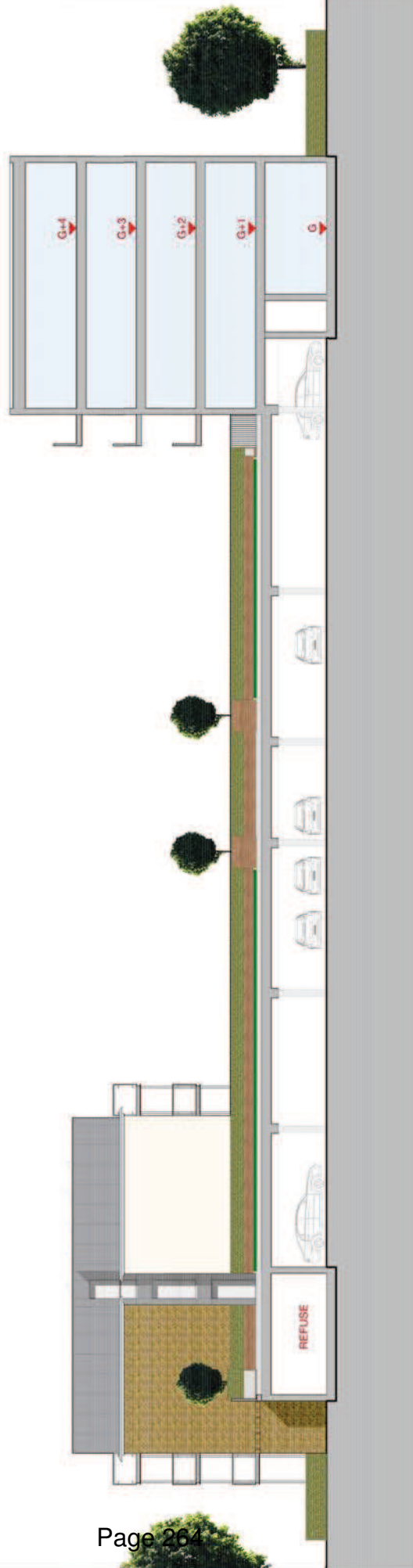
Scale: 1:100 @ A1
Date: 04 September 2014

Elevation FF:
Proposed



Landscaping is indicative. Refer to
Landscape Consultant drawings for details

Planning



Landscaping is indicative. Refer to
Landscape Consultant drawings for details

Planning



PHASE BOUNDARY

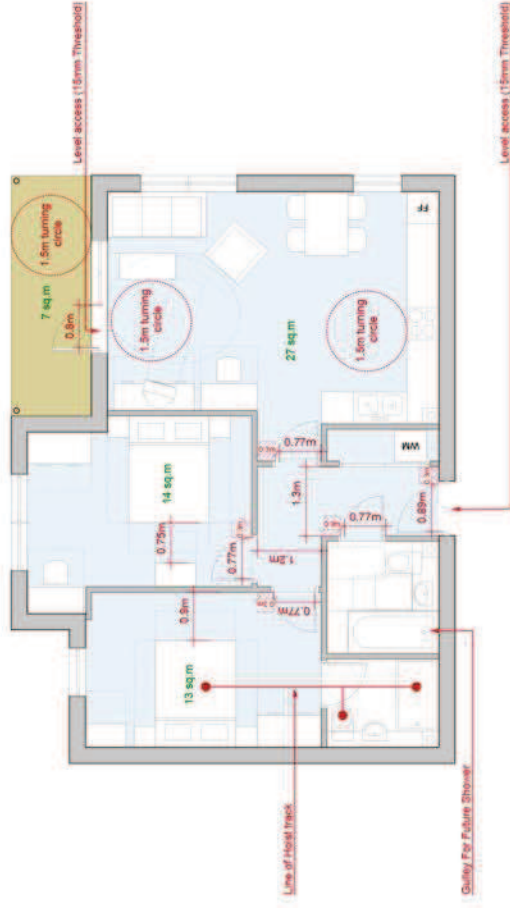
PHASE BOUNDARY



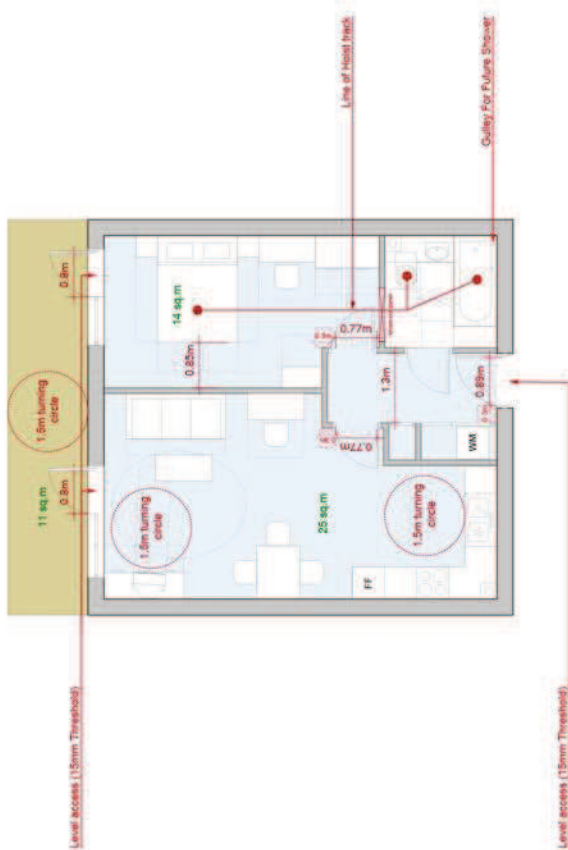
Landscaping is indicative. Refer to
 Landscape Consultant drawings for details

Planning

Typical 2 Bed Flat 70.6 sq.m



Typical 1-Bed Flat 50.0 sq.m



The Lifetime Home Standards

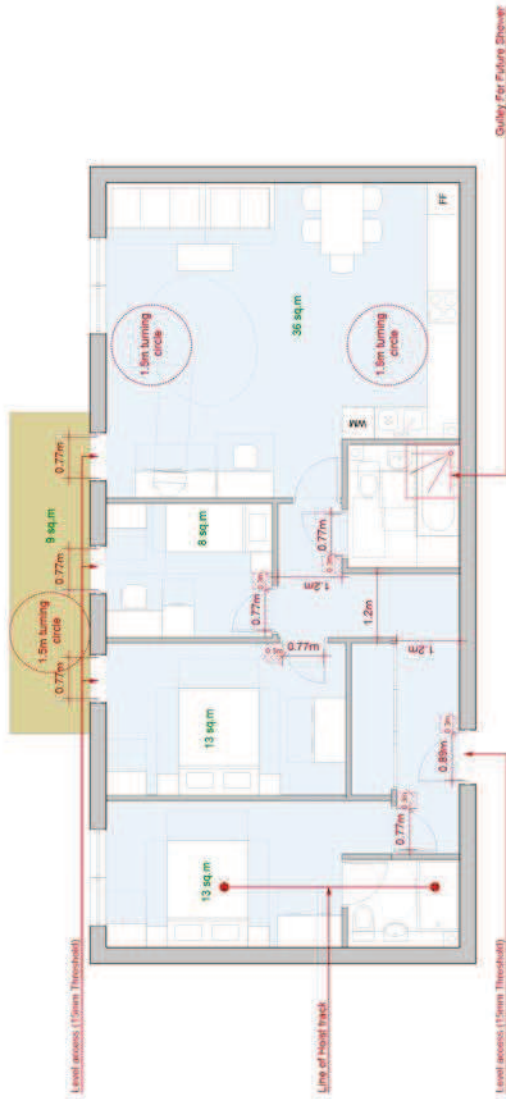
- 1. Car Parking** - Where parking is provided by communal or shared bays, at least one parking space 3300mm wide x 4800mm deep should be provided adjacent to (or close to) each block's entrance or lift core.
The access route between the parking and communal entrance (or in the case of basement parking, the lift core) should maintain a minimum clear width of 1200mm.
- 2. Access from Car Parking** - The distance from the car parking space to the dwelling entrance (or relevant block entrance or lift core), should be kept to a minimum and be level or gently sloping. The distance from visitors parking to relevant entrances should be as short as practicable and be level or gently sloping.
- 3. Approach** - The approach to all entrances should be level or gently sloping.
- 4. External Entrances** - All entrances should be illuminated and have level access over the approach. They should have effective weather protection as follows. There should be 300mm clear opening above the door, the side of the door on the level. For communal doors - When the approach is head on or at right angles to an access route at least 1500mm wide, the opening should have a clear width of 800mm. When the approach is at right angles to an access route at least 1200mm wide, the opening should have a clear width of 825mm.
Main entrances should also have adequate weather protection and have a level external landing.
- 5. Communal Stairs** - Communal stairs should provide easy access and, where homes are reached by a lift, it should be fully accessible.
- 6. Internal Doorways & Hallways** - Movement in hallways and through doorways should be as convenient to the widest range of people, including those using mobility aids or wheelchairs, and those moving furniture or other objects. As a general principle, narrower hallways and landings will need wider doorways in their side walls.
For internal dwelling doors - When the approach is head on or at right angles to an access route at least 1200mm wide, the opening should have a clear width of 750mm. When the approach is at right angles to an access route at least 1050mm wide, the opening should have a clear width of 775mm. When the approach is at right angles to an access route less than 1050mm wide, the opening should have a clear width of 900mm.

- 7. Wheelchair Accessibility** - There should be space for turning a wheelchair in dining areas and living rooms and adequate circulation space for wheelchairs elsewhere.
- 8. Living Rooms** - The living room should be at entrance level of every dwelling.
- 9. Two or more storey requirements** - In dwellings with two or more storeys, with no permanent bedroom on the entrance level, there should be space on the entrance level that could be used as a convenient temporary sleep space.
- 10. WC** - In dwellings with two or more storeys, and no more than two habitable rooms in addition to the main living room and any kitchen diner, the entrance level should have an accessible WC compartment with potential for a shower to be installed. If an accessible bathroom is not provided on that level, in houses with two bedrooms the downstairs WC should at least conform to Part M.
- 11. Bathroom & WC Walls** - Walls in all bathrooms and WC compartments should be capable of firm fixing and support for adaptations such as grab rails.
- 12. Stairs and potential through-lift in dwellings** - The design within a dwelling of two or more storeys should incorporate potential for stair lift installation and a suitable identified space for a through-be-floor lift from the entrance level to a storey containing a main bedroom and a bathroom satisfying criterion 14.
- 13. Main Bedroom** - Structure above a main bedroom and bathroom ceilings should be capable of supporting ceiling hoists and the design should provide a reasonable route between this bedroom and the bathroom.
- 14. Bathroom Layout** - An accessible bathroom, providing ease of access in accordance with the specification below, should be provided in every dwelling on the same storey as a main bedroom.
- 15. Window Specification** - Windows in the principal living space (typically the living room), should allow people to see out when seated. In addition, at least one opening light in each habitable room should be approachable and usable by a wide range of people including those with restricted movement and reach.
Living room window glazing should begin no higher than 800mm from the floor level and windows should be easy to open/close.
- 16. Fixtures & Fittings** - Location of service controls Service controls should be within a height band of 450mm to 1200mm from the floor and at least 300mm away from any internal room corner.

SEE DRAWING	YES	N/A	N/A	YES (stud walls will include plywood sublayer)	N/A	SEE DRAWING	SEE DRAWING	YES	YES
-------------	-----	-----	-----	--	-----	-------------	-------------	-----	-----

Planning

Typical 3 Bed Flat 94.5 sq.m



The Lifetime Home Standards

- 1. Car Parking** - Where parking is provided by communal or shared bays, at least one parking space 3300mm wide x 4800mm deep should be provided adjacent to (or close to) each block's entrance or lift core.
The access route between the parking and communal entrance (or in the case of basement parking, the lift core) should maintain a minimum clear width of 1200mm.
- 2. Access from Car Parking** - The distance from the car parking space to the dwelling entrance (or relevant block entrance or lift core), should be kept to a minimum and be level or gently sloping. The distance from visitors parking to relevant entrances should be as short as practicable and be level or gently sloping.
- 3. Approach** - The approach to all entrances should be level or gently sloping.
- 4. External Entrances** - All entrances should be illuminated and have level access over the step. They should have effective openings, as follows. There should be 300mm clear width to the side of the opening, as follows:
External Doors - When the approach is head on or at right angles to an access route at least 1500mm wide, the opening should have a clear width of 800mm. When the approach is at right angles to an access route at least 1200mm wide, the opening should have a clear width of 825mm.
Main entrances should also have adequate weather protection and have a level external landing.
- 5. Communal Stairs** - Communal stairs should provide easy access and, where homes are reached by a lift, it should be fully accessible.
- 6. Internal Doorways & Hallways** - Movement in hallways and through doorways should be as convenient to the widest range of people, including those using mobility aids or wheelchairs, and those moving furniture or other objects. As a general principle, narrower hallways and landings will need wider doorways in their side walls.
For internal dwelling doors - When the approach is head on or at right angles to an access route at least 1200mm wide, the opening should have a clear width of 750mm. When the approach is at right angles to an access route at least 1050mm wide, the opening should have a clear width of 775mm. When the approach is at right angles to an access route less than 1050mm wide, the opening should have a clear width of 900mm.

SEE DRAWING

YES

N/A

N/A

YES (stud walls will include plywood sublayer)

N/A

SEE DRAWING

SEE DRAWING

YES

YES

- 7. Wheelchair Accessibility** - There should be space for turning a wheelchair in dining areas and living rooms and adequate circulation space for wheelchairs elsewhere.
- 8. Living Rooms** - The living room should be at entrance level of every dwelling.
- 9. Two or more storey requirements** - In dwellings with two or more storeys, with no permanent bedroom on the entrance level, there should be space on the entrance level that could be used as a convenient temporary sleep space.
- 10. WC** - In dwellings with two or more storeys, and no more than two habitable rooms in addition to the main living room and any kitchen diner, the entrance level should have an accessible WC compartment with potential for a shower to be installed, if an accessible bathroom is not provided on that level. In houses with two bedrooms the downstairs WC should at least conform to Part M.
- 11. Bathroom & WC Walls** - Walls in all bathrooms and WC compartments should be capable of firm fixing and support for adaptations such as grab rails.
- 12. Stairs and potential through-floor lift in dwellings** - The design within a dwelling of two or more storeys should incorporate potential for stair lift installation and a suitable identified space for a through-floor lift from the entrance level to a storey containing a main bedroom and a bathroom satisfying criterion 14.
- 13. Main Bedroom** - Structure above a main bedroom and bathroom ceilings should be capable of supporting ceiling hoists and the design should provide a reasonable route between this bedroom and the bathroom.
- 14. Bathroom Layout** - An accessible bathroom, providing ease of access in accordance with the specification below, should be provided in every dwelling on the same storey as a main bedroom.
- 15. Window Specification** - Windows in the principal living space (typically the living room), should allow people to see out when seated. In addition, at least one opening light in each habitable room should be approachable and usable by a wide range of people including those with restricted movement and reach.
Living room window glazing should begin no higher than 800mm from the floor level and windows should be easy to open/close.
- 16. Fixtures & Fittings** - Location of service controls. Service controls should be within a height band of 450mm to 1200mm from the floor and at least 300mm away from any internal room corner.

YES

YES

YES

YES

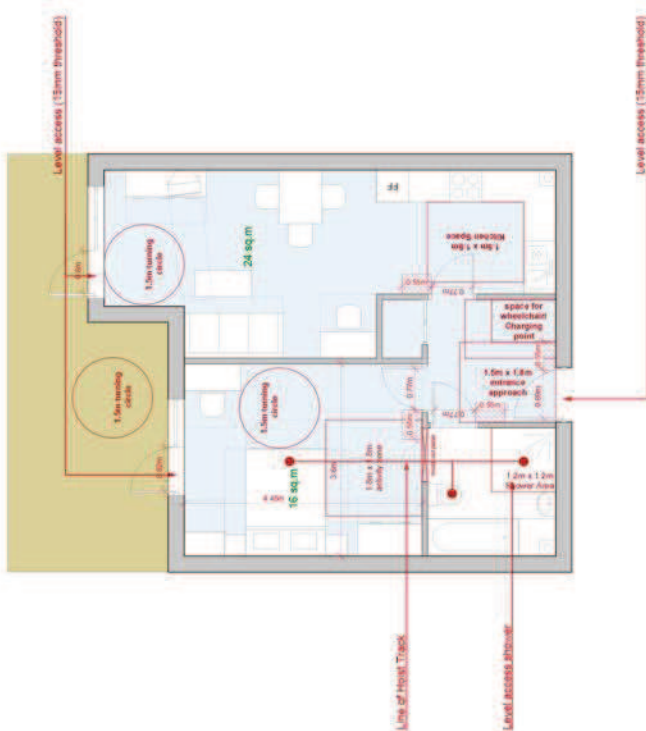
YES

SEE DRAWING

Typical 2 Bed Wheelchair Flat
78.2 sq.m



Typical 1 Bed Wheelchair Flat
54 sq.m



Planning

Typical 3 Bed Wheelchair Flat 107.9 sq.m





Elevation Detail B



Elevation Detail A

Detail Location Key



Elevation AA - Refer to drawing D2700 for full elevation

Material Key

- 1. Buff Stock Brickwork
- 2. White Through Coloured Render
- 3. Grey Render
- 4. Double Glazed Window
- 5. Double Glazed Door
- 6. Glazed Balustrade
- 7. Glazed Corner Balustrade
- 8. Grey Render
- 9. Painted Balcony Support Column
- 10. Buff Stock Brickwork Soldier Course
- 11. White Moulding Detail
- 12. Painted Metal Entrance Canopy
- 13. Painted Metal Roof Edge Detail
- 14. Painted Structural Column
- 15. PV Panels
- 16. Raised Flat Numbers
- 17. Louvered Bin Store Door

Planning

Revisions:

- 00 24 September 14
- 01 03 October 14
- 02 28 October 14
- 03 30 October 14
- 04 10 November 14
- 05 10 November 14
- 06 25 November 14

Drayton Garden
Village Block A

for

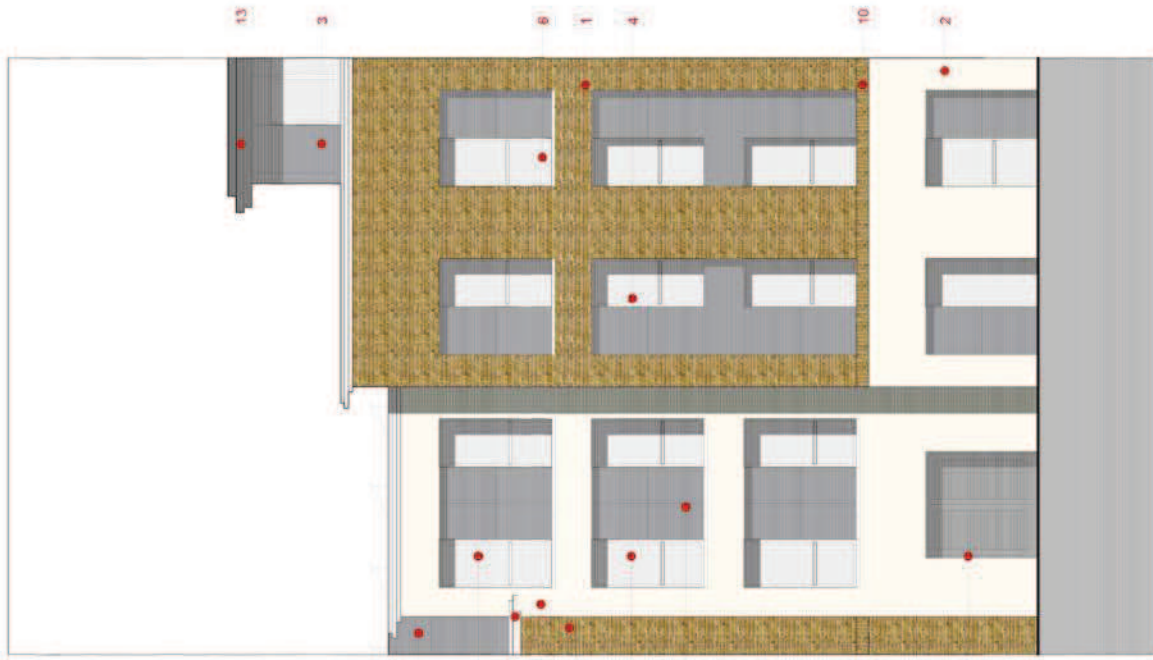


1363-D2751-rev06

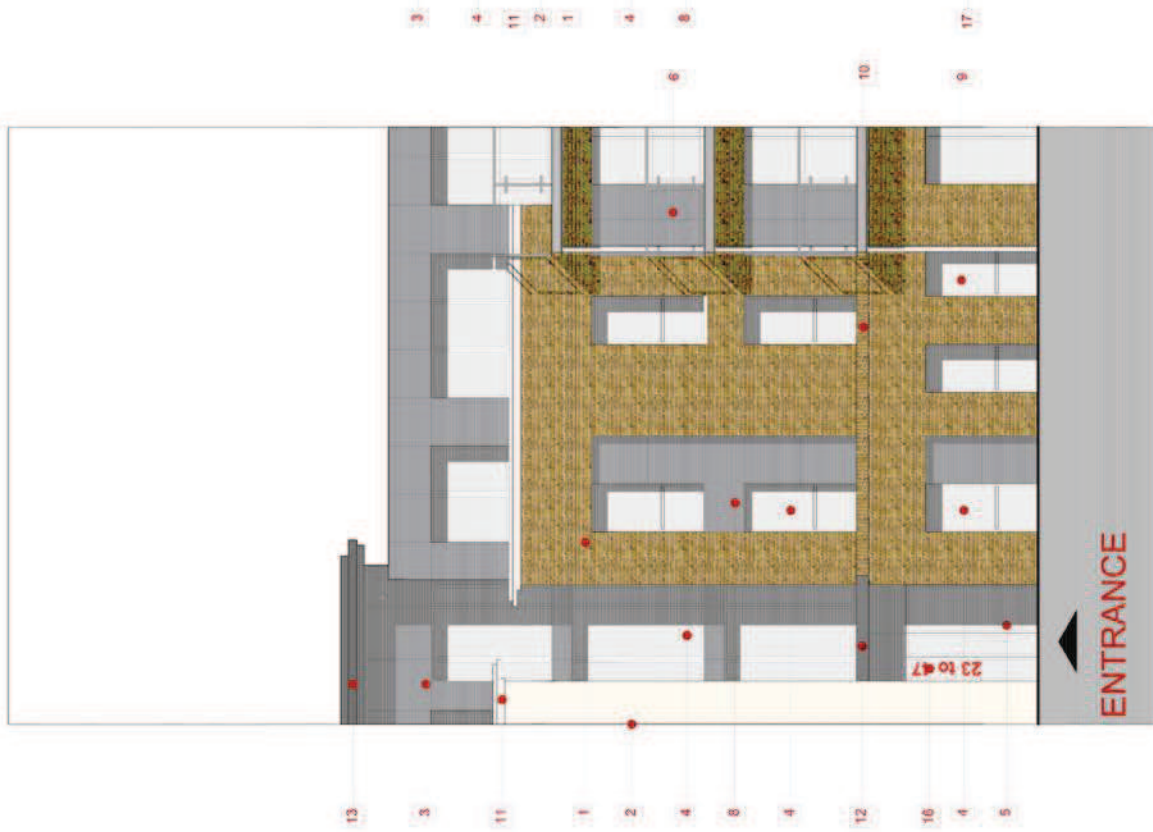
Scale: 1:50 @ A1
Date: 19 September 2014

Detail Elevations 2

Planning



Elevation Detail D



Elevation Detail C

Detail Location Key



Elevation BB - Refer to drawing D2701 for full elevation

- Material Key**
- 1. Buff Stock Brickwork
 - 2. White Through Coloured Render
 - 3. Grey Render
 - 4. Double Glazed Window
 - 5. Double Glazed Door
 - 6. Glazed Corner Balustrade
 - 7. Grey Render
 - 8. Painted Ralsonny Support Column
 - 10. Buff Stock Brickwork Soldier Course
 - 11. White Moulding Detail
 - 12. Painted Metal Entrance Canopy
 - 13. Painted Metal Roof Edge Detail
 - 14. Painted Structural Column
 - 15. PV Panels
 - 16. Raised Flat Numbers
 - 17. Louvered Bin Store Door

Revisions:

- 00 24 September 14
- 01 03 October 14
- 02 28 October 14
- 03 30 October 14
- 04 17 November 14
- 05 10 November 14
- 06 25 November 14

Drayton Garden
Village Block A

for



1363-D2752-rev06

Scale: 1:50 @ A1
Date: 19 September 2014

Detail Elevations 3

Planning



Elevation Detail E

Detail Location Key



Elevation CC - Refer to drawing D2702 for full elevation

Material Key

- 1. Buff Stock Brickwork
- 2. White Through Coloured Render
- 3. Grey Render
- 4. Double Glazed Window
- 5. Double Glazed Door
- 6. Glazed Corner Balustrade
- 7. Grey Render
- 8. Painted Balcony Support Column
- 9. Buff Stock Brickwork Soldier Course
- 10. White Moulding Detail
- 11. Painted Metal Entrance Canopy
- 12. Painted Metal Roof Edge Detail
- 13. Painted Structural Column
- 14. PV Panels
- 15. Raised Flat Numbers
- 16. Louvered Bin Store Door

NOTES

1. Surface water drainage will employ SUDs devices such as trench soakaways, infiltration basins or porous roadways.
2. Chase testing indicates an infiltration rate of 14.10 l/m² (0.350 in/hr), this would equate to a trench of 1m width, 1m effective depth and 2.0m length for an average house area of 50 sq.m.
3. Information given for foul water pumping stations is for guidance only, final specification, depths, storage etc. are subject to the domestic engineering design for each Phase.
4. All surface water disposal to be undertaken by Phase Developer.

Block/No.	Counting Area (m ²)	Area (m ²)	Volume (m ³)
1	2845	221	0.001
2	1503	100	0.045
3	1834	147	0.059
4	1695	133	0.059
5	1700	133	0.059
6	2180	170	0.080
7	474	36	0.001
8	625	65	0.074
9	720	57	0.069
10	1133	87	0.046
11	1133	87	0.046
12	1520	118	0.080
13	662	77	0.085
14	713	55	0.084
15	1134	110	1.000
16	1134	110	1.000
17	816	88	1.000
18	699	68	1.000

KEY

- Existing surface water sewer
- Existing surface water sewer to be abandoned
- Division of surface water sewer
- Phase 1 boundary
- Phase 1b boundary
- Proposed foul storage
- Proposed foul pumping station
- Proposed foul rising main
- Numbered infiltration basins
- Trench soakaways
- Domestic harvesting tank
- Wetland areas fed by roof water of neighbouring properties
- Freshwater Floor Levels

No.	Date	Details	By	Checked
1	04.12.14	Issue 4 revised	SLW	SLW
2	05.12.14	Issue 5 & 6 typed updated Phase 5 content	SLW	SLW
3	10.01.15	Issue 7 typed updated Phase 5 content	SLW	SLW
4	25.03.15	Issue 8 typed updated Phase 5 content	SLW	SLW
5	25.03.15	Issue 9 typed updated Phase 5 content	SLW	SLW
6	11.02.15	Foul pumping notes revised, infiltration basins revised	JAM	JAM
7	11.02.15	Foul pumping notes revised, infiltration basins revised	SLW	SLW
8	10.02.15	Updated to latest AGD report, incorporating revised development priority	SLW	SLW
9	27.9.10	Party information added, issue 4 content	SLW	SLW
10	15.05.10	Party information added, issue 4 content	SLW	SLW
11	15.05.10	Party information added, issue 4 content	SLW	SLW
12	15.05.10	Party information added, issue 4 content	SLW	SLW
13	15.05.10	Party information added, issue 4 content	SLW	SLW
14	15.05.10	Party information added, issue 4 content	SLW	SLW
15	15.05.10	Party information added, issue 4 content	SLW	SLW
16	15.05.10	Party information added, issue 4 content	SLW	SLW
17	15.05.10	Party information added, issue 4 content	SLW	SLW
18	15.05.10	Party information added, issue 4 content	SLW	SLW

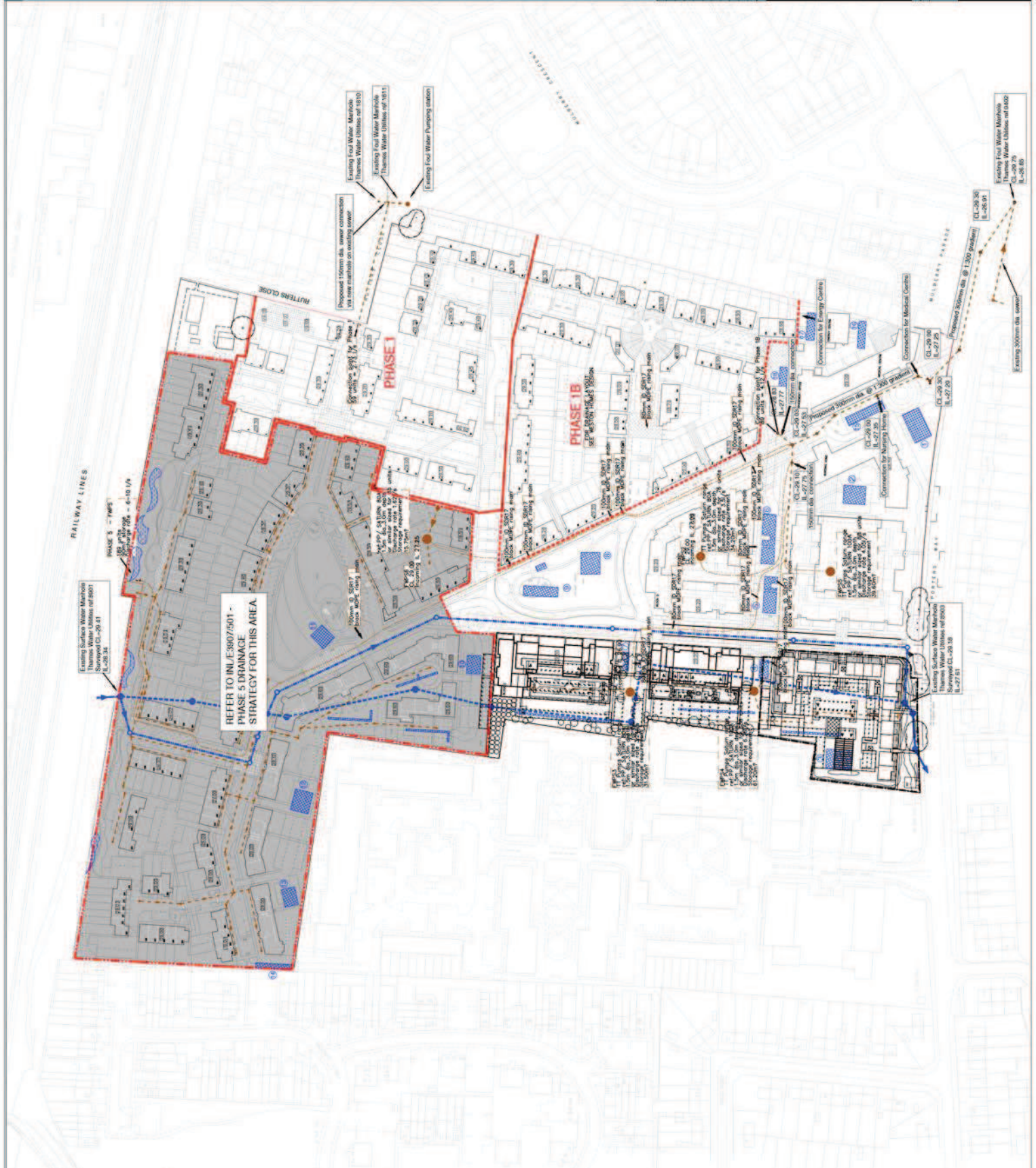
Client:
 DGV Ltd
 2 Anglo Office Park
 67 White Lion Road
 Antrim
 Baha HPT #19

Site:
 **DRAINAGE STRATEGY**

Scale: 1:500
Drawn: SLW
Checked: KJM

Drawing No.: INL/E3685/300 M

Project: Rogers Cory Partnership
 Civil, Structural and Architectural Consultants
 100 Old School Hill School Road
 Hook Norton Oxfordshire
 OX10 9JG
 Tel: 01295 765050
 Fax: 01295 765055
 Email: info@rogerscory.co.uk
 Website: www.rogerscory.co.uk





Landscaping is indicative. Refer to
 Landscape Consultant drawings for details

Preliminary

- Revisions:
- 00 08 September 14
 - 01 09 September 14
 - 02 03 October 14
 - 03 29 October 14
 - 04 30 October 14
 - 05 17 November 14
 - 06 18 November 14
 - 07 25 November 14



Drayton Garden
Village Block A

for

Inland Homes



1363-D2721-rev07

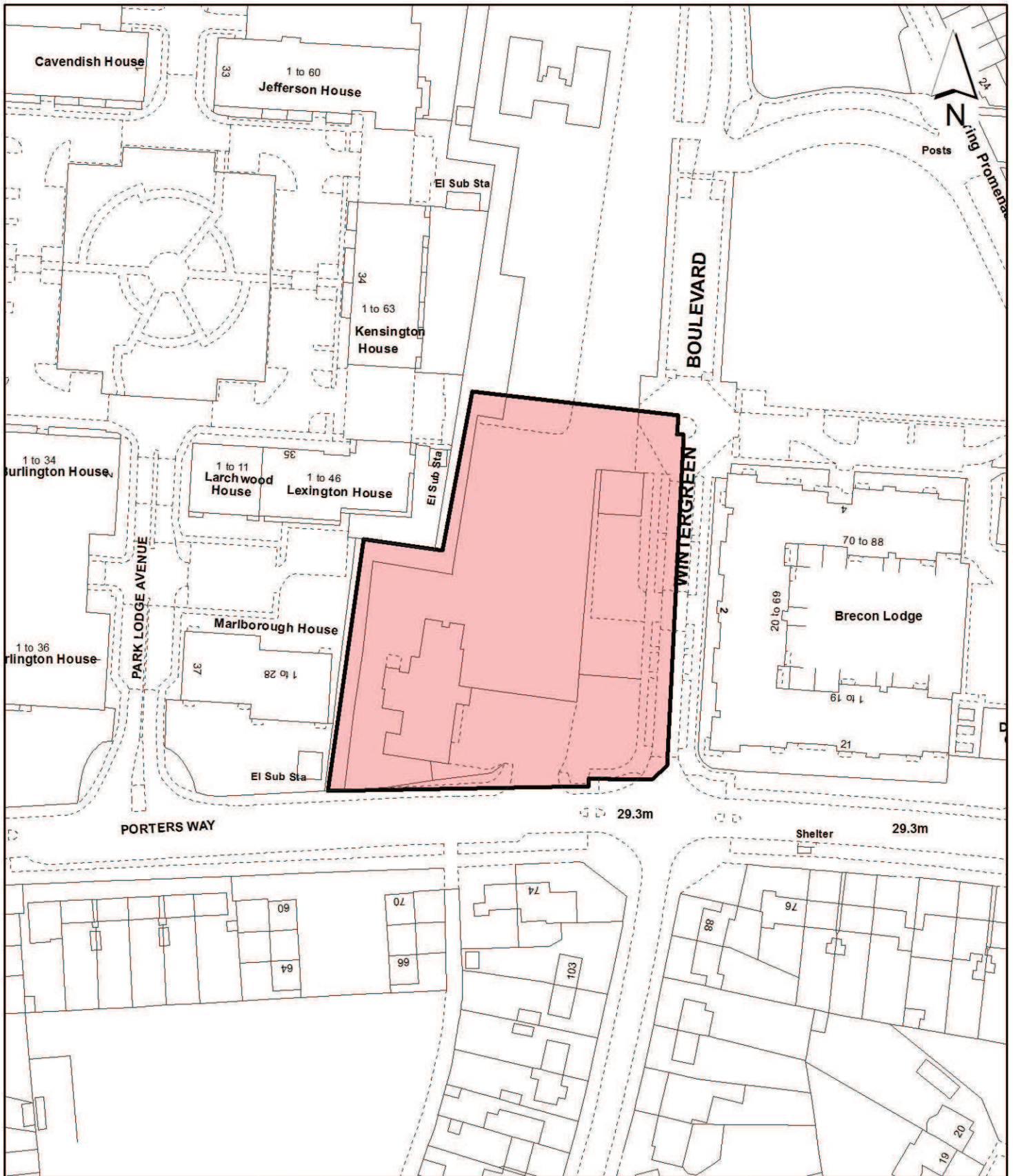
Scale: 1:200 @ A0
Date: 08 September 2014

Boulevard
Street Elevation



Landscaping is indicative. Refer to
Landscape Consultant drawings for details

Preliminary



Notes

 Site boundary

For identification purposes only.

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Site Address

**West Drayton Garden Village
Porters Way
West Drayton**

**LONDON BOROUGH
OF HILLINGDON**

**Residents Services
Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
Telephone No.: Uxbridge 250111

Planning Application Ref:

5107/APP/2014/4304

Scale

1:1,250

Planning Committee

Major Page 276

Date

March 2015



HILLINGDON
LONDON